

6 Settings and Views

As Wynyard Station has almost no aboveground presence, apart from the entrances to the station, there are no significant views to or from the subject site. The entrances to the station comprise predominantly of covered stairways and escalators from the surrounding buildings and streets, with major entrances through the original frontage on York Street and pedestrian subways from George Street, Pitt Street, and Wynyard Park.

Within Wynyard Station itself, the closed-in nature of the underground precinct minimises view lines. The lack of major site lines through the station concourse and on the platforms is also added to by the predominance of way-finding and information signage, station furniture and advertising displays.

Like the entrances, the station precinct is a highly transitional and utilitarian setting, its sole purpose being as a place of transport. Within the space, people mainly only linger on the platforms and, in these instances, are there with a very specific purpose. Therefore, the setting within Wynyard Station both reflects and facilitates its functional purpose and the nature of how people use the space.

Transport House is a prominent building within York Street, the architecture dominating the streetscape above the station. The entrances to the station are modest and utilitarian, neither contributing nor detracting from the environment.

Wynyard Park is identified as a local heritage item for a range of historical associations (see Statement of Significance in Archaeological Potential and Significance below). The above-ground features of the park are predominately late 20th century and include the dome which provides light and access into the eastern unpaid concourse of Wynyard Station, mature trees on the boundary of the park and sandstone landscaping elements including gates, garden edging vent enclosures and other minor elements. The park provides for a small view corridor in this area of York Street, and serves as both a green space for adjacent buildings to overlook, and an area from which significant buildings, including Transport House, may be viewed.

7 Heritage Items in the Vicinity

Wynyard Station, in addition to its associated elements of Transport House, the remnant tram tunnels and Wynyard Park, is in the vicinity of several heritage items, which are:

- The Menzies Hotel - Former “Shell House” including interior (I691) (refer to section 2.1);
- Former “Lisgar House” including interiors (I692) (refer to section 2.2);
- Former “John Sands” building including interior (I713) (refer to section 2.3);
- The Tank Stream (I1764) (refer to section 2.4);

The history and significance of these items is set out below.



Figure 58 - Heritage items in the vicinity of Wynyard Station

- Wynyard Station
- Wynyard Walk
- Local heritage item (City of Sydney LEP, 2012)

7.1 The Menzies Hotel, Former Shell House

Sydney Local Environmental Plan 2012	
Item Name	The Menzies Hotel - Former "Shell House" including interior
Listing No.	I1691
Address	2-12 Carrington Street, Sydney NSW 2000
Significance	Local

The Menzies Hotel is listed in Schedule 5, Part 1 Heritage Items of the *Sydney LEP 2012*. It was formerly the corporate offices for Shell Oil Co. The former Shell House is a prominent corner building, dominated by a clock tower and displaying a fine ochre-coloured glazed terracotta tile façade in the Inter-War Commercial Palazzo style. Consistent with this style, the building is clearly divided horizontally into three sections. The lower two storeys are boldly formed as a sandstone base with arched ground floor openings; the central section, of seven levels, has repetitive rectangular windows creating a simple façade; a projecting cornice and columns distinguish the upper two storeys.

Like many buildings of this style, the use of smoothly finished, durable materials was favoured. The original interior was striking, particularly for the inclusion of the corporate logo, however, much of this detail has been lost through subsequent works, including the 1978-1979 fit-out for the hotel. This building forms part of the One Carrington Street development.

Statement of Significance:

"The Menzies Hotel is significant because of its former association with a prominent corporate identity, the Shell Company, and reflects the growing importance of Sydney as a location for corporate headquarters. The building serves as an important historic record of that company's presence in all Australia's capitals by the late 1930s. Generally, it reflects an important period of development in the city during the 1920s-1930s. Its later conversion for use as a hotel reflects the growing importance of tourism and the hospitality industry. Its aesthetic and formal qualities are of a high standard. Situated on a prominent corner site, it makes a distinguished contribution to the townscape character around Wynyard Park and represents a scale of city streetscape prior to the "skyscraper era". It is a representative and late example of Inter-War Commercial Palazzo and it provides a now relatively rare illustration of the use of glazed tiles as an external facing material. It is unique in that it is the only major Inter-war Palazzo style building remaining that is clad in terracotta." (Sydney LEP 2012 Item #I1691)

7.2 Former Lisgar House:

Sydney Local Environmental Plan 2012	
Item Name	Former “Lisgar House” including interiors
Listing No.	I1692
Address	30-32 Carrington Street, Sydney NSW 2000
Significance	Local

National Trust of Australia Register	
Item Name	Lisgar House Facade
Listing No.	C6109
Address	30-32 Carrington Street, Sydney NSW 2000

The former Lisgar House is listed in Schedule 5, Part 1 Heritage Items of the *Sydney LEP 2012* and the National Trust of Australia (NSW) register. The building is a well-proportioned, eight-storey office building with a restrained brick facade overlooking Wynyard Park. It was designed in the Inter-war Georgian Revival style as offices with commercial tenancies on the ground floor. The symmetrical facade is a plain wall surface of fine face brick, consistent with the style, with regular fenestration of small multi-pane timber frame windows and doors.

Statement of Significance:

“The construction of Lisgar House was part of an ongoing tradition that established the CBD as an important commercial and financial focus, and, generally, reflects an important period of the development in the 1920s. An association with the original owners, the St Joseph's Investment and Building Society, is maintained by the statue of St Joseph the carpenter centrally located at the top level (moved from the original building). The relatively tall scale of this Georgian Revival style building and its unusual cornice treatment make it a rare example of the period. It is comparable to Beneficial House in George Street (refer no 2017) formerly Peapes menswear, and to some extent also the Royal Automobile Club in Macquarie Street (refer no 2114.) The building is an important contributor to the townscape character around Wynyard Park.” (Sydney LEP 2012 Listing #I1692)

7.3 Former John Sands Building:

Sydney Local Environmental Plan 2012	
Item Name	Former “John Sands” building including interior
Listing No.	I1713
Address	62 Clarence Street, Sydney NSW 2000
Significance	Local

The former John Sands Building is listed in Schedule 5, Part 1 Heritage Items of the *Sydney LEP 2012*. The building is a six-storey, Victorian Proto-Arts-&-Crafts commercial building, which remains highly intact. Originally constructed c.1882 as four storeys, the building was part of a row of four seemingly identical four-storey warehouses at No’s 56-62 Clarence Street. The building features eye-catching polychromatic brickwork, with a three bay façade. The interior of the building is well preserved, however, does not reflect the three-bay external construction, being a large space where substantial timber girders, resting upon sandstone corbels built into the sidewalls, span the full width of the building.

The internal space is framed by wide piers of cream bricks at intervals above modelled cement emblems at first floor level, which are supported by sandstone brackets and symbolise ‘progress’ on the north and ‘manufacture’ on the south. Between piers, brickwork is generally red-orange. Though the ground floor exterior has been rendered, the bay rhythm is maintained and the rest of the original fabric is substantially unaltered.

Statement of Significance:

“The former John Sands building is a reminder of the scale and quality of buildings in this part of Sydney. It has historic significance for its association with John Sands, one of Sydney’s oldest business houses. The building also displays evidence of its historic links with one of Australia’s leading publishing companies. The building has aesthetic significance for its extraordinary composition of polychrome brickwork, ceramic tiling, modelled cement and timber joinery, forming a harmonious, engaging ensemble and that enriches the streetscape.” (Sydney LEP 2012 Listing #I1713)

7.4 The Tank Stream

NSW State Heritage Register	
Item Name	Tank Stream
Listing No.	636
Address	(Various), Sydney NSW 2000
Listing Boundary	The Tank Stream has a curtilage of 3 metres from all surfaces shown as heavy black on plan in the office of the Heritage Council of NSW (<i>catalogue item #H.C. 1665</i>).

Sydney Local Environmental Plan 2012	
Item Name	The Tank Stream including tanks and tunnels
Listing No.	L1656
Address	(Various), Sydney NSW 2000
Significance	State

National Trust of Australia Register	
Item Name	The Tank Stream
Listing No.	C6455
Address	Pitt Street (below), Sydney NSW 2000

The Tank Stream is listed as an item of NSW State Heritage significance on the SHR, in Schedule 5, Part 1 Heritage Items of the *Sydney LEP 2012* and the National Trust of Australia (NSW) Register. The Tank Stream was the Colony's first reliable water source, beginning as a stream running into Sydney Cove, then gradually being channelled and covered over to improve flow and manage the supply.

The surviving fabric of the Tank Stream is extant in the south from King Street, at a point between Pitt and George Streets, and runs to Circular Quay in the north. The Tank Stream has been blocked at a point just south of King Street and, for 13 metres north of this point, represents the 1866 open sewer, which was covered in 1876. The dimensions are 810mm broad by 1220mm high. Between King Street and Martin Place (163 metres) there are three phases of construction, beginning with a modern concrete pipe (750mm diameter), the section approaching the GPO is part of the historic oviform sewer (810 by 1220mm) and lastly a stainless steel box-profile pipe (1070 by 750mm). The section between Martin and Angel Places returns to the 1866 brick open drains enclosed in 1876. This form continues between Angel Place and Hunter Street for 95 metres before being interrupted by a 36m of modern cement-lined pipe laid in 1962 and a steel section laid in 1958 and 1978 (both are 1350mm).

From Hunter to Bond Street, the Stream is a semi-circular stone arch with a shallow V shaped floor for 35 metres (1500 by 3000mm). The 1790 cut tanks were originally located in this area but are thought not to have survived. For the following 86

metres, below Australia Square, the sewer has been replaced with concrete box-profile pipe (1220 by 1830mm), inserted during construction of the Square in 1962.

The Australia Square Tower basement houses the access to Tank Stream for public tours and as an inspection point for Sydney Water. The boxed concrete section (1220 by 1830mm) continues from Bond Street to Abercrombie Lane, a distance of 60 metres. The Tank Stream between Abercrombie Lane and Bridge Street, a length of 40 metres, is of c.1860 semi-circular stone arch (1500 by 3000mm). From Bridge Street the Tank Stream diverts to run under Pitt Street to Circular Quay, a length of 185 metres. Beginning with a stone oviform sewer of 810mm by 1220mm, constructed in c.1878, the shape changes to a semi-elliptic stone arch of 3000mm and varying between 1100 and 1400mm.

Statement of Significance:

“The Tank Stream is significant because it was the reason the First Fleet settlement was established in Sydney Cove, and therefore influenced the future shape of Sydney over two centuries. It is linked in the public mind with the period of first European settlement and retains value as an iconic representation of that period and is interpreted as a metaphor of the period of contact and early urban settlement in Australia. The Tank Stream itself has retained an identity through the functional changes from being a fresh water supply, through subsequent use as combined sewer and stormwater drain to its current function as a stormwater drain. It is an important survivor of the first period of organised and integrated water management in an Australian city. The stone-cut water tanks, which may survive archaeologically, are important symbols of the reliance upon water in the colony, both in absolute terms and as an indication of the fragility of the European presence in Australia. The surviving fabric documents mid-nineteenth century sanitation design and construction, and subsequent changes in methods and also the theory of urban wastewater management. This evidence is preserved in the drain enclosing the Tank Stream, in physical evidence of change, and may also be present archaeologically in buried parts of the Tank Stream line. The archaeological evidence of the Tank Stream has the potential to contain deposits that can contain information about pre-human and pre-urban environments in Sydney, Aboriginal occupation and early non-indigenous occupation of Sydney. The fabric enclosing the watercourse demonstrates one of the most comprehensive collections of hydrological technology in Australia. The sections of the former Tank Stream south of King Street which survive have potential for retaining evidence of the earliest periods of its human use, although this is likely to have been severely compromised by development. The swampy source of the stream may provide evidence of past environmental conditions (Tank Stream Conservation Management Plan, Sydney Water, June 2003).” NSW State Heritage Register, Item #636

8 Archaeological Potential and Significance

The *Central Sydney Archaeological Zoning Plan 1997* identifies Wynyard Park, the public space directly above Wynyard Station, as an Area of Archaeological Potential – Partially Disturbed²². However, the ‘Schedule of Sites of Archaeological Potential’ in the *Central Sydney Archaeological Zoning Plan 1997* is not a statutory list.

The Statement of Significance relating to the archaeology of the Park and, specifically, the landscaping and historic associations of Wynyard Park, is as follows:

“Wynyard Park is the site of the first military barracks in Australia, the present park having its location, form and dimensions from the space of the original parade ground that the barracks occupied. The site is one of Australia’s oldest parks to be continuously set aside as open space for public use in an urban context. The park has maintained its role as an open square for public meetings, political protests, entertainment, assembly and passive recreation since its dedication as a public park in 1887. The park is held in high esteem by visitors, workers, and commuters to the city as a place of public assembly, passive recreation and a transport interchange and as a green corridor space within the built fabric of the city. Wynyard park is a landmark place and contributes significantly to the ambience within the city.”

(Source: “Archaeological Item – Wynyard Park”, State Heritage Inventory, Database #2424820)

In order to construct the Wynyard Station and the City Circle Rail Line, much of Wynyard Park was excavated down to, and through, bedrock²³. In view of the extent of this excavation, the potential for archaeological remains of the former military barracks, or from other periods, is low²⁴. However, the archaeological potential of the area cannot be entirely eliminated. As such, an excavation external to the station precinct could potentially require approval under the *Heritage Act 1977*, depending on the location and depth of excavation.

The areas proposed to be affected by this project have been substantially excavated into rock for the construction of Wynyard Station, the City Circle Rail Line, the former tramway tunnels and the four basement levels of Transport House. While the proposed works would potentially require excavation to the southern half of Wynyard Park, it is expected that the area would be archaeologically sterile, based on the low probability of the survival of any items of archaeological significance.

²² Lavelle, S. & Mider, D. (1993) *Archaeological Zoning Plan for Central Sydney*; City of Sydney Council, Sydney, page 11

²³ Ibid.

²⁴ NSW Office of Environment & Heritage: Online Heritage Database,

<http://www.environment.nsw.gov.au/heritageapp/ViewHeritageItemDetails.aspx?ID=4800281>.

The Tank Stream (SHI Item #636) is also located within the general vicinity of Wynyard Station. However, only the pedestrian subway from the concourse to Pitt Street crosses over the path of the Tank Stream. As no excavations or other works are proposed in this area, there would be no archaeological impact to the Tank Stream.



Figure 59 – Construction of Wynyard Station, c. 1940s. View of Wynyard Park looking northwest towards corner of York and Margaret Streets. Note the extensive disturbance of the area for the construction works, making the archaeological potential of the area low. (Source: “Wynyard Park under Construction for the Underground Railway”, City of Sydney Archives, file # 036/036148).



Figure 60 – Pouring the roof slab over Wynyard Station during its construction in the 1930s. Photograph dated 19 May 1930. (Source: State Records NSW, image ID 12685_a007_a00704_8735000019r.jpg)



Figure 61 - Military barracks parade ground (now Wynyard Park) and the Tank Stream

- Wynyard Station
- Wynyard Walk
- Local heritage item (City of Sydney LEP, 2012)

9 Proposal

9.1 Outline of Proposed works

The proposed works have been divided into the five distinct precincts:

1. Wynyard Station – Platforms
2. Wynyard Station – Paid Concourse
3. Wynyard Station – Unpaid Concourse
4. Transport House –
 - York Street Foyer
 - Basement Level 1
 - Basement Level 2
 - Basement Level 3
 - Concourse Level (B4)
5. Wynyard Park

Please refer to the plans at the end of this chapter for details of the proposed works.

It should be noted that works in Precincts 1, 2, 3 and 5 are within areas listed on the RailCorp Section 170 Heritage and Conservation Register and/or the *Sydney Local Environmental Plan* and are therefore subject to the approval requirements of the *State Environmental Planning Policy (Infrastructure) 2007* and Part 5 of the *Environmental Planning and Assessment Act 1979*.

Works within Precinct 4 are within the State Heritage Register curtilage of Transport House and are therefore subject to the approval requirements of section 60 of the *Heritage Act 1977*.

Excavation works within Precinct 5, if any archaeological impacts are identified, are subject to the approval requirements of section 139 of the *Heritage Act 1977*.

The areas and spaces within each of these precincts are defined in Section 2.2 above.

The proposal for the paid and unpaid concourse precincts have been combined in section 9.1.2 below.

9.1.1 Precinct: Wynyard Station - Platforms

The following works have been proposed to the commuter platforms at Wynyard Station:

1. Application of new finishes to all platform floor surfaces, including tiles and tactile indicators.
2. Application of new to all stair surfaces up to the concourse level.
3. Redundant and existing services to be removed or concealed but access to essential services is to be maintained.
4. Application of new surface treatment to the walls and balustrading on all platforms, including both tile and paint finishes.
5. De-cluttering of platforms and removal of all non-essential features.

6. Replacement of existing seating with newer style approved seats or rest-rails.
7. Platform control rooms to be reduced in size to facilitate more platform space for customers.
8. Redundant storerooms, staircases, control rooms etc. on all platforms to be removed.
9. Doors to rooms on platforms to be replaced for fire safety upgrades.
10. Replacement and upgrade of lighting.
11. Installation of a temporary hoist during construction through floor of platforms 3 & 4 and through the roof of platforms 5 and 6.

9.1.2 Precinct: Wynyard Station – Paid Concourse

The following works are proposed to the paid concourse at Wynyard Station:

1. Change of the existing rectangular/linear gateline to a curved gateline design. The design is to be created using a combination of screens that are not full height and new opal ticket gates.
2. Expanding the pedestrian access route to the north of the paid concourse, to link with Wynyard Walk.
3. Use of lighting, finishes and ceiling treatments to accentuate the transition between the two distinct precincts.
4. Removal of the suspended ceiling and re-routing of services within the paid concourse, to expose the original ceiling structure with reinstatement of recessed lantern lights similar to the original lighting design.
5. Reorientation of the southern-most stairs, from the paid concourse area to Platforms 5 and 6, to face southwards, requiring the partial demolition of the stair.
6. Insertion of a new stair at the south end of platforms 3/4 to better facilitate passenger access.
7. General de-cluttering and refurbishment, including re-tiling, flooring, lighting, new paint finishes, improved signage and wayfinding.
8. Enclosure of the southern unpaid concourse and station amenities area into the paid concourse with attendant expansion and refurbishment.

The proposed design by Cox Architects Pty Ltd for the paid concourse retains the remnant original structure and applies contemporary finishes for areas of the unpaid concourse. The base of the escalators on the western side of the unpaid concourse (also level B4 of Transport House) serves as a transition zone between the historic fabric of Transport House and the contemporary treatments of the paid concourse.

While the structural grid and columns remain throughout the station, the proposed new ticket gate design is proposed as a curved alignment, which accommodates improved sight lines to gates and allows for a greater volume of passenger movements. This is driven by the substantial increase in patronage, which is expected upon the completion of Wynyard Walk and growth in patronage generally. At peak times, the patronage is expected to increase substantially from existing usage. The new curved ticket gatelines and screens would provide a relatively

transparent view between the paid and unpaid concourses. Lighting would be used to provide a clear delineation between the paid and unpaid concourses.

Within the paid concourse, the original ceiling structure is proposed to be revealed, the intent being to show the column and beam structures with reconstructed lantern lights within the coffered ceiling recesses. Columns would be refinished to retain the original curved corners and the original blue feature colour for Wynyard Station re-introduced.

The remaining heritage features within this area, in addition to the ceiling structure itself, are restricted to the metal balusters and timber handrails, which are being retained. The staircase to Platforms 5/6 is being reoriented to improve passenger movements, however, the baluster and handrail would be retained, reinstated and generally detailed to match the other platform stairs. In general, the paid concourse would present as an uncluttered space and would reveal heritage features that are currently concealed by the suspended ceiling.

9.1.3 Precinct: Wynyard Station – Unpaid Concourse

Within the unpaid concourse, the existing retail spaces would be substantially altered to provide additional space for passenger movements to the north of the paid concourse. These spaces are heavily modified and only contain small elements of original decorative ceiling plaster (related to the former Grill Room), in some areas. Sections of this remnant decorative ceiling would be lost. However, it may be possible to retain and conceal some elements, subject to the rerouting of services. Detailed design for the interiors of the retail spaces and the station facilities would be subject to future planning and a separate approval. An existing fire stair would be reconfigured within the current retail spaces along the north of the unpaid concourse. With the exception of the remnant ceiling features in some areas, there is no other heritage fabric in this area, other than the underlying station structure.

The existing modern lift core and stair within the eastern unpaid concourse, which provides access to Wynyard Park above, would be retained and refurbished.

Within the transition zone between the base of the escalators and the proposed new west side of the paid concourse, the original structural fabric of the unpaid concourse ceiling is exposed and reinstated to its full height, with services routed to the edges of the area, allowing the full exposure of the original structural grid.

Within the eastern unpaid concourse, a new suspended ceiling would be constructed. This would serve to conceal the considerable number of services within this area, and facilitate the exposure of the ceiling structure in the paid concourse and western area of the unpaid concourse. The eastern unpaid concourse would use a modern lighting treatment. The final lighting scheme for these areas is subject to detailed design.

The gateline configuration is subject to detailed design. During detailed design, the configuration may undergo refinement to ensure it maximises customer experience,

meets operational requirements and delivers a high standard of architectural design²⁵.

²⁵ Caitlin Bennett, AECOM *pers. Comms.* 10/11/14.



Figure 62 - Proposed Wynyard Station - concourse layout (indicative)



Figure 63 – The proposed works overlaid over the original layout of Wynyard Station (Source: Futurepast 20/10/2014)

9.1.4 Precinct: Transport House

The following works proposed to the Basement Levels of Transport House include work that was identified as being in the western unpaid concourse (Basement Level B4). Much of the proposed works within the Transport House basements are to reactivate areas which are presently disused within the building for staff accommodation, including relocation of some of the existing station services from the concourse levels to facilitate the improved passenger flow at concourse level.

The work specifically affecting fabric in Transport House includes:

York Street Foyer

- General de-cluttering and refurbishment of the York Street Foyer, including re-tiling, new paint finishes, improved signage and wayfinding.
- Removal of the existing suspended ceiling to restore the cross-beam ceiling configuration.
- New Art Deco style ceiling box lights reflective of the original lighting design.
- Reconfiguration of rooms within the western back of house areas – these areas are primarily staff service corridors and lifts.
- Refurbishment of original tile walls and terrazzo flooring within the western staff corridor.
- Reinstating the operational capacity of York Lane service lift from the York Street Lobby.



Unpaid Concourse
 Station facilities
 Surrounding buildings & arcades

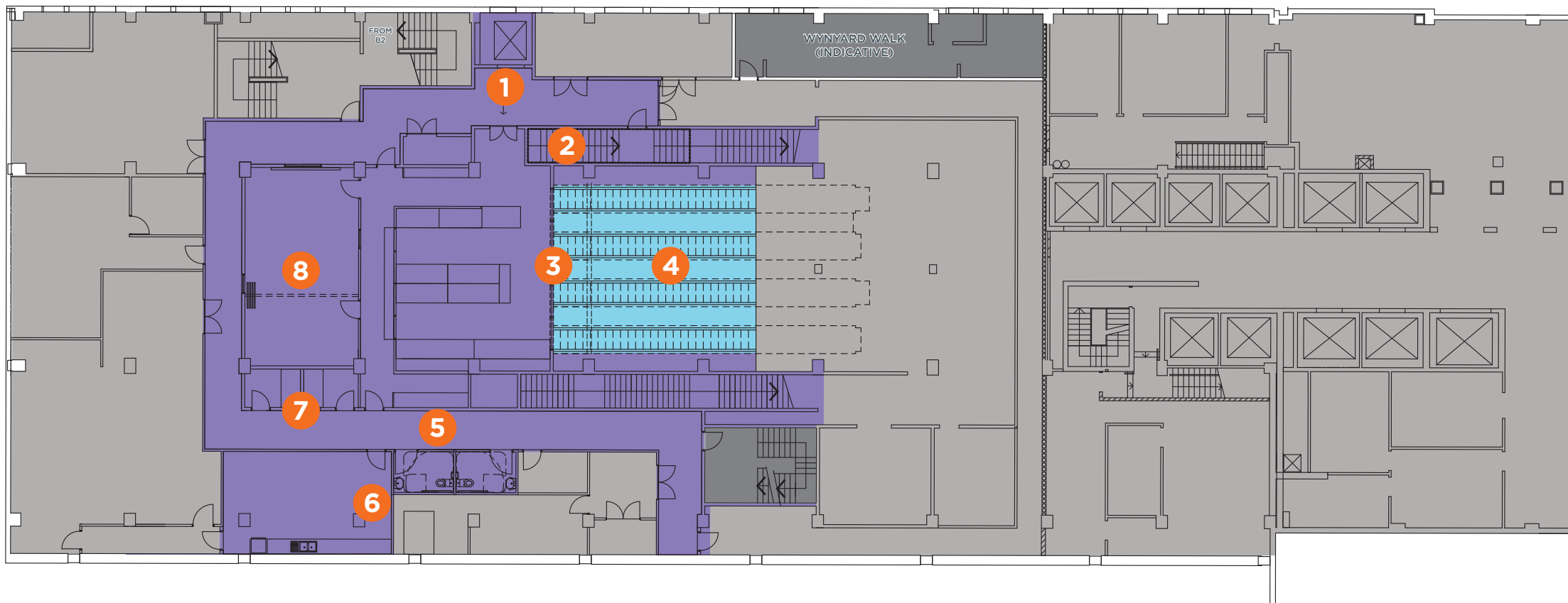
- 1
 REPLACEMENT OF LIFT AND REINSTATEMENT OF YORK LANE LIFT OPENING
- 2
 REFURBISHMENT OF WESTERN CORRIDOR
- 3
 REFURBISHMENT OF YORK STREET FOYER
- 4
 REFURBISHMENT OF YORK ST ESCALATORS AND TEMPORARY WORKS PLATFORM



Figure 64 - Transport House York Street foyer layout (indicative) - key heritage works

Basement Level 1 (B1)

- Demolition of existing bulkhead above escalators and installation of a viewing window to the northern wall of the back of house office directly above the escalators.
- Reconfiguration of rooms in back of house areas for use as staff accommodation.
- Reconfiguration of stairwells to accommodate pedestrian access between back of house levels B1 to B4.
- Refurbishment and reuse of heritage joinery (primarily architraves and doors) within the replanned office layout.
- Refurbishment of back of house areas, including services and amenities as required.
- Reconstruction of the western fire stair, including a new door opening to access B1 (this stair commences from B3).



Unpaid Concourse
 Station facilities
 Surrounding buildings & arcades

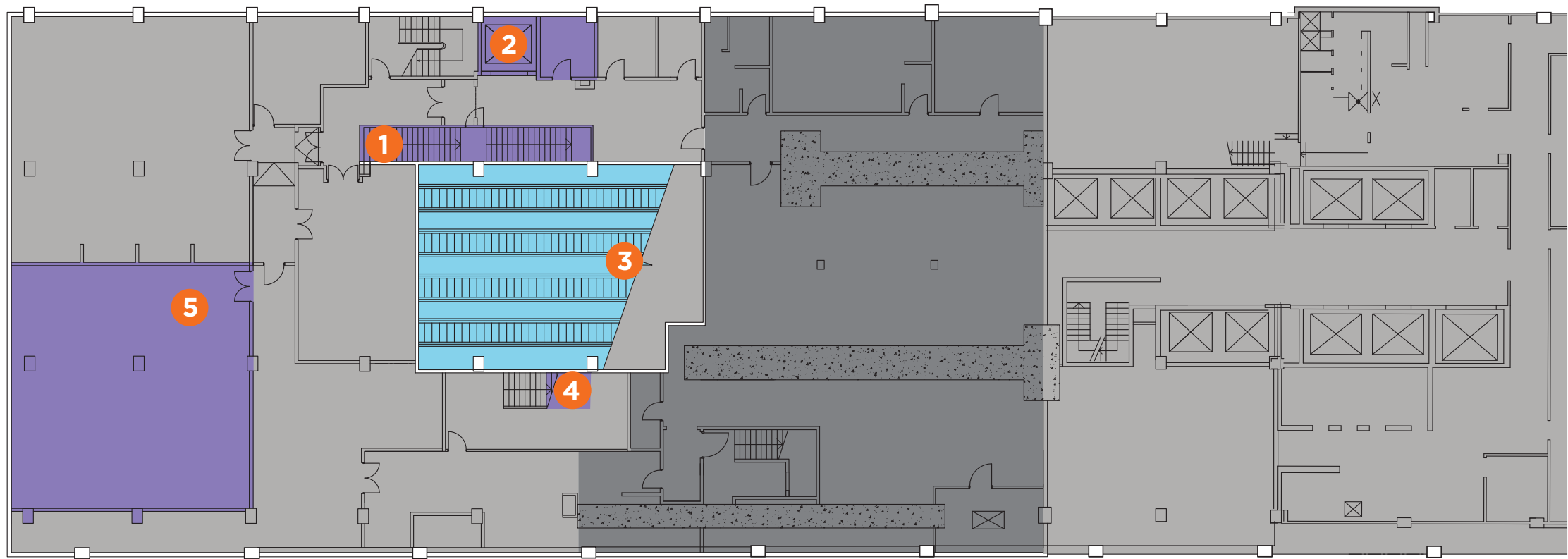
- | | |
|---|---|
| <p>1 REPLACEMENT OF LIFT</p> <p>2 RECONSTRUCTION OF WESTERN FIRESTAIR BETWEEN B3 AND B1, INCLUDING NEW DOOR TO B1</p> <p>3 REMOVAL OF SOFIT AND INSTALLATION OF WINDOW OVERLOOKING YORK ST ESCALATORS</p> <p>4 REFURBISHMENT OF YORK ST ESCALATORS AND TEMPORARY WORKS PLATFORM</p> | <p>5 RE-USE OF TWO HERITAGE TIMBER DOORS</p> <p>6 REMOVAL OF ORIGINAL TILES AND DEMOLITION OF WALL AND CONSTRUCTION OF NEW WALL</p> <p>7 IN-SITU RETENTION OF HERITAGE DOORS</p> <p>8 FITOUT OF B1 AREAS FOR STATION FACILITIES</p> |
|---|---|



Figure 65 - Transport House, basement level 1 layout (indicative) - key heritage works

Basement Level 2 (B2)

- Truncation of eastern fire stair between B2 and B4.
- Reconfiguration of existing stairwells to facilitate staff movement between basement levels.
- Refurbishment of the existing office accommodation within the south eastern portion of B2 for use as a communications room.
- Refurbishment of back of house areas including services and amenities as required.
- Reconstruction of the western fire stair from B3 to B1.



Unpaid Concourse
 Station facilities
 Surrounding buildings & arcades

1 RECONSTRUCTION OF WESTERN
 FIRESTAIR BETWEEN BASEMENT
 LEVEL 3 AND BASEMENT LEVEL 1

2 REPLACEMENT OF LIFT

3 REFURBISHMENT OF YORK ST
 ESCALATORS AND TEMPORARY
 WORKS PLATFORM

4 DEMOLITION OF EASTERN
 FIRESTAIR BETWEEN BASEMENT
 LEVEL 4 AND BASEMENT LEVEL 2

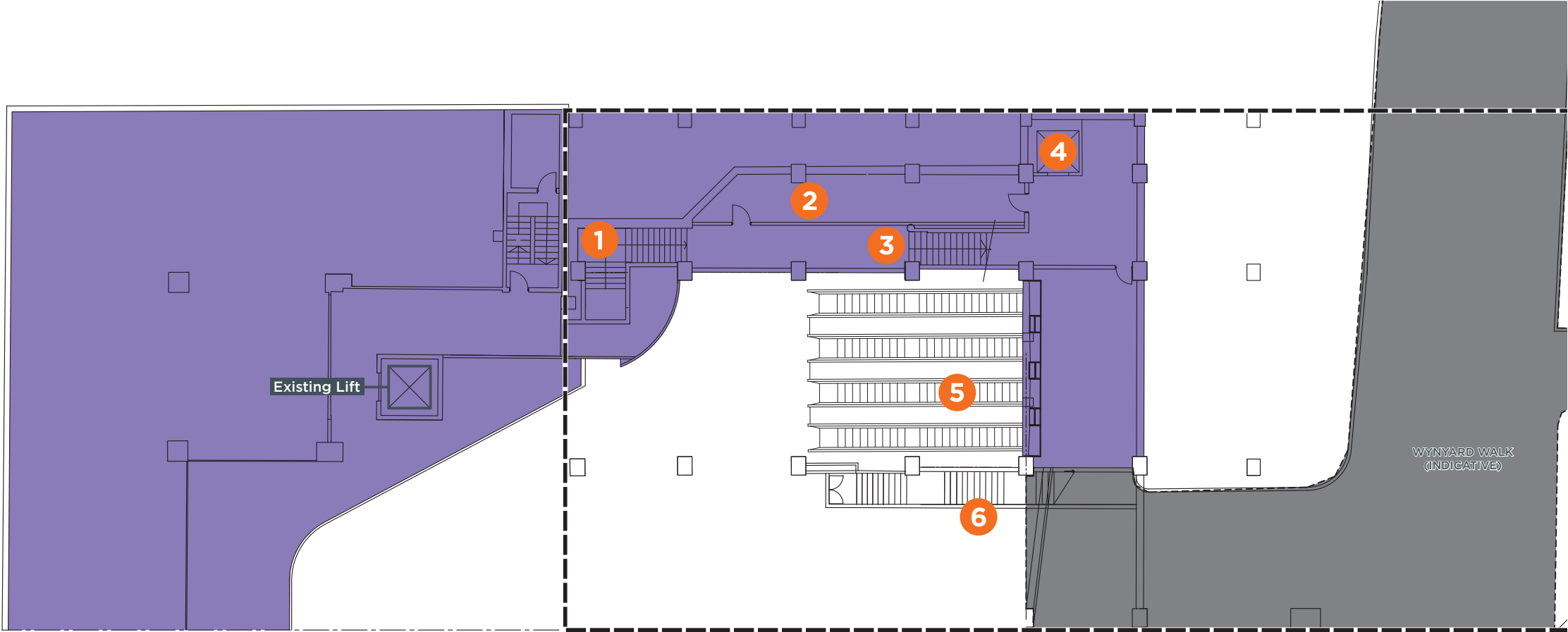
5 FITOUT FOR STATION FACILITIES







Figure 66 - Transport House, basement level 2 layout (indicative) - key heritage works

Basement Level 3 (B3)

- Demolition of eastern fire stair from concourse level (B4) through to Basement Level 3 (note this stair is not accessible from B3 but passes through the level).
- Demolition and reconfiguration of southwest stair from B4 as a compliant stair, with reinstatement of the existing metal balustrade.
- Refurbishment of wall and floor finishes throughout the staff corridors.
- Refurbishment of back of house areas including services and amenities as required.
- Reconstruction of the western fire stair from B2 to B1.



-  Transport House state heritage listing curtilage
-  Unpaid Concourse
-  Station facilities
-  Surrounding buildings & arcades



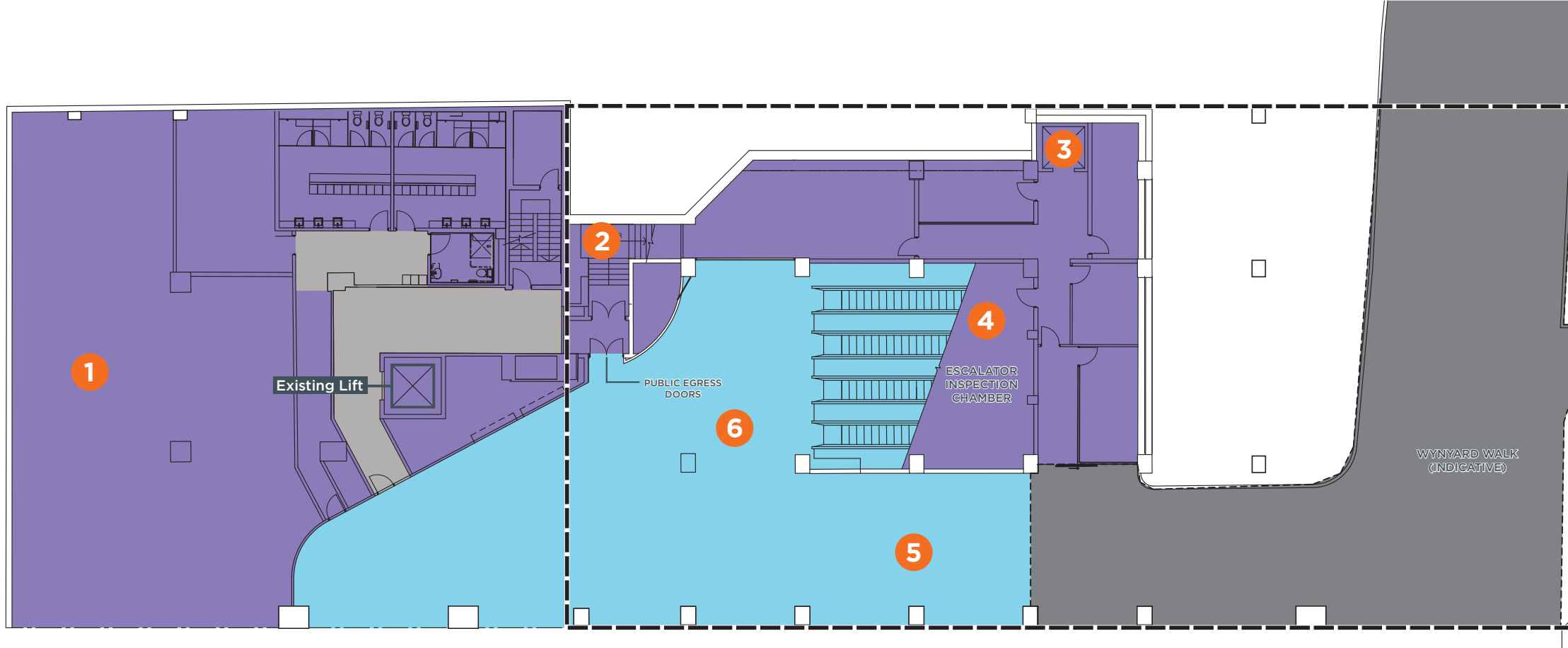
-  1 RE-ORIENTATION OF STAIR BETWEEN BASEMENT LEVEL 3 AND 4
-  2 FITOUT OF BASEMENT LEVEL 3
-  3 RECONSTRUCTION OF WESTERN FIRESTAIR BETWEEN BASEMENT LEVEL 3 AND BASEMENT LEVEL 4
-  4 REPLACEMENT OF LIFT
-  5 REFURBISHMENT OF YORK ST ESCALATORS AND TEMPORARY WORKS PLATFORM
-  6 DEMOLITION OF EASTERN FIRESTAIR BETWEEN BASEMENT LEVEL 4 AND BASEMENT LEVEL 2







Figure 67 - Transport House, basement level 3 layout (indicative) - key heritage works

Concourse Level (B4)

- Enlarging the western unpaid concourse through the demolition of the Concourse Bar, reconstruction and reconfiguration of the space and use of the space for station facilities (such as back of house or additional retail space). It should be noted that the area of the Concourse Bar is outside of the State Heritage Register curtilage for Transport House.
- Partial demolition of the eastern fire stair adjacent to the escalators, from Concourse Level B4 to Basement Level 2.
- Demolition of old stair to the southwest of the escalators, to be replaced with new compliant stairs with access to York Street Foyer.
- Removal of suspended ceiling at the base of the escalators in order to expose a section of original beams.
- Sanding and polishing of the timber finishes to the escalators.
- Refurbishment of the York Street escalators including painting, new tiles and the sanding and polishing of the timber finishes
- Installation, use and decommissioning of a temporary elevated works platform above the York Street escalators.
- Establishment of new floor and wall finishes to match the unpaid concourse.
- Refurbishment of back of house areas including services and amenities as required



WYNARD STATION

-  Transport House state heritage listing curtilage
-  Unpaid Concourse
-  Station facilities
-  Surrounding buildings & arcades

1 FITOUT OF STATION FACILITIES

4 REFURBISHMENT OF YORK ST ESCALATORS AND TEMPORARY WORKS PLATFORM

2 RE-ORIENTATION OF STAIR BETWEEN BASEMENT LEVEL 3 AND BASEMENT LEVEL 4

5 DEMOLITION OF EASTERN FIRESTAIR BETWEEN BASEMENT LEVEL 4 AND BASEMENT LEVEL 2

3 REPLACEMENT OF THE LIFT

6 REFURBISHMENT OF UNPAID CONCOURSE



Figure 68 - Transport House, basement level 4 layout (indicative) - key heritage works

9.1.5 Wynyard Park

The following works are proposed to Wynyard Park:

- Potential provision of temporary construction access and compound to Wynyard Station through the southern portion of Wynyard Park, namely between the Wynyard Park entry pavilion and Erskine Street. This would include the construction of a temporary construction hoist, which would require the penetration of the car park structure.
- Protection of the heritage features of the Park during construction.
- Reinstatement of the Park at the completion of works.

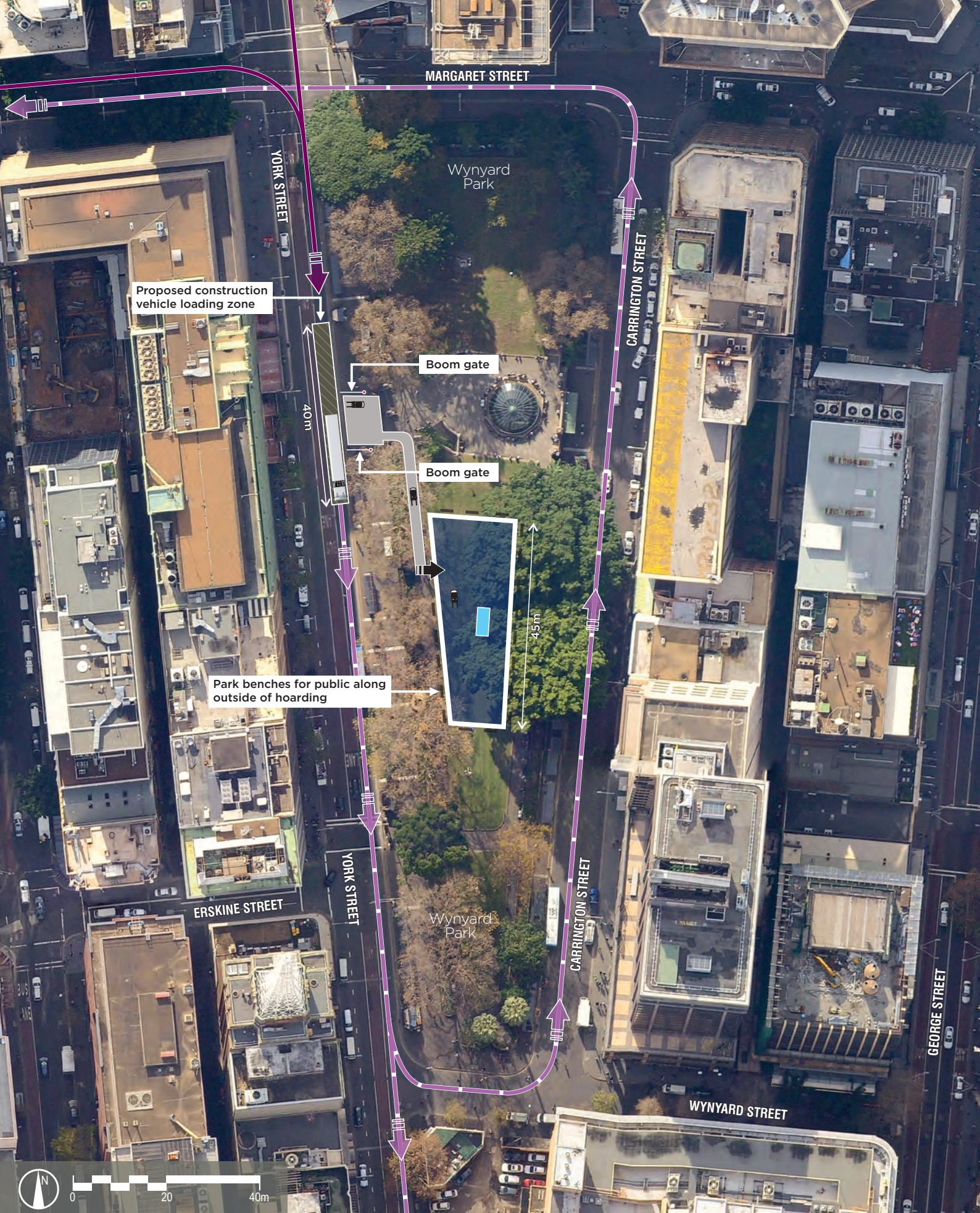







Figure 69 - Wynyard Park compound

- | | | | |
|---|--|---|------------------------|
|  | Proposed hoist location |  | Inbound vehicle route |
|  | Proposed Construction vehicle loading zone |  | Outbound vehicle route |
|  | Proposed Construction path (forklift) | | |

9.2 Rationale

The need for the changes to the station, adjacent concourse areas and areas within Transport House is driven by three major factors:

- The need to accommodate projected passenger demand. This includes growth in demand that is attributed to Barangaroo, One Carrington, CSELR and the North West Rail Link (NWRL).
- Associated with this growth is the future connection of Wynyard Walk to the station, which would provide a link to Barangaroo and the western CBD. This is anticipated to have the potential to accommodate 21,000 pedestrians per hour, which includes pedestrians passing through the station. This additional space needs to be created to facilitate safe and comfortable passage through the station.
- The need for a general ‘station refresh’ to address worn finishes and fittings and enhancing passenger experience through the improvement of pedestrian flows and movements.

The existing Wynyard Station Concourse (including both paid and unpaid concourses) consists of a post-1980s overlay with subsequent ad-hoc alterations, changes and upgrades. Original features and design elements are largely obscured, excepting elements such as the metal balusters and timber handrails on the platform staircases. The ticket gates are late 20th century and they have been repositioned and expanded over time as the demands on the station have progressively increased.

Within the unpaid concourse, there are very few elements that reflect the original design of the station, apart from the structural grid. Remnant plasterwork ceilings of the former room survive in some of the retail tenancies along the north side of the unpaid concourse and the potential exists for remnant finishes and features to be concealed above suspended ceilings and behind partition walls. Generally, however, the layout of the eastern concourse has been fundamentally altered and the most significant original elements, including the former Grill Room, have been completely lost. The remnant ceilings represent a very small portion of the original Art Deco detailing of the retail spaces.

This is reflected in the heritage listing for the station, which ascribes significance primarily to the platforms and the timber escalators at the western side of the unpaid concourse. The concourse area itself is acknowledged as having been significantly altered over time:

“Integrity/Intactness: Wynyard Station is largely intact in terms of its broad functional layout and features, although the retail and public concourse, as well as the platform décor, has been continually upgraded in line with trends in public architecture and retail presentation.”²⁶

²⁶ Ibid

The works to connect into the basement levels of Transport House are an opportunist to allow Sydney Trains to relocate some staff and back of house functions from the concourse level into the under-utilised basement levels of that building. This would free up more space for pedestrian flow and other uses that require direct interfaces with the public domain.

10 Assessment of Heritage Impact

The impact assessment for this project has been individually considered according to the five distinct precincts:

1. Wynyard Station – Platforms
2. Wynyard Station – Paid Concourse
3. Wynyard Station – Unpaid Concourse
4. Transport House
 - York Street Foyer
 - Basement Level 1
 - Basement Level 2
 - Basement Level 3
 - Concourse Level (B4)
5. Wynyard Park

A summary outline of the areas and spaces in each of these precincts is in Section 2.2 above. Each of these areas have been separately assessed for heritage impact. Where specific conservation treatments have been considered, the recommendations have been included against each proposed work.

Due to the interface between the paid and unpaid concourse, the impacts for these two precincts have been combined in Section 10.2 below.

10.1 Wynyard Station – Platforms

Proposed Works

Application of new floor finishes to all platform floor surfaces.

Assessment

The current platform floor surface is a modern replacement, dating to the station upgrade works in the 1980s, with other more recent modifications. There is no identified original surface to the platforms, though the potential exists for original fabric to remain beneath the modern finish in certain areas.

Though the original colours and finishes of the station precinct have been determined through historical and archival research, including the original bituminous and concrete platform surface, to remain consistent with the RailCorp corporate strategies and design guidelines for station upgrades, as well as safety specifications, new finishes would not be a replica of original elements.

The current floor tiling is expected to be replaced. As the current tiling to be replaced is not original, this work involves removal of non-significant functional fabric and replacement with a more modern version of the same finish. The proposed new tiles would result in a cleaner, more modern aesthetic for the platforms, improving visual amenity for commuters. The tactile and safe area indicators would be retained or replaced as necessary.

Heritage Impact

Although reinstating original finishes would be more desirable from a heritage perspective, there are practical advantages to the proposed finishes (particularly durability) and there is no substantive adverse heritage impact arising from this proposed item of works.

Where any historic finishes are exposed during works, these finishes should be recorded, sampled and, if possible, have a sample retained in situ for potential use in future upgrade works.

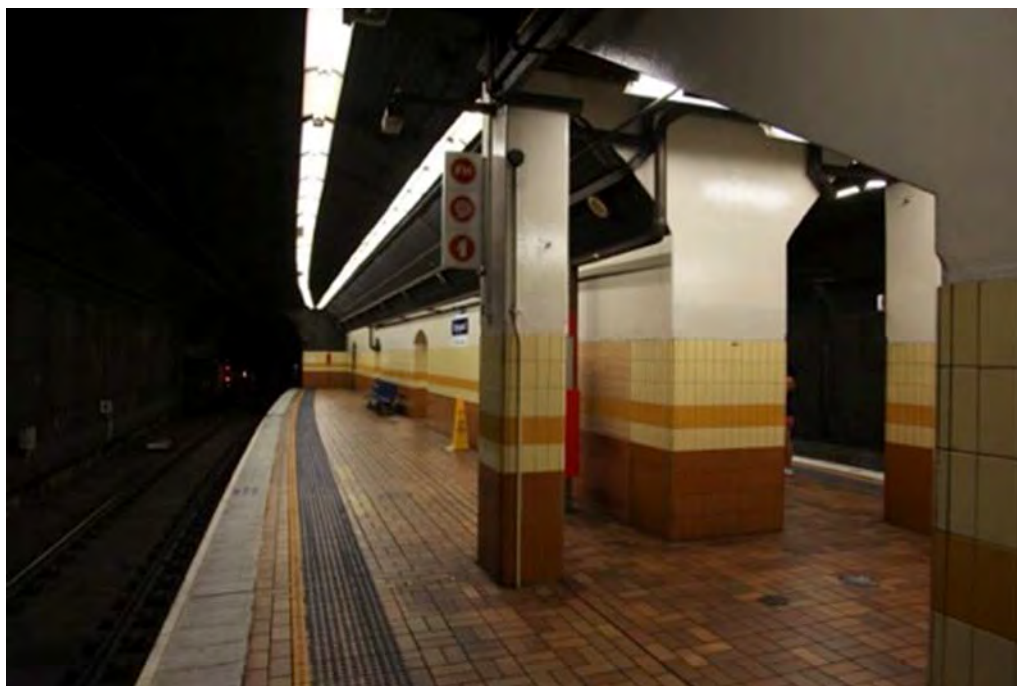


Figure 70 – View along Platform 5, showing the existing floor tiling from the 1980s upgrade works (Futurepast 2013).



Figure 71 – View along Platforms 3 and 4, showing the existing floor tiling from the 1980s upgrade works (Futurepast 2013).

Proposed Works

Application of new tiles to all stair surfaces up to the concourse level.

Assessment

The current stair tiles are a modern replacement, dating to the station upgrade works in the 1980s, with other recent modifications. Other than the balustrading, there are no other original features identified on the stairways.

Though the original colours and finishes of the station precinct have been determined through historical and archival research (see Section 5 for details), to remain consistent with the Sydney Train's corporate strategies and design guidelines for station upgrades, as well as safety specifications, new finishes would not be a replica of original elements.

The current stair tiling on both risers and treads is to be replaced with modern tiles in a neutral tone. As the current tiling to be replaced is not original, this work involves removal of non-significant functional fabric and replacement with a more modern version. The proposed new finishes would result in a cleaner, modern aesthetic for the stairs, improving visual amenity for commuters. The replacement of the much worn non-slip stair nosing on the stair treads would also improve customer safety.

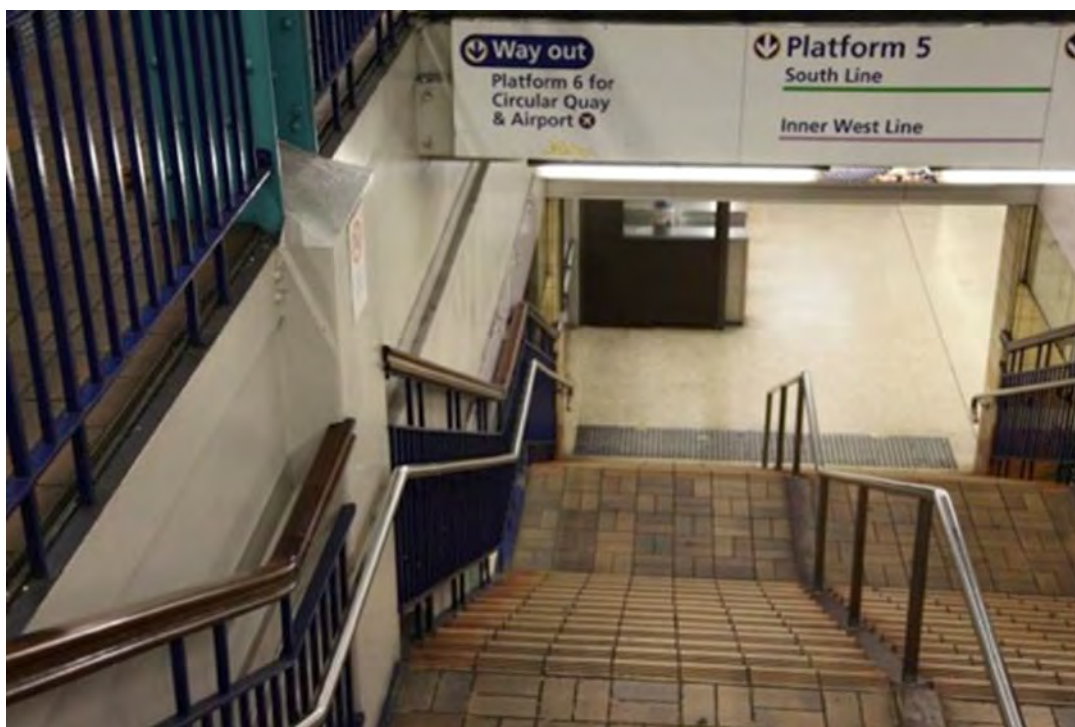


Figure 72 – View of the stairs from Platforms 3 and 4 down to the paid concourse. The non-original tile finish is to be replaced. Note the retained original balustrading (*Futurepast 2013*).

Heritage Impact

Although reinstating original finishes would be more desirable from a heritage perspective, there are practical and safety advantages to the chosen finishes and there is no substantive adverse heritage impact arising from this item of works.

Proposed Works

Application of new surface treatment to the walls, columns and balustrading on all platforms, including both tile and paint finishes.

Assessment

The majority of the current finishes within the platform areas are modern replacements, dating to the station upgrade works in the 1980s. There are no identified original wall or column finishes on any of the platforms. There are also no identified original signage elements on any of the platforms.

The only original elements identified within the platform spaces are the metal and timber balustrading on the stairs, and the riveted steel girder columns on Platforms 3 and 4. The metal components of the balustrading and railings and the columns have, in many cases, been repainted a number of times during previous refresh works. Anecdotal evidence suggests previous upgrade works involved the removal of original lead paint throughout the station. Therefore, paint sample testing to discern the original colour of painted metal elements was not seen as practical, as the original colour may no longer exist beneath the new paint layers. However, should the painted metal elements be required to be stripped during the painting works,

testing should be undertaken first to determine whether original finishes do exist, to allow such finishes to be recorded.

Where the timber handrails of the balustrades have been painted previously, the repainting works may include the timber. However, where the timber remains unpainted, which is the case on all of the main staircases to the platforms, the timber should not be painted over and doing so would result in a negative impact to original station fabric.

Though the original colours and finishes of the station precinct have been determined through historical and archival research (see Section 5 for details), to remain consistent with the RailCorp corporate strategies and design guidelines for station upgrades, the new finishes would not be a replica of original elements.

The walls are to be retiled from floor to ceiling with rectangular tiles in neutral tones. Columns, which are currently tiled, would be retiled in a similar fashion, while metal columns and structural elements would be painted in complementary neutral tones. As none of the finishes to be replaced are original, this work involves removal of non-significant functional fabric and replacement with a more modern version of the same finishes. The proposed new finishes would result in a cleaner, more modern aesthetic for the platforms, improving visual amenity for commuters.

Heritage Impact

Although reinstating original finishes would be more desirable from a heritage perspective, there are practical advantages to the chosen finishes and there is no substantive adverse heritage impact arising from this proposed item of works.

A paint test analysis should be undertaken prior to repainting works.

Consideration should be made regarding reinstating historic styles that reflect the original Sydney city circle station signage, particularly any styles, materials and colour schemes specific to Wynyard. Reflection of the original signage designs for Wynyard Station would be advantageous for interpretation in these areas. Advice can be sought from the Sydney Trains heritage specialist regarding this.



Figure 73 – View of Platforms 3 and 4. The original balustrading to the stairs and the riveted girder columns are to be repainted (*Futurepast 2013*).



Figure 74 – Photograph of Platform 5, showing the non-original wall and column tiling which is to be replaced (Futurepast 2013).

Proposed Works

Redundant and existing services to be removed or concealed but access to essential services is to be maintained.

Assessment

As a component of re-finishing the walls and columns on the platforms, redundant services are to be removed and existing essential services, which require retention, are to be retained and made accessible where necessary. All services are modern intrusive accretions of no heritage significance, which in many cases detract from the visual aesthetic of the station.

The removal of redundant services involves removal of non-significant functional fabric. The concealing of essential services would involve modification to the new finishes, which would be simultaneously installed and, therefore, would have no

impact on significant fabric. This work is positive in that it would result in a cleaner and more streamlined appearance to the platforms.

Heritage Impact

There is no adverse heritage impact arising from this proposed item of works. The removal and relocation of modern services to more discrete locations would have an overall positive impact on the platforms.

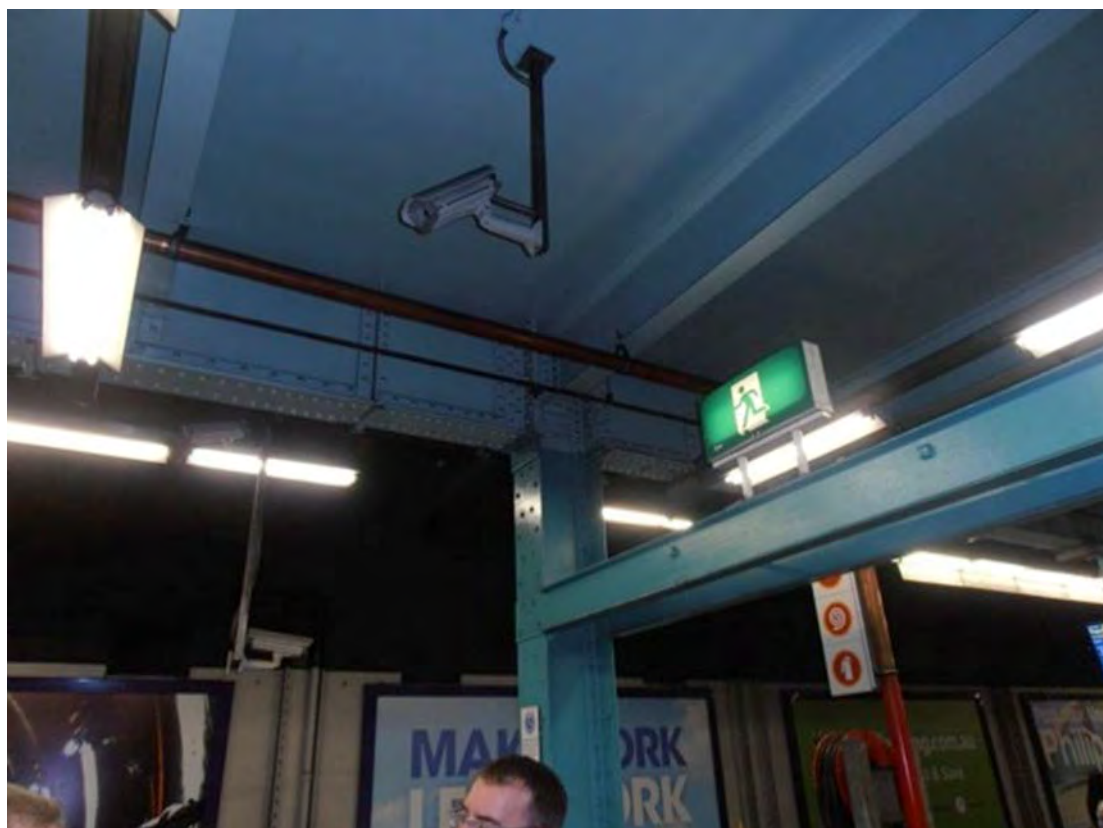


Figure 75 – Example of services to Platforms 3 and 4, which run along the ceilings and walls/columns. Redundant services are to be removed, while existing and essential services would be concealed and made accessible where required (*Futurepast 2013*).

Proposed Works

De-cluttering of platforms and removal of all non-essential features.

Assessment

The de-cluttering of the platforms and removal of all non-essential features would be confined only to modern intrusive elements and would not impact any original fabric.

This work would have a positive impact upon the station precinct as it represents a partial return to the original aesthetic qualities of the platforms, which were based upon clean, uncluttered straight lines and exposed functionality. Currently, the original clean and open aesthetic is already compromised by modern signage, lighting and security equipment, as well as advertising and miscellaneous amenities.

As such, any minimisation of unnecessary objects within the platform spaces represents an improvement.

Heritage Impact

There is no adverse heritage impact arising from this proposed item of works.



Figure 76 – View along Platform 4, showing the signage and accretions which are to be minimised by a platform wide de-cluttering (Futurepast 2013)

Proposed Works

Replacement of existing seating with newer style approved seats or rest-rails.

Assessment

The existing seating and rest facilities are modern items dating from the various upgrades of the station precinct, as recently as 2009. Modifications to these items involve the removal of non-significant functional fabric and replacement with a more modern version of the same elements.

New seating and rest-rails are generic, non-intrusive public amenity items that are already common elements to the platforms and would not affect any original fabric.

Heritage Impact

There is no adverse heritage impact arising from this proposed item of works.

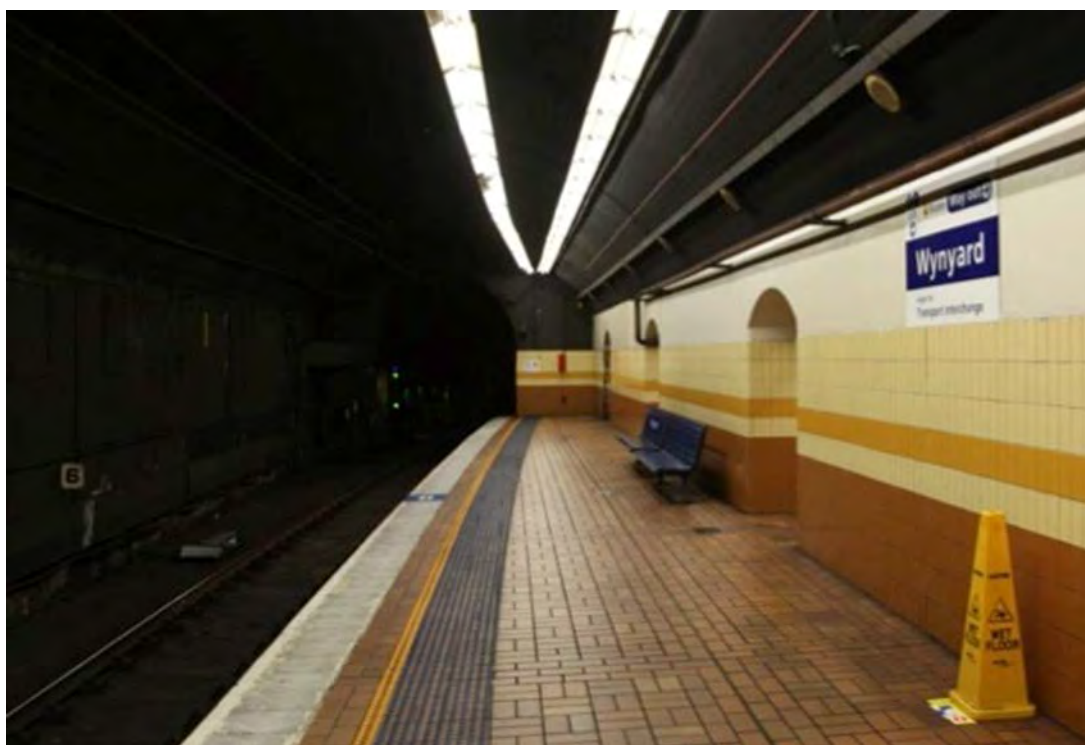


Figure 77 – Photograph of the existing platform seating at Wynyard, viewed on Platform 5 (Futurepast 2013).

Proposed Works

Platform control rooms to be reduced in size to facilitate more platform space for customers.

Assessment

The platform control rooms are non-original infill additions to under-stair spaces. The fabric to be removed is functional in nature and no longer retains its original finishes or aesthetic impact.

The provision of greater platform space is essential in light of the present overcrowding issues at peak times, which is not only uncomfortable for commuters but can become dangerous should customers venture beyond the designated safe zone at the platform edges.

Heritage Impact

The potential loss of non-original fabric would have no substantive adverse heritage impact arising from this proposed item of works.

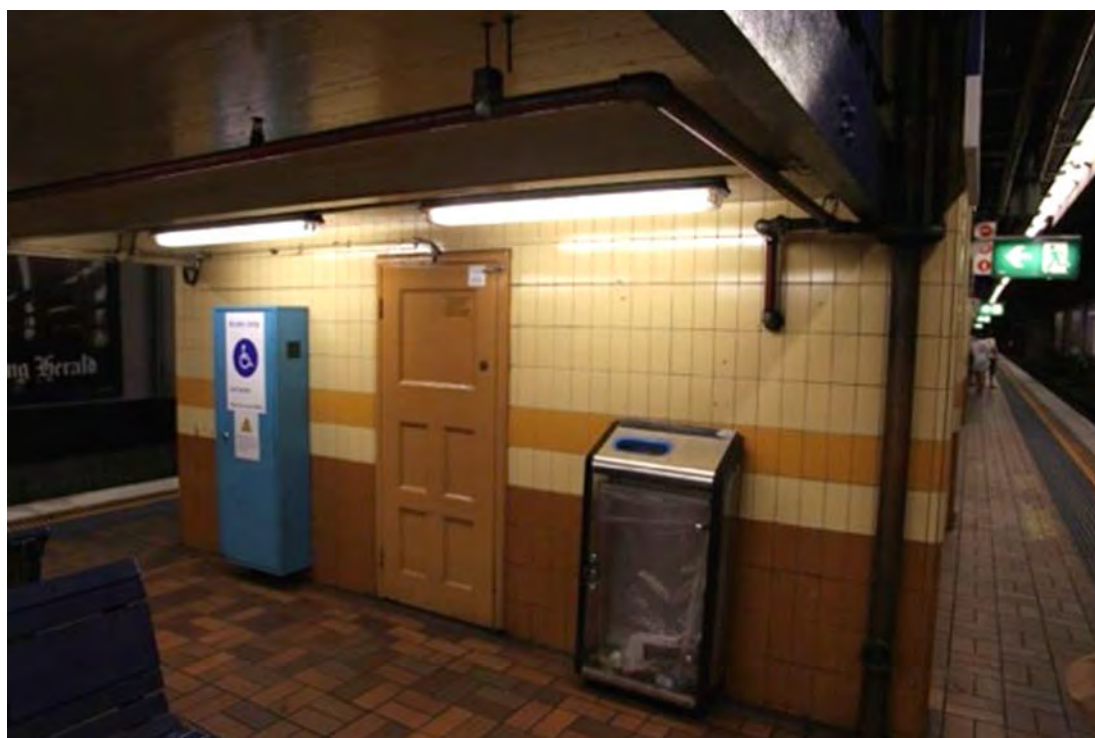


Figure 78 – Photograph of one of the station control rooms to be reduced in size to facilitate more platform space (Futurepast 2013).

Proposed Works

Redundant storerooms, control rooms and disused escalator wells on all platforms to be removed.

Assessment

A number of the redundant store rooms, control rooms, etc., are non-original infill additions below the stair access to the platforms. However, some of the rooms are reappropriation or intended usage of the existing under-stair spaces. While the removal of these rooms, if structurally feasible, would result in the removal of some original fabric, the fabric to be removed is functional in nature and no longer retains its original finishes or aesthetic impact. The unused enclosures for escalators, which were never installed, would represent a loss of original fabric, however, they are essentially empty brick shells for site infrastructure that was never built. This loss is minor.

The facilitation of greater platform spaces is essential in light of the present overcrowding issues at peak times, which is not only uncomfortable for commuters but can become dangerous should customers venture beyond the designated safe zone at the platform edges.

Heritage Impact

The removal of non-contributory and non-original fabric is acceptable. The potential loss of such fabric is outweighed by the practical and safety benefits and there would be no substantive adverse heritage impact arising from this proposed item of works.



Figure 79 – Photograph of the unused escalator pits on Platforms 5 and 6. For an example of the control etc. rooms to be impacted (*Futurepast 2013*).

Proposed Works

Doors to rooms on platforms are to be replaced for fire safety upgrades.

Assessment

Where platform rooms are to be retained, the doors may be required to be replaced to conform to fire safety specifications. While the majority of doors are modern replacements, a small number of original doors were noted upon inspection.

Though some of the doors are original, replacement of the doors to comply with safety requirements is seen as an unavoidable loss of fabric of low significance. Keeping an example of the original timber doors either in storage on site or within the Office of Rail Heritage movable heritage collection would be a desirable option to mitigate the impact of removing the doors from their original location. New doors could be replaced with ones featuring a heritage styled sheeting to replicate the appearance of panelled timber doors. Heritage styled doors based on a traditional panelled arrangement would visually communicate an historic aspect of the platforms, partially offsetting the fact that so few original features remain. Alternatively, the proposed relocation of original doors to somewhere that does not require fire safety doors is also positive from a heritage perspective.



Figure 80 – Example of one of the remnant original timber doors, which the design of the new fire compliant doors is to be based on (*Futurepast 2013*).

Heritage Impact

The heritage impact arising from this proposed item of works is acceptable, as the potential loss of fabric would be outweighed by the practical and aesthetic benefits.

It is recommended that the replacement safety doors are sheeted in a timber-panelled style to replicate the look of the original doors. This would be a desirable heritage outcome in lieu of the loss of fabric. Alternatively, the original doors should be reused in areas that do not require fire rated doors.

Proposed Works

Replacement and upgrade of lighting.

Assessment

No original light fittings remain within the station precinct. Though the style and design of the original fittings has been determined through historical and archival research (see Section 5 for details), the current scheme of works does not propose to reinstate original 1930s light fittings.

The replacement of light fittings in the platform areas is consistent with the re-equipment of light fittings throughout the Sydney metropolitan railway system. This work would lower operating costs and has indirect environmental benefits.



Figure 81 – View of the current light fittings on Platform 4. This style of fitting is common across all the platforms (Futurepast 2013).

Heritage Impact

Although reinstating original fittings would be more desirable from a heritage perspective, there is no substantive adverse heritage impact arising from this proposed item of works.

Proposed Works

Install temporary hoist during construction through floor of platforms 3 & 4 and through the roof of platforms 5 and 6.

These works would provide an access shaft through the platform ceiling to facilitate the use of a hoist for the delivery and removal of construction materials and waste. The works would be temporary during construction only and the ceiling would be reinstated upon completion of works.

Assessment

There would be no lasting impact from this work, as the hoist would be removed at the completion of the project and existing surfaces reinstated.

Heritage Impact

There is no adverse heritage impact arising from this proposed item of works.

10.2 Wynyard Station – Paid and Unpaid Concourse

Due to the interface between the paid and unpaid concourse, the heritage impact assessment for these two precincts have been combined into this section.

The gateline configuration, as described in Section 9.1.5 above and the rendered architect impressions below (figures 70 to 72), is subject to detailed design. During detailed design, the configuration may undergo refinement to ensure the final gateline configuration meets customer and operational requirements, as well as delivering a high standard of architectural design that responds to the spatial configuration of the paid and unpaid concourse.

The proposal assessed in this heritage impact assessment has been developed following careful consideration of options as outlined in Chapter 3 of the Review of Environmental Factors (AECOM, 2014). The preferred option has been developed to provide certainty of layout for the construction contractor while also enabling flexibility around future fit-out of station facilities during detailed design, noting that fit-out of retail spaces would not form part of this proposal.

Proposed Works

Changes to the existing rectangular/linear footprint of the paid and unpaid concourses consist of a curved alignment design. The design is to be created using a combination of screens that are not full height and new opal ticket gates along this curved alignment. The use of lighting, finishes and ceiling treatments will accentuate the transition between the two distinct precincts.

Assessment

This proposal impacts the historic configuration and size of the paid concourse at Wynyard Station, a footprint that originated in the 1930s but removed in the 1980s and reinstated in the 1990s.

However, increasing the floorspace within the paid concourse is necessary to accommodate pedestrian flow through these newly expanded spaces. In particular, the proposed configuration would address one of the major contributors to congestion occurring during peak periods. At present, the ticket gates are located directly adjacent from the platform stairs leading to Platforms 3 and 4 (up) and Platforms 5 and 6 (down). Congestion occurs regularly as people descending/ascending the stairs from the platforms are stalled at the ticket gates. Relocation of the ticket gates away from the platform stairs would assist the pedestrian flow through these points.

The proposed design for the unpaid and paid concourses represents a departure from the current rectilinear design, which was itself largely driven by the nature of the structural grid itself. This structural grid would be retained and the original ceiling form of the concourse would be revealed within the new paid concourse area.

The ticket gates and barriers, which separate the paid and unpaid concourses, are new elements on a curved alignment. The curved design has been developed to improve passenger movements to the north side of the concourse, as well as improve sightlines to the ticket gates themselves. A view analysis undertaken by Cox Architects (see attached) demonstrates that the curved alignment improves visibility, and therefore access to the ticket gates. This is functionally justified due to the very

substantial anticipated increase in passenger numbers following the completion of Wynyard Walk and the opening of the Barangaroo Precinct.

The establishment of the new paid concourse barrier would require the removal of all fabric from the existing ticket gates and paid concourse barriers. This is a minor loss, as the fabric and finishes of these areas is primarily 1980s or later, of indifferent quality and, in many cases, very heavily worn. The colours of the existing tiles are not original to the station.

The retail spaces, amenities and back of house office areas are late 20th century fabric with subsequent modifications and are of no particular significance. Again, most of this fabric is utilitarian and worn. There is also no known original signage in these areas.

The exception to this is the remnant section of the former Grill Room decorative plaster ceiling within the western-most shop at the north edge of the unpaid concourse. This remnant ceiling is, however, only a very small portion of the former Grill Room ceiling. The fragmentary nature of these remnant decorative finishes and the further expansion of the unpaid concourse into the former retail areas to the north, to accommodate increased station patronage, makes the reinstatement of these details impractical. However, being the last ceiling elements remaining, the retention of fabric is where possible is preferred.

Other elements of this ceiling or other related decorative finishes may exist behind bulkhead and partition walls in adjacent retail spaces. Where these finishes exist, they should be recorded and, if possible, be retained above any new ceiling or behind partition walls. This may not always be practical, however, due to the large number of services in the ceiling cavity and the intent to re-route services out of the paid concourse area to reveal the original ceiling configuration and reinstate the original lighting design.

Within the unpaid concourse, a modern ceiling would be established to allow the concealment of the considerable number of services within the area. Linear lights would run across the space, providing an element of visual direction to passengers from the various concourse entrances to the new ticket barriers. This would replace the existing metal panel suspended ceiling and fluorescent strip lighting currently within this area, which is not significant.

Other elements within the unpaid concourse, which would be removed under the proposed scheme, represent fabric related to various retail enterprises that are later additions and are of no significance. An existing fire stair along the north side of the unpaid concourse would be reconfigured with further encroachment into areas of remnant plaster ceilings, however again this fabric is highly compromised. The fire stair itself is a late 20th century addition and has no significant fabric.

The paid concourse area would reinstate much of the original design intent for the Wynyard Station concourse. Once within the modern curved ticket barriers, the original ceiling structure and grid would be revealed and refurbished, lifting the ceiling height and opening up the space. Reconstructed lantern lights would be placed within the ceiling grid, which reflect the original lighting design in the area.

The new ceiling within the unpaid concourse provides the space for the services, which are to be routed out of this area, allowing the original ceiling structure to be revealed. The existing columns will be retained. Where columns have been boxed in, this cladding will be removed. All columns will have the later original curved corners retained or reinstated where they have been removed or concealed. In addition, historic metal balusters and timber handrails are being retained to the staircases that lead to the platforms from within the paid concourse. One staircase to Platform 3/4 would be reoriented to improve passenger flows, however these historic elements would be reinstated on the staircase. The reorientation of the stair is a minor impact.

Heritage Impact

While the proposal for the paid and unpaid concourses represents a reconfiguration of the area, these spaces have been reconfigured on many occasions in the past. The proposed modern interpretation of these areas would provide a contrast between the historic and modern fabric and would facilitate the reinstatement of the historic ceiling and lighting configuration, as well as the curved cornered columns within the paid concourse and at the transition zone at the based of the Transport House escalators.

The remnant decorative fabric within the unpaid concourse is highly compromised and the proposed design does not allow for its meaningful restoration, therefore all historic finishes should be recorded before removal and, where possible, examples should be retained and concealed behind modern ceilings and walls. Reflection of the original signage designs for Wynyard Station would be advantageous for interpretation in these areas.

Within the paid concourse, the reinstatement of the historic ceiling, lighting configuration and curved cornered columns is a positive heritage impact, as is the retention of the metal balustrading. The reconfiguration of the stairway to Platform 5/6, the new staircase to Platforms 3/4 and the northern fire stair are minor impacts.

Overall, there is no substantive adverse heritage impact arising from this proposed set of works. Wherever possible, archival recordings and paint test analyses should be undertaken before any demolition of fabric, insertion of new elements, and application of new surface finishes.



Figure 82 – Artists impression of the proposed refurbishment of the Unpaid Concourse. *(Source: Wynyard Station Upgrade, Station Working Group Presentation 09 October 2014, prepared for TfNSW by Cox Richardson Architects)*



Figure 83 – Artists impression of the new refurbishment for the Paid Concourse. The existing suspended ceiling is to be removed to expose the original crossbeam ceiling. (Source: Wynyard Station Upgrade, Station Working Group Presentation 09 October 2014, prepared for TfNSW by Cox Richardson Architects)

Proposed Work

Reorientation of the stairs from the paid concourse area to 5/6 to better facilitate passenger access, requiring the partial demolition of the stair.

Assessment

The partial demolition of stairs to Platforms 5 and 6, would facilitate the reorientation of the stairs to improve pedestrian flows to/from the platforms. While the works would remove some original fabric, much of the material dates to various station upgrade works in the 1980s with other recent modifications. The fabric to be removed is functional in nature and no longer retains its original finishes or aesthetic impact, except for the balusters and handrails that will be reinstalled on a re-oriented stair.

Heritage Impact

The loss of non-contributory original fabric would be outweighed by the practical benefits and, as such, the heritage impact arising from this proposed item of works is minor and acceptable. Metal balustrades and timber handrails are proposed to be reinstated within these areas, to match the historic fabric.

General comment

Prior to the commencement of this work, photographs and annotated plans indicating the existing alignments and layouts should be prepared for future

reference. Where any historic finishes or structural elements are exposed during works, these finishes should be recorded, sampled and, if possible, have a sample retained in situ for potential use in future upgrade works.

Proposed Work

Insertion of a new stair at the south end of platforms 3/4 to better facilitate passenger access.

Assessment

New stairs are proposed on the southern end of Platforms 3/4 to improve pedestrian flows to/from the platforms and the paid concourse. While the works would remove some original fabric by inserting an access point into the platform floor, the fabric to be removed is functional in nature and no longer retains most of its original finishes or aesthetic impact.

Heritage Impact

The loss of non-contributory original fabric would be outweighed by the practical benefits and, as such, the heritage impact arising from this proposed item of works is minor and acceptable.

General comment

Prior to the commencement of this work, photographs and annotated plans indicating the existing alignments and layouts should be prepared for future reference. Where any historic finishes or structural elements are exposed during works, these finishes should be recorded, sampled and, if possible, have a sample retained in situ for potential use in future upgrade works. Where possible, new fabric to be inserted should be in a material, style, look and finish that compliments the existing original platform stairs.

10.3 Wynyard Station – Unpaid Concourse (former Grill Room Ceiling)

The proposed work to the unpaid concourse and the heritage impact to this area have been addressed in the section 10.2 above, which has combined the heritage assessment for both the paid and unpaid concourse.

The heritage impact assessment for proposed works in the western unpaid concourse of Wynyard Station, also known as Basement Level 4 of Transport House, is addressed in section 10.4.

Special consideration has been made with regards to the original 1930s Art Deco ceiling that is present in a retail space within the eastern concourse. This ceiling is the only remnant of the original Wynyard Railway Refreshment Rooms (the former Grill Room) and would be affected by the proposal to reconfigure the Station facilities in the northern and eastern unpaid concourse. Additional assessment of this work is included below.

FORMER GRILL ROOM CEILING

Proposed Work

Widening of the pedestrian corridor to the north of the paid concourse with attendant removal of parts of the existing retail and back of house spaces. This work would remove areas of original ceiling in retail spaces within the present northwest corner of the eastern unpaid concourse.

Assessment

This work involves changing the existing footprint of the retail areas at the northern and southern ends of the eastern unpaid concourse, to accommodate the expanded width of the pedestrian access north of the paid concourse. At present, the northern pedestrian corridor is not linear, creating unnecessary alcoves, corners and doglegs. The purpose of changing the footprint is to allow for improved pedestrian sightlines and thoroughfares throughout the unpaid concourse. This would be necessary due to both the expanded pedestrian numbers and the enclosure of the current southern pedestrian corridor within the reconfigured paid concourse.

At present, the ceiling spaces in some of the retail spaces in the northern end of the concourse contain original fabric relating to the 1930s construction of Wynyard Station, as part of the former Wynyard Railway Refreshment Rooms, also known as the Grill Room. Of particular note is the ceiling of the retail space in the northwest corner. The ceiling, cornices, beam casings and duct casings retain the original, elaborately decorative Art Deco style. Furthermore, the stylistic finishes clearly relate to the style and finishes of Transport House, forming a sense of aesthetic continuity between the two distinct spaces. Historic photographs of the original fitout of the Railway Refreshment Rooms show that the ceiling in the northwest corner retail space is quite intact. However, plans of Wynyard Station have demonstrated that these rooms are already dramatically modified in terms of space/footprint by previous works in the station. The current space is roughly only two-thirds of the original size of the overall Railway Refreshment Rooms. The proposal would effectively halve the already diminished existing space. There is also the potential for original wall finishes to be intact behind the current modern shop fittings. More significant fabric, such as the façade of the former Grill Room, has long since been removed during previous reconfigurations of this area. What remains is a remnant only of a much grander design.

The work proposes to truncate the front portion of the retail space, which involves the removal of a portion of this original ceiling. Scope may exist for retaining sections of the remaining ceiling fabric within the new ceiling proposed for this area.



Figure 84 – Retail spaces in the northern unpaid concourse with areas of original ceiling, moulded fibrous plaster (Image source: Futurepast 22/08/2014)



Figure 85 – The original moulded ceiling in a retail space in the northern unpaid concourse (Image Source: Futurepast 22/08/2014)

Heritage Impact

The potential loss of original ceiling fabric in one area of the station, considered in light of the intent to improve the commuter and pedestrian thoroughfare spaces, is outweighed by the practical benefits and as such, the heritage impact arising from this proposed item of works is minor and acceptable.

However, an archival recording of these areas should be undertaken prior to removal works. Photographs and annotated plans indicating the existing alignments and layouts should be prepared for future reference. A paint test analysis should also be undertaken here as part of this pre-works recording exercise.

For the ceiling in the northwest corner retail space specifically, a Reflected Ceiling Plan should be prepared showing the layout and sections of the decorative profiles present. Where possible, areas of the ceiling that can be retained and covered over with suspended ceilings or casings should be identified and protected.

Where any historic finishes or structural elements are exposed during works, these finishes should be recorded, sampled and, wherever possible, a sample retained in situ for potential use in future upgrade works.



Figure 86 – The original moulded ceiling in the northern unpaid concourse, once a part of the Refreshment Room (Image Source: Futurepast 22/08/2014)

10.4 Transport House

While most of the impacts in this precinct are located in the basement levels of Transport House, the assessment includes impacts to each of the basement levels, structured from the York Street Foyer down through the basement levels.

YORK STREET FOYER

Proposed Work

General de-cluttering and refurbishment of the York Street Foyer, including re-tiling, new paint finishes, improved signage and wayfinding. The existing suspended ceiling would be removed and the existing cross-beam ceiling design would be exposed and highlighted with new Art Deco style ceiling box lights.

Assessment

The York Street Foyer is to be refurbished to reinstate elements of the Art Deco appearance of the Foyer. This work would involve the removal of existing wall, floor and column finishes and replacement with new, modern finishes. The existing suspended ceiling is to be removed to expose the existing cross-beamed ceiling. New box lights would be installed that complement the Art Deco style of the refurbishment.

Heritage Impact

Much of the fabric in the York Street Foyer is modern and neutral or intrusive to Transport House and the removal of the existing wall, ceiling and floor finishes would have a negligible heritage impact. Revealing the remnant crossbeam ceiling is a positive impact, as it would reveal the original fabric and Art Deco style of the Foyer. The new box ceiling lights would also reflect the Art Deco style, complementing the building façade. The new wall and floor finishes are consistent with the new treatments proposed for the unpaid concourse below, which would strengthen the visual relationship between Transport House and Wynyard Station. Other historic finishes may be revealed during the removal of non-significant fabric.

Prior to the commencement of this work, photographs and annotated plans indicating the existing alignments and layouts should be prepared for future reference. Where any historic finishes or structural elements are exposed during works, these finishes should be recorded, sampled and, if possible, have a sample retained in situ for potential use in future upgrade works.

New claddings to the columns in the foyer should aim to be complementary in appearance, colour and style/profile to the existing stone cladding on columns at the York Street boundary. The green wall tiles along the western back-of-house corridor should be retained and conserved.

Wherever possible, archival recordings and paint test analyses should be undertaken before any demolition of fabric, insertion of new elements, and application of new surface finishes.

Consultation and advice from the Sydney Trains heritage specialist should be sought for opportunities on retaining, conserving or restoring heritage styles and features in this area.

Overall, this set of works would have a positive heritage impact.



Figure 87 – Artists impression of the new refurbishment for the York Street Foyer, with new floor, wall and column cladding. The original cross-beam ceiling is to be exposed, with new art-deco style box lights to be positioned in between the cross-beams. (Source: Wynyard Station Upgrade, Station Working Group Presentation 09 October 2014, prepared for TfNSW by Cox Richardson Architects)



Figure 88 – Artists impression of the new refurbishment for the York Street Foyer, with new floor, wall and column cladding. The original crossbeam ceiling is to be exposed, with new art-deco style box lights to be positioned in between the cross-beams. (Source: Wynyard Station Upgrade, Station Working Group Presentation 09 October 2014, prepared for TfNSW by Cox Richardson Architects)



Figure 89 – The York Street entrance at the top of the Escalators facing east (Image source: Futurepast 22/08/2014).

Proposed Work

Reconfiguration of rooms within the western back of house areas.

Assessment

This work involves making changes to the configuration of areas within the back of house areas and egress areas on the western side of the York Street escalators. This would be done by demolishing some existing walls. Existing original wall tiles and terrazzo finishes within the western access corridor would be protected and conserved.

Heritage Impact

The impact of this work is considered minor, as the demolition and reconfiguration work would have very little effect on the heritage significance of the site or the place. The amount of demolition is small and is to take place in an area that is not seen by the public. The works would increase the space in these areas by moving the doorways of the western back of house hallway and the western fire stair outwards, which would improve these facilities. The retention and conservation of the wall and floor finishes within the western access corridor would have a positive heritage impact.

While some of the affected fabric is original to the space, it is considered non-contributory to the overall significance of the building. The removal of non-contributory original fabric is a minor impact, which is outweighed by the practical benefits of the work, and as such, the heritage impact arising from this proposed item of works is minor and acceptable.

Where any historic finishes or structural elements are exposed during works, these finishes should be recorded, sampled and, if possible, have a sample retained in situ for potential use in future upgrade works.

Proposed Work

Reinstating the operational capacity of York Lane goods lift from the York Street Lobby.

Assessment

This proposed work involves removal of the brick enclosure wall behind the York Lane goods lift roller door in order to reopen the lift for operation. It will also involve installation of new lift mechanisms and lift cage to allow its operation.

The work may have some physical impacts to original fabric, namely wall fabric. However, it will only impact on fabric within the internal space of a goods lift shaft, which is considered non-contributory to the significance of the building overall.

Heritage Impact

The removal of non-contributory original fabric is a minor impact, which is outweighed by the practical benefits of the work, and as such, the heritage impact arising from this proposed item of works is minor and acceptable.

The loss of the small areas of original fabric represents a minor acceptable impact.

Where possible, the reuse of existing penetrations is preferable.

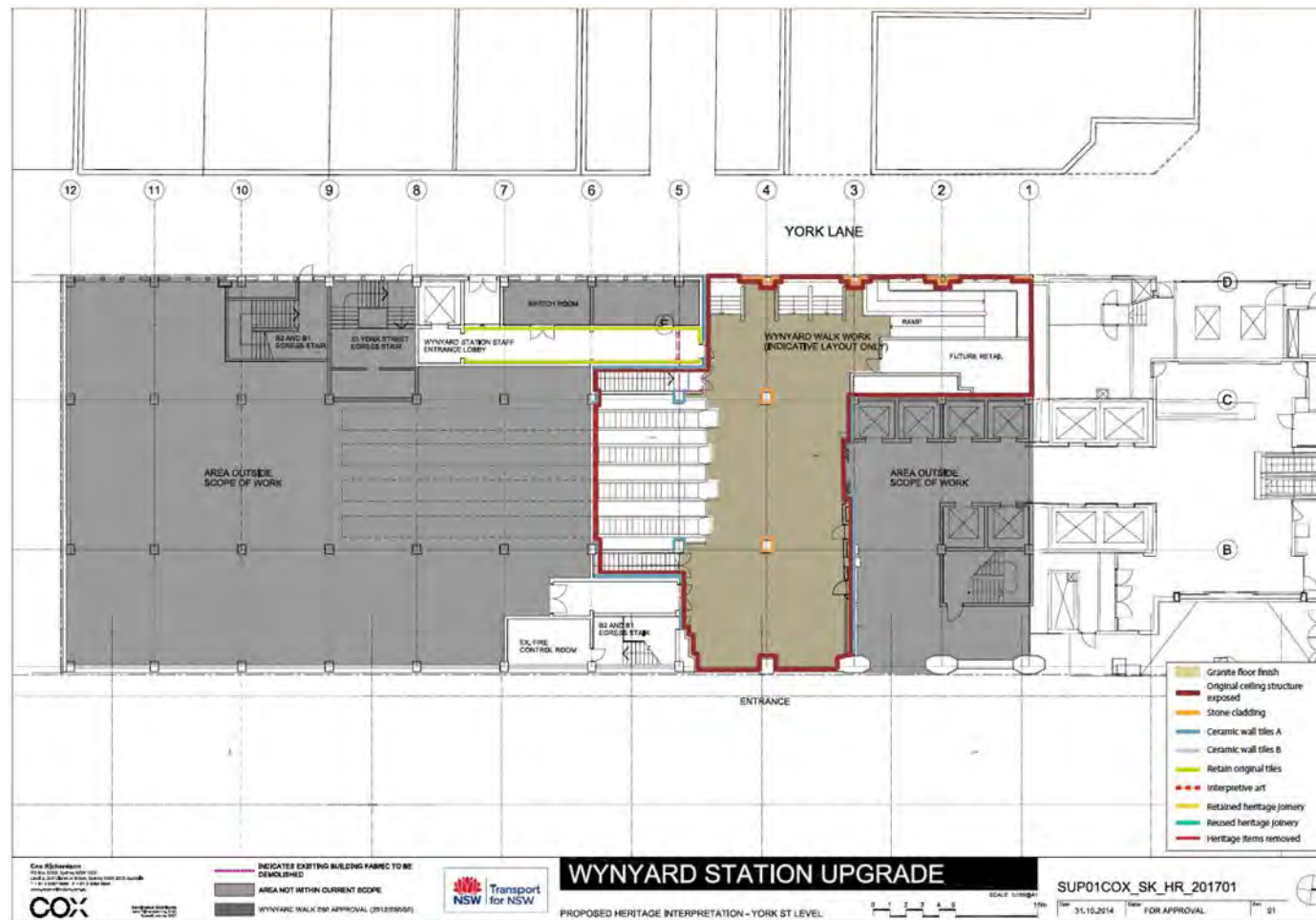


Figure 90 – Proposed works to the York Street Foyer, with the intact heritage fabric to be retained indicated in colour. (Source: Wynyard Station Upgrade, Drawing Issue For Approval, prepared by Cox Richardson, dated 31/10/14).

BASEMENT LEVEL 1

Proposed Work

- *Demolition of existing bulkhead above escalators and installation of a viewing window to the northern wall of the back of house office directly above the escalators.*
- *Refurbishment of the escalators including new tiles, painting and sanding/polishing of the timber.*
- *Installation, use and decommissioning of an elevated works platform to support refurbishment works.*

Assessment

An original bulkhead that is positioned between columns above the escalators is to be removed. In lieu of this bulkhead, the northern wall of the office space (directly behind the subject bulkhead) and above the escalators is to be demolished and replaced with a new glazed viewing panel with one-way glass to look out over the escalators. In addition, other upgrade works such as painting would be carried out in this area above the escalators.

This work would require a temporary works platform to be constructed above the York Street escalators for construction (such as painting) to be safely undertaken above the escalators. A scaffold that straddles the escalators would be erected to facilitate the installation and removal of the works platform²⁷.

Heritage Impact

The bulkhead is a utilitarian building feature and is not contributory to the heritage values of the building. Removal of the bulkhead would allow for the office space behind it to have an external window, improving the conditions and facility of the room, which is currently fully enclosed.

The removal of such low significance, non-contributory fabric is a minor impact, which is outweighed by the practical benefits of the work, and as such, the heritage impact arising from this proposed item of works is minor and acceptable. It is proposed that this new bulkhead, and the others within the escalator roof space, would be used for interpretive panels (subject to further detail).

The scaffolding above the escalators would have a minor visual impact while the work platform is being erected, however it would be removed once the works platform is in place. Affixing into the reinforced concrete fabric of the side walls would have a minor impact, which can be repaired. Protective measures should be developed for the historic fabric of the escalators and there should be no penetrations to or fixing into the escalators or their surrounds.

²⁷ Caitlin Bennett, AECOM Pers. Comms. 10/11/14.

Where any historic finishes or structural elements are exposed during works, these finishes should be recorded, sampled and, if possible, have a sample retained in situ for potential use in future upgrade works.



Figure 91 – Cross section of Transport House looking east, showing each of the basement levels down to Concourse Level (B4). The red line shows where the existing non-original bulkheads would be removed in front of a new glazed one-way viewing window, with opportunity to use it as a heritage interpretation panel. (Source: Pers. Comms. Caitlin Bennett 10/11/14)

Proposed Work

Reconfiguration of rooms in back of house areas.

Assessment

This proposed work involves the reconfiguration of back of house spaces through the demolition of existing walls or the enclosure of existing doorways and access points. Reconfiguration of the back of house rooms would result in a more efficient space and improve the operational needs for Sydney Trains staff.

While the works would remove original fabric, the fabric is functional in nature and is a low-contributory feature to Transport House. One area of historic wall tiles, likely relating to a former kitchenette, is proposed for removal. Historic timber doors and architraves would be retained and reused within the reconfigured space, although not necessarily in their original positions.

Heritage Impact

The removal of non-contributory original fabric is a minor impact, which is outweighed by the practical benefits of the work, and as such, the heritage impact arising from this proposed item of works is minor and acceptable.

The loss of the small section of wall tiles represents a minor acceptable impact.

The reuse of historic joinery, architraves and doors is a positive heritage impact.

Prior to the commencement of this work, photographs and annotated plans indicating the existing alignments and layouts should be prepared for future

reference. Where any historic finishes or structural elements are exposed during works, these finishes should be recorded, sampled and, if possible, have a sample retained in situ for potential use in future upgrade works.

Proposed Work

Refurbishment for back of house purposes, including refurbishment of services and amenities as required.

Assessment

This proposed work involves the insertion of new and updated services and amenities which seek to improve facilities, uses and operations in these spaces.

The work may have some physical impacts to original fabric, namely walls, floors and ceilings.

Heritage Impact

The removal of non-contributory original fabric is a minor impact that is outweighed by the practical benefits of the work and as such, the heritage impact arising from this proposed item of works is minor and acceptable.

The loss of the small areas of original fabric represents a minor acceptable impact.

Where possible, the reuse of existing penetrations is preferable.

Prior to the commencement of this work, photographs and annotated plans indicating the existing alignments and layouts should be prepared for future reference. Where any historic finishes or structural elements are exposed during works, these finishes should be recorded, sampled and, if possible, have a sample retained in situ for potential use in future upgrade works.

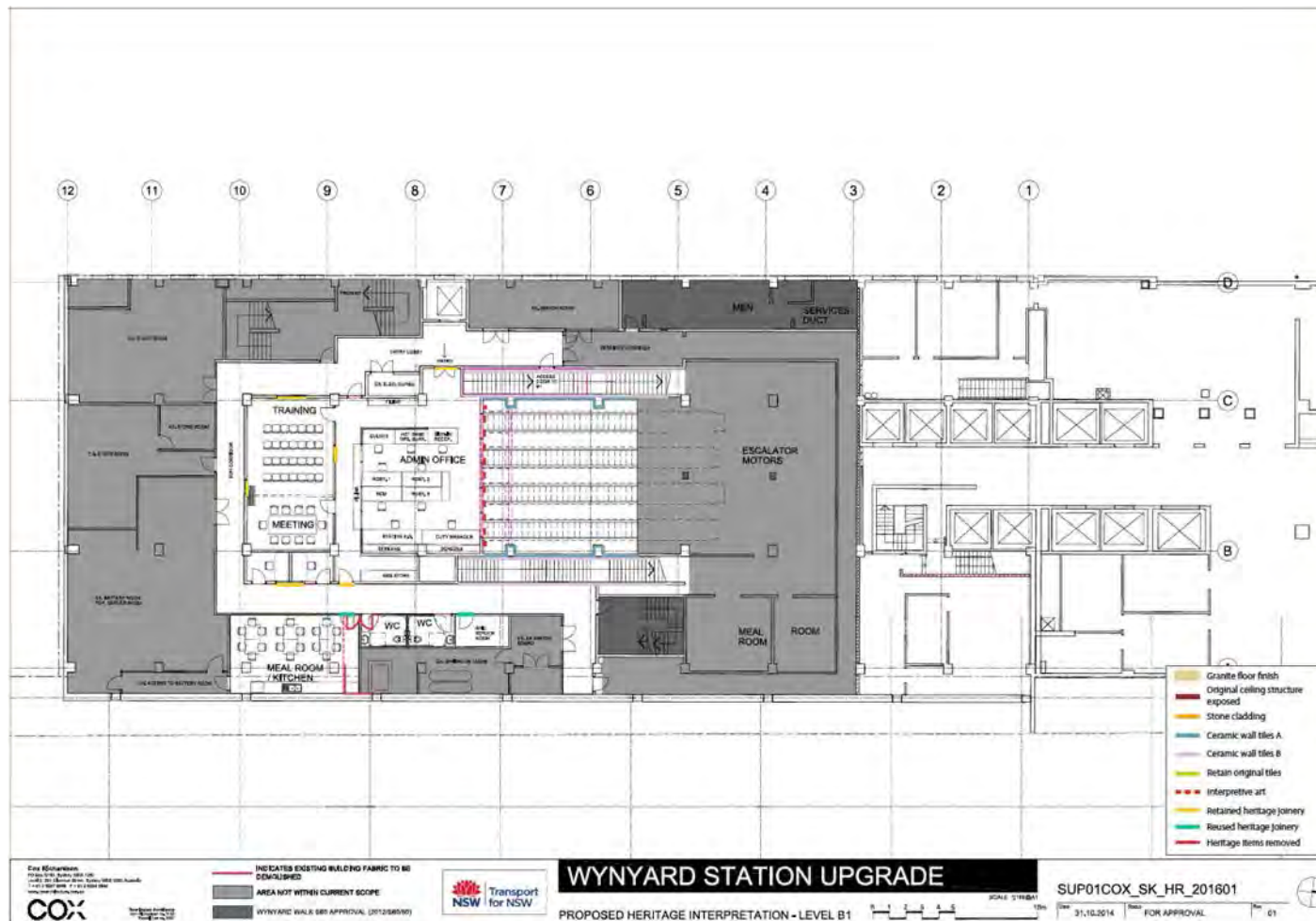


Figure 92 - Proposed works to the Basement Level 1, with the intact heritage fabric to be retained indicated in colour. (Source: Wynyard Station Upgrade, Drawing Issue For Approval, prepared by Cox Richardson, dated 31/10/14).

BASEMENT LEVEL 2

Proposed Work

Truncation of eastern fire stair between B2 and B4.

Assessment

At present, the existing fire stair on the eastern side of the escalators provides direct access between the western unpaid concourse (Basement Level 4) and the York Street Foyer. This work proposes to demolish part of the stair (at Basement Level 4 and Basement level 3) (this demolition work is assessed separately below), resulting in the eastern stair commencing from Basement Level 2 and providing direct access to the York Street Foyer. There would be remediation work to the stair fabric and structure to allow for this partial truncation at this point.

Heritage Impact

The removal of the eastern fire stair at Basement Level 3 and 4 would increase the available space and consequently improve facilities for rail staff and pedestrian flow in this area. The new stair between the western Concourse (B3) and York Street foyer is now planned to be an egress stair as well as serve for back-of-house access.

The eastern fire stair contains original fabric, the original wall tiling with blue geometric decorative banding, as well as original balustrading. The truncation of the stair at Basement Level 2 would involve the removal of some of this fabric.

While the wall cladding has remnant decorative qualities and is a relic of the historic aesthetic of Wynyard Station, it has been impacted by unsympathetic repairs and the removal of this fabric is a minor impact. Conservation of the fabric is outweighed by the practical benefits of the work, and as such, the heritage impact arising from this proposed item of works is minor and acceptable.

Prior to the commencement of this work, photographs and annotated plans indicating the existing alignments and layouts should be prepared for future reference.

Where possible, original wall tiles to be removed should be removed carefully to avoid damage and salvaged as surplus materials for any future restorations. The truncation of the original balustrades must be done in a sympathetic manner.

Proposed Work

Refurbishment of back of house areas, including services and amenities as required.

Assessment

This proposed work involves the insertion of new and updated services and amenities which seek to improve facilities, uses and operations in these spaces.

The work may have some physical impacts to original fabric, namely walls, floors and ceilings.

Heritage Impact

The removal of non-contributory original fabric is a minor impact, which is outweighed by the practical benefits of the work, and as such, the heritage impact arising from this proposed item of works is minor and acceptable.

The loss of the small areas of original fabric represents a minor acceptable impact.

Where possible, the reuse of existing penetrations is preferable.

Prior to the commencement of this work, photographs and annotated plans indicating the existing alignments and layouts should be prepared for future reference. Where any historic finishes or structural elements are exposed during works, these finishes should be recorded, sampled and, if possible, have a sample retained in situ for potential use in future upgrade works.

Wherever possible, archival recordings and paint test analyses should be undertaken before any demolition of fabric, insertion of new elements, and application of new surface finishes.



BASEMENT LEVEL 3

Proposed Work

Reconstruction of the western fire stair adjacent the escalators, from Basement Level 3 to Basement Level 1.

Assessment

The York Street fire stair off the western side of the York Street escalators is proposed to be demolished and reconstructed from its current location B3 location, to B1. A new BCA compliant stair would be constructed in the same location but at a lower level to provide fire egress to levels B1 and B2, as well as a new door to B1. The stair would continue on to the York Street lobby in its current location.

This area, while original to the space, was used as an egress route from the former concourse mezzanine directly to the York Street lobby with no connections to other levels in Transport House. It was refitted to become an emergency egress route at a later stage. It does retain a small amount of decorative tiling on the walls of the stairwell. However, it is impacted by unsympathetic repairs, is not remarkable and is considered non-contributory to the overall significance of the building.

Heritage Impact

While the fabric is original to the space, it is considered non-contributory to the overall significance of the building. The removal of this fabric is a minor impact, which is outweighed by the practical benefits of the work, and as such, the heritage impact arising from this proposed item of works is minor and acceptable.

Prior to the commencement of this work, photographs and annotated plans indicating the existing alignments and layouts should be prepared for future reference. Where any historic finishes or structural elements are exposed during works, these finishes should be recorded, sampled and, if possible, have a sample retained in situ for potential use in future upgrade works.

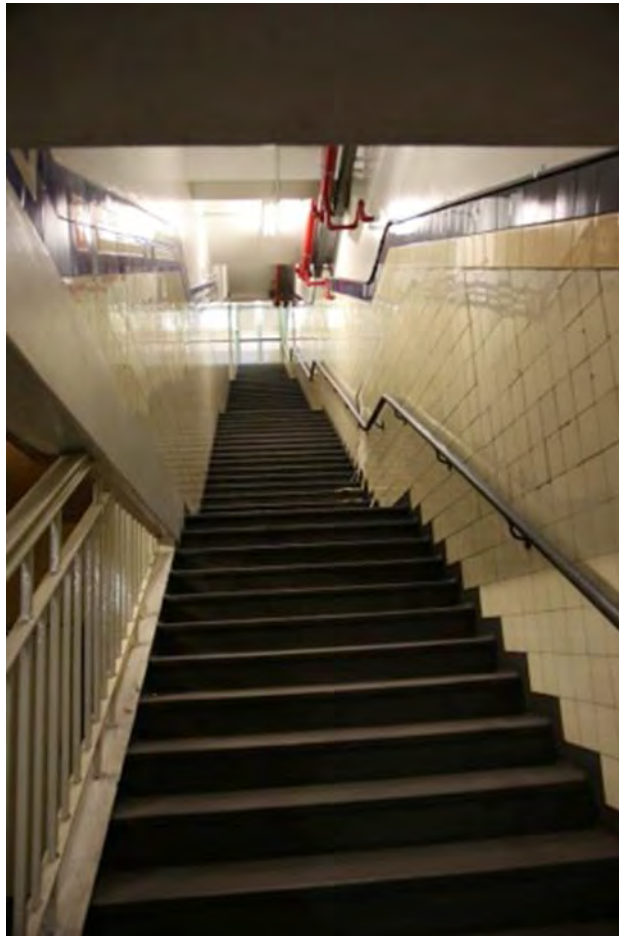


Figure 94 – The western fire stair to the right of the York Street Escalators. The cream tiling is modern refurbishment, however, the blue tiles are original features (Image Source: Futurepast 22/08/2014)

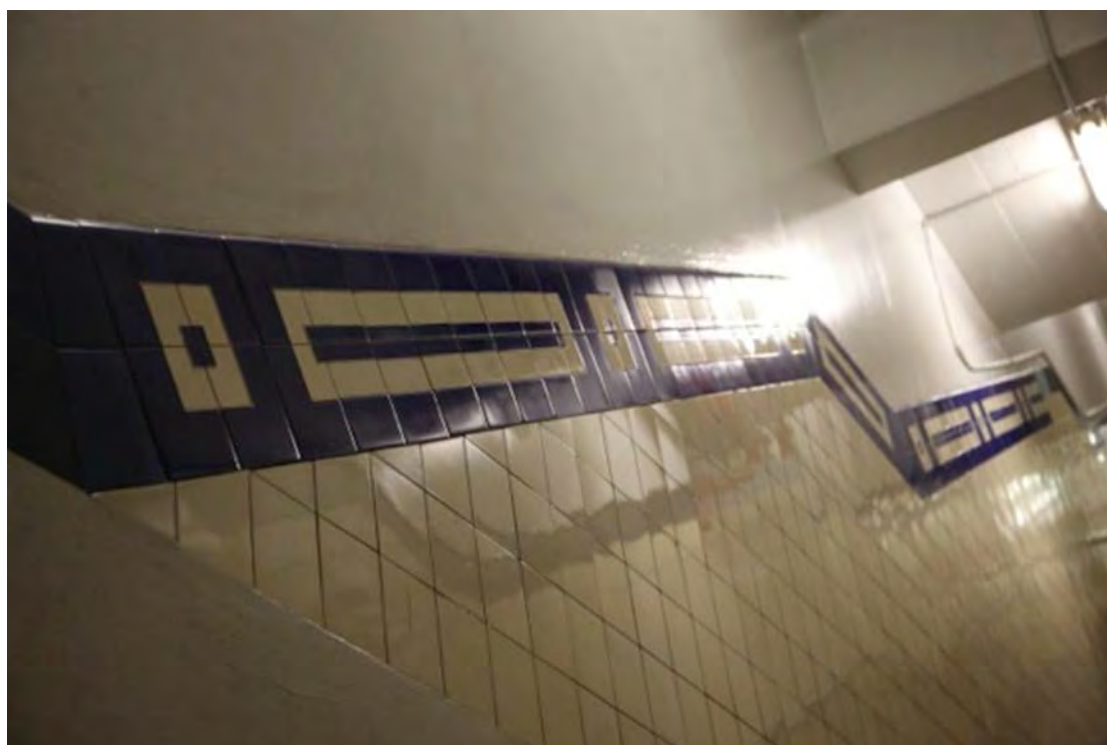


Figure 95 – The original blue detailed tiling on the western fire stair (Image source: Futurepast 22/08/2014)

Proposed work

Demolition of eastern fire stair from concourse level (B4) through to Basement Level 3.

Assessment

The York Street fire stair off the eastern side of the York Street escalators is proposed to be demolished from its current location at B4 through to B3, to commence at B2. This would open up the storage spaces underneath and around the northern side of the escalators.

This area, while original to the space, was only ever used as an emergency egress route. It does retain a small amount of decorative tiling on the walls of the stairwell; however, this tiling has also been impacted by mismatched patching, is not remarkable and is considered non-contributory to the overall significance of the building.

Heritage Impact

While the wall cladding has remnant decorative qualities and is a relic of the historic aesthetic of Wynyard Station, the removal of this fabric is a minor impact, which is outweighed by the practical benefits of the work, and as such, the heritage impact arising from this proposed item of works is minor and acceptable.

Where possible, original wall tiles should be retained and protected or should be removed carefully to avoid damage and salvaged as surplus materials for any future

restorations. The truncation of the original balustrades must be done in a sympathetic manner.

Prior to the commencement of this work, photographs and annotated plans indicating the existing alignments and layouts should be prepared for future reference. Where any historic finishes or structural elements are exposed during works, these finishes should be recorded, sampled and, if possible, have a sample retained in situ for potential use in future upgrade works.

Wherever possible, archival recordings and paint test analyses should be undertaken before any demolition of fabric, insertion of new elements, and application of new surface finishes.

Proposed Work

Refurbishment of back of house areas, including services and amenities as required.

Assessment

This proposed work involves the insertion of new and updated services and amenities which seek to improve facilities, uses and operations in these spaces.

The work may have some physical impacts to original fabric, namely walls, floors and ceilings.

Heritage Impact

The removal of non-contributory original fabric is a minor impact, which is outweighed by the practical benefits of the work, and as such, the heritage impact arising from this proposed item of works is minor and acceptable.

The loss of the small areas of original fabric represents a minor acceptable impact.

Where possible, the reuse of existing penetrations is preferable.

Prior to the commencement of this work, photographs and annotated plans indicating the existing alignments and layouts should be prepared for future reference. Where any historic finishes or structural elements are exposed during works, these finishes should be recorded, sampled and, if possible, have a sample retained in situ for potential use in future upgrade works.



Figure 96 – The eastern fire stair within the western concourse. This stair is to be removed from B4 and B3 to commence at B2 (Image source: Futurepast 22/08/2014)

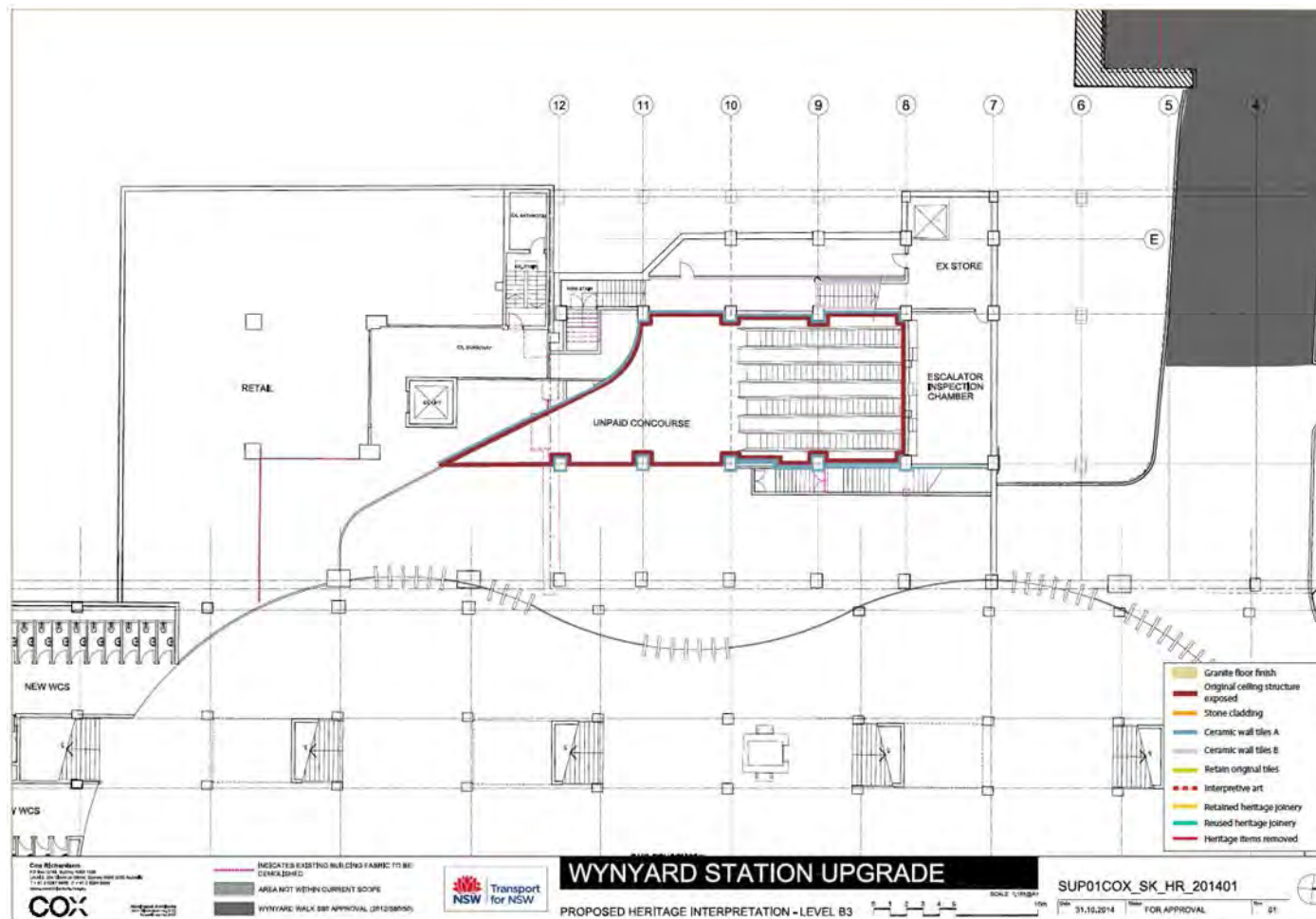


Figure 97 - Proposed works to the Basement Level 3, with the intact heritage fabric to be retained indicated in colour. (Source: Wynyard Station Upgrade, Drawing Issue For Approval, prepared by Cox Richardson, dated 31/10/14).

CONCOURSE LEVEL (B4)

Comments on this area should be read in conjunction with the discussion of the unpaid concourse area above, as the B4 area is a de facto part of the unpaid concourse.

It should also be noted that the Concourse Bar area to the south is outside of the State Heritage Register curtilage for Transport House.

Proposed Work

Enlarging the western Unpaid Concourse through the demolition of the Concourse Bar, reconstruction and reconfiguration of the space, use of the space for station facilities (such as back of house or additional retail space).

Assessment

At present, the western unpaid concourse, which is much smaller in size than the eastern Unpaid concourse, consists of the Concourse Bar, the York Street escalators, small irregular-shaped retail outlets and various back of house areas (accessed from underneath the escalators). The western unpaid concourse is also known as the Basement Level 4 of Transport House.

The proposal involves changing the footprint and configuration of the space to allow for station facilities (areas designated for back of house area or extra retail spaces). In changing the footprint of this area, it would increase the space within the western concourse for commuter and pedestrian circulation.

This proposal impacts the original configuration and size of the western concourse, a footprint that has been retained since the 1930s. However, the 1930s configuration was never intended to deal with the level of patronage Wynyard experiences now, let alone what is projected in the future once Barangaroo is fully occupied. Increasing the floorspace is necessary to cater for the projected patronage and would allow for better pedestrian flow through these newly expanded spaces. In particular, the proposed configuration would address one of the major contributors to congestion occurring during peak hours at the base of the York Street escalators.

Fabric that relates to the basement levels of Transport House consists of undecorated, rendered concrete walls, and modern floor and wall tiles. While the fabric is original to the space, it is considered non-contributory to the overall significance of the building.

Heritage Impact

While the fabric is original to the space, it is considered non-contributory to the overall significance of the building. The removal of this fabric is a minor impact, which is outweighed by the practical benefits of the work, and as such, the heritage impact arising from this proposed item of works is minor and acceptable.

Wherever possible, archival recordings and paint test analyses should be undertaken before any demolition of fabric, insertion of new elements, and application of new surface finishes.



Figure 98 – Retail spaces in the western concourse to be demolished (Image source: Futurepast 22/08/2014)



Figure 99 – The entrance to the Concourse Bar, facing south from the western concourse (Image source: Futurepast 22/08/2014)

Proposed Work

Demolition of stair at the southwest corner of the western concourse, to be replaced with a new compliant stairs with access to B3.

Assessment

The existing stair commencing at concourse level (B4) and continuing up to the service corridor within B3 is not compliant with the Building Code of Australia (BCA). It is proposed to demolish this stair and replace it to ensure compliance with the BCA and to provide better access to back-of-house areas. The original metal balustrade is to be salvaged and reused in the new stair.

Heritage Impact

While the fabric is original to the space, it is a minor element with respect to the overall significance of the building. The removal of this fabric is a minor impact, which is outweighed by the practical benefits of the work, and as such, the heritage impact arising from this proposed item of works is minor and acceptable.

The retention and reuse of balustrades from the original staircase is a positive mitigation of the work.

Where any historic finishes or structural elements are exposed during works, these finishes should be recorded, sampled and, if possible, have a sample retained in situ for potential use in future upgrade works.

Proposed Work

Removal of suspended ceiling at the base of the escalators in order to expose a section of original beams.

Assessment

The proposal involves the removal of a section of suspended ceiling in order to expose an area of the original ceiling beams. The removal of the suspended ceiling eliminates non-contributory fabric with no significance to the heritage value of the building, while exposing original fabric of the Station ceiling.

Heritage Impact

This element of the proposed works would have a positive impact upon the heritage values of Wynyard Station.

Proposed Work

Refurbishment of back of house areas, including services and amenities as required.

Assessment

This proposed work involves the insertion of new and updated services and amenities which seek to improve facilities, uses and operations in these spaces.

The work may have some physical impacts to original fabric, namely walls, floors and ceilings.

Heritage Impact

The removal of non-contributory original fabric is a minor impact, which is outweighed by the practical benefits of the work, and as such, the heritage impact arising from this proposed item of works is minor and acceptable.

The loss of the small areas of original fabric represents a minor acceptable impact.

Where possible, the reuse of existing penetrations is preferable.

Prior to the commencement of this work, photographs and annotated plans indicating the existing alignments and layouts should be prepared for future reference. Where any historic finishes or structural elements are exposed during works, these finishes should be recorded, sampled and, if possible, have a sample retained in situ for potential use in future upgrade works.

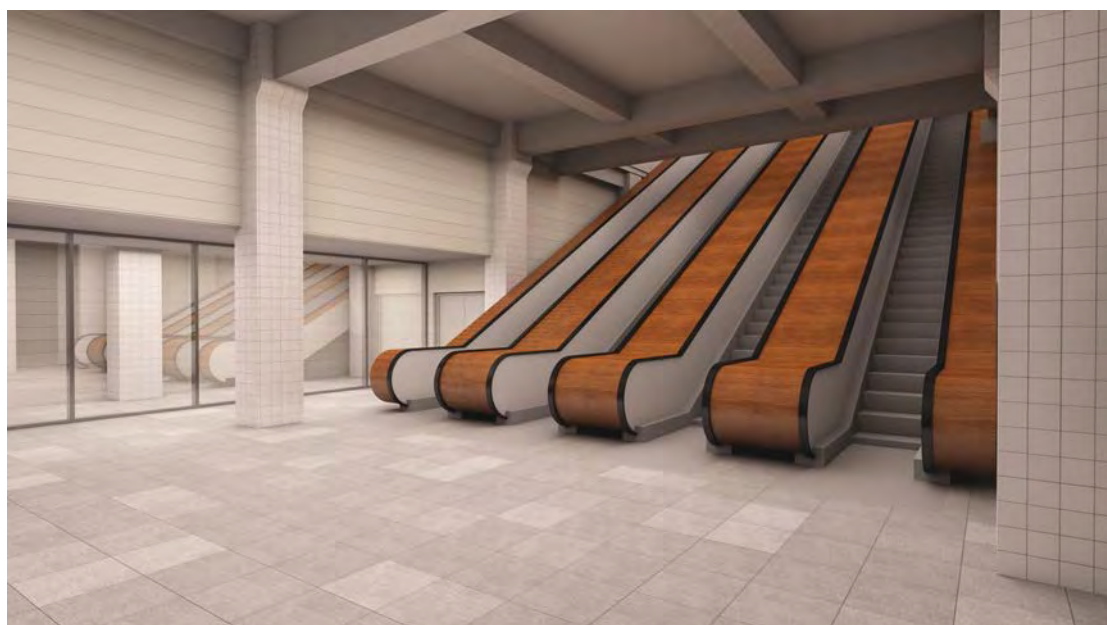


Figure 100 - Artists impression of the new refurbishment for the Concourse Level (B4), with new floor, wall and column cladding juxtaposed against the timber escalators (to be retained and restored), as well as the newly exposed original cross-beam ceiling. (Source: Wynyard Station Upgrade, Station Working Group Presentation 09 October 2014, prepared for TfNSW by Cox Richardson Architects).

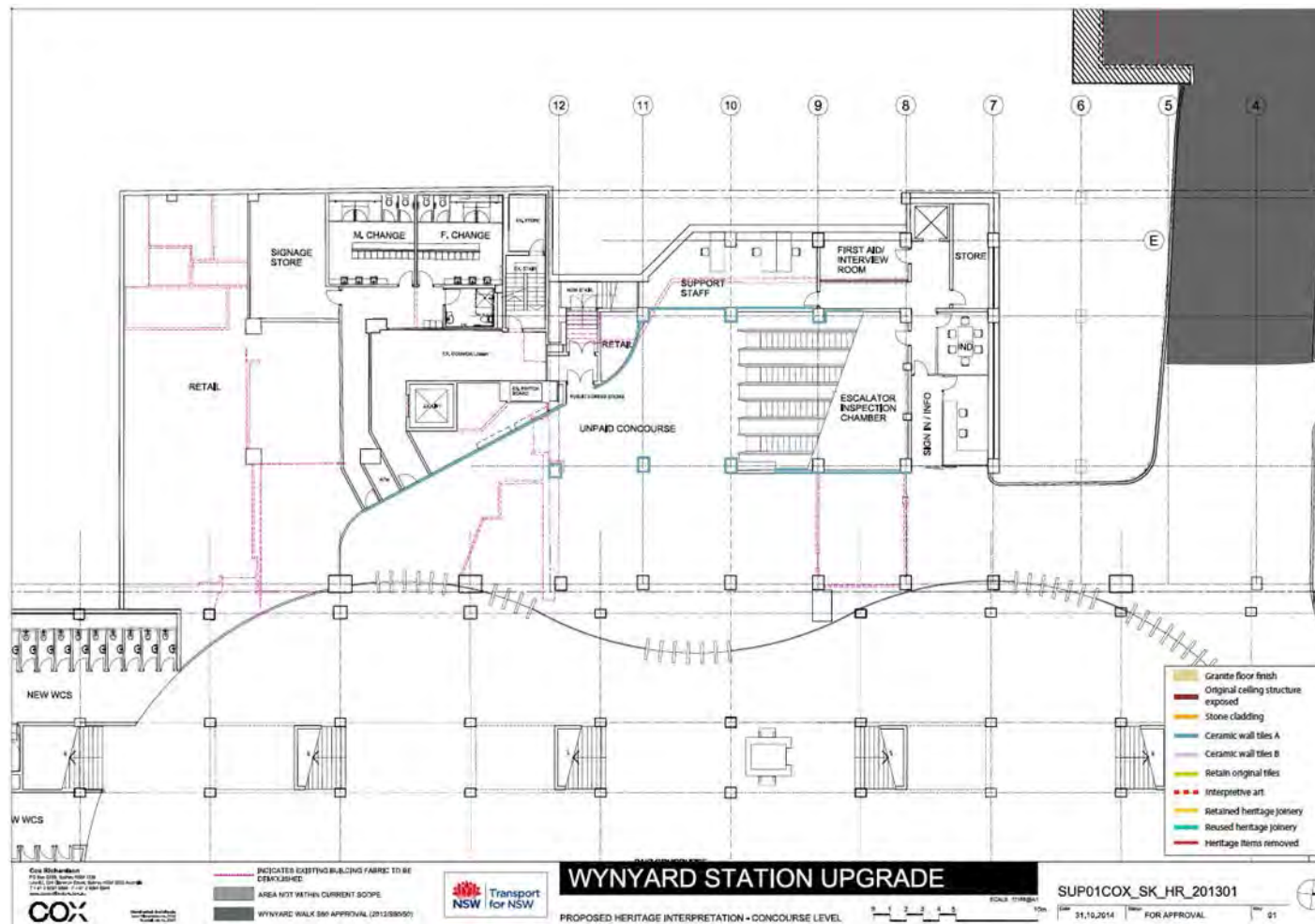


Figure 101 - Proposed works to the Basement Level 4, with the intact heritage fabric to be retained indicated in colour. (Source: Wynyard Station Upgrade, Drawing Issue For Approval, prepared by Cox Richardson, dated 31/10/14).

10.5 Wynyard Park & Former Tram Tunnels

Proposed Works

Potential provision of temporary construction hoist and compound to Wynyard Station through the southern portion of Wynyard Park, namely between the Wynyard Park entry pavilion and Erskine Street.

Assessment

In order to facilitate construction works, it is proposed to excavate a large access pit, approximately 8m x 10m in size, through the southern portion of Wynyard Park. The purpose of this pit is to be able to deliver large amounts of construction materials required for the redevelopment works at the Wynyard Station concourse level. The access pit would be excavated down to the level of the Wynyard Car Park. However, it would not directly impact on fabric contained within the curtilage of the Wynyard Tram Tunnels. With respect to impacts on heritage fabric, the work would involve temporary disturbance of the landscaping and fabric in Wynyard Park.

The proposed Wynyard Park compound and construction hoist would only be pursued if other available access became constrained or prohibited. Any compound and hoist installations would be temporary and removed at the conclusion of construction.

The *Central Sydney Archaeological Zoning Plan 1997* has identified Wynyard Park as a site with “Archaeological Potential – partially disturbed” and it is clear from the construction of the City Circle Rail Line in the 1930s that the archaeological fabric in Wynyard Park was disturbed around this time. The archaeological significance retained within this area is mostly only in reference to the area’s associated significance to the original military parade grounds, which would have already been impacted dramatically with the construction of the Wynyard Car Park. Therefore, original fabric in this area is also likely to be considerably disturbed. All works would be outside of the heritage curtilage for the Wynyard Tram Tunnels, and therefore not impact on these areas.

The archaeological potential in the southern half of Wynyard Park is negligible considering the impact made to it in the 1950s. Photographs of the park during the construction of Wynyard Station in the 1930s, and again during the extension of the City Circle line through to Circular Quay in the 1950s, show the extensive excavation works already undertaken throughout the park and indicating the effectively void opportunities for archaeological material of significance remaining. In terms of any historic landscaping with associations to the original military parade grounds, the proposal does not involve any permanent changes to the landscaping or space, only temporary during the duration of the redevelopment of works.



Figure 102 – Wynyard Park, facing south over the area of proposed works (Image Source: Futurepast 22/08/2014)

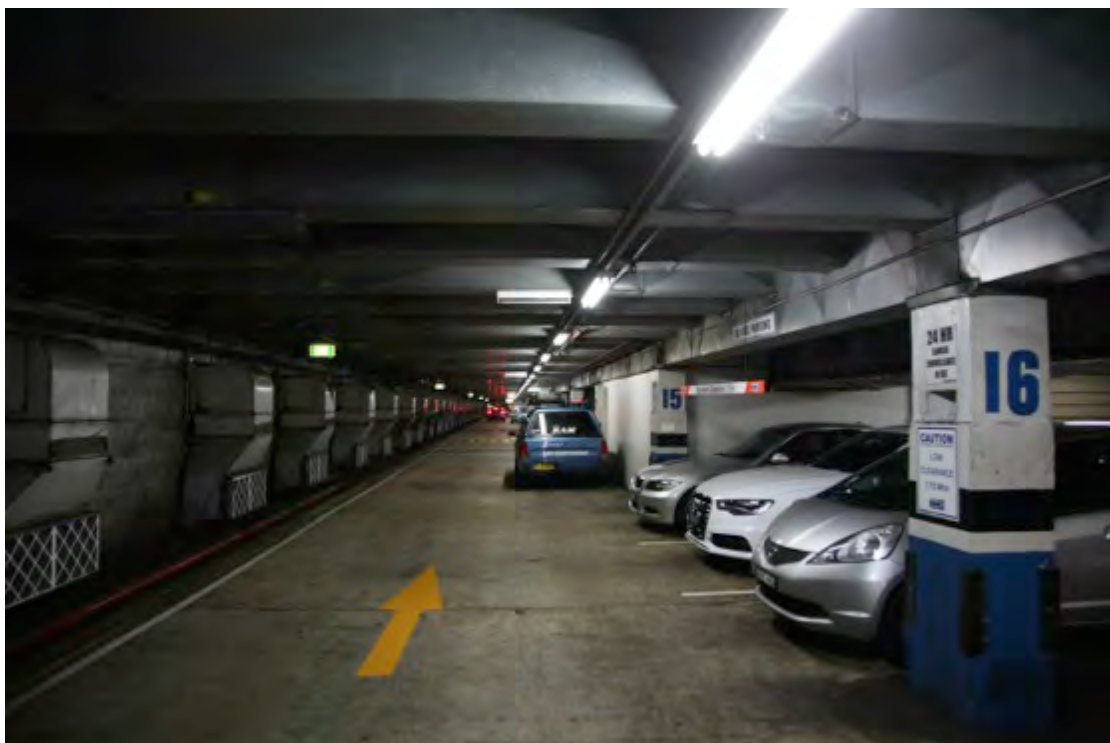


Figure 103 – The Wynyard Station car park proposed for excavation for the movement of materials (Image Source: Futurepast 22/08/2014)

Heritage Impact

The removal of a portion of the concrete roof of the Wynyard Car Park would have a minimal and reversible impact. Excavation in this area is unlikely to result in the disturbance of archaeological material.

The work is also only intended as temporary and in heritage terms, is minor and inconsequential. The heritage impact arising from this proposed item of works is minor and acceptable.

10.6 Summary

All of the works above have been formulated to address current operational requirements and technological upgrades to operational infrastructure, whilst improving the appearance and functionality of the station. As assessed above, none of the works identified have substantial adverse or unacceptable heritage impacts.

The majority of the areas impacted by these works have already been heavily modified and contain late 20th century finishes that are in poor condition. The major works are impacting on utilitarian areas of the basement of Transport House and would have minimal heritage impact.

The new curved gateline design ticket gate arrangement between the paid and unpaid concourse represents a departure from the original design of the concourse area. That said, the concourse areas (both paid and unpaid) are not identified as major contributors to the significance of Wynyard Station. The major loss of significance within the concourse area occurred with the loss of the Art Deco elements, such as the former Grill Room and the connection of the concourse to Hunter Arcade and other adjacent retail areas. Those actions in the 1970s to the 1990s saw the substantial reconfiguration of the space and the loss of most original fabric, save for the underlying structure itself.

The remnant decorative finishes which are visible or partially concealed within the concourse area are too fragmentary to warrant reinstatement and the increased patronage demands does not provide the physical space to accommodate the reconstruction of significant elements such as the former Grill Room. Consideration should be given as to how these historical elements can be interpreted within the unpaid concourse and remnant elements should be recorded and, where possible, retained and concealed by new works.

The new concourse design would be clearly contemporary and would replace fabric, which is of low significance, worn and of low quality. The revealing of the historic structural elements and reinstatement of the original lighting design within the paid concourse and within the transition zone from the base of the escalations on Basement Level 4 of Transport House would provide for a suitable expression of the historic character of the station within the increased modern demands on the space. Other areas of the station, including the platform staircases, the timber escalators and the platforms themselves, would retain historic fabric and finishes in these areas, which are of greater heritage significance.

10.7 Curtilage and Subdivision

The proposed works would have no impact on the subject sites curtilage or subdivision.

10.8 Views and Setting

With the works being undertaken to elements that are underground, the proposed works would have no permanent impact on views to or from these items, or their setting.

The proposed construction hoist and compound within Wynyard Park would have a temporary visual impact.

10.9 Heritage Items in the Vicinity

As works are all located below ground, there would be no consequential impacts on any heritage items in the vicinity. These include the heritage items identified in Section 7 above.

11 Statutory Controls

11.1 Heritage Act 1977

The *Heritage Act* 1977 provides protection for items of State heritage significance that are listed on the SHR, as well as for unlisted archaeological relics. Works proposed for items protected by the *Heritage Act* are approved by the Heritage Council of NSW or its delegates, as appropriate.

Transport House is an item of State Heritage significance and is listed on the State Heritage Register (SHR item # 1271). This report forms part of the supporting information for a Section 60 application for works within the Transport House curtilage (the York Street foyer, and Basement Levels 1 to 4 only).

Outline of Approvals and Exemptions – Section 57(1), Section 60

Pursuant to Section 57(1) of the *Heritage Act*, the approval of the Heritage Council of NSW is generally required for proposed development within a site included on the SHR, including works to the grounds or structures²⁸. However, within the legislation are provisions for certain works to be exempt from requiring approval.

Minor activities do not require approval under the *Heritage Act*, if undertaken in accordance with the guidelines set out in *Standard Exemptions For Works Requiring Heritage Council Approval* (NSW Heritage Council, 2009). The Standard Exemptions include works relating to:

1. Maintenance And Cleaning
2. Repairs
3. Painting
4. Excavation
5. Restoration
6. Development Endorsed By The Heritage Council Or Director-General
7. Minor Activities With Little Or No Adverse Impact On Heritage Significance
8. Non-Significant Fabric
9. Change Of Use
10. New Buildings
11. Temporary Structures
12. Landscape Maintenance
13. Signage
14. Burial Sites And Cemeteries
15. Compliance With Minimum Standards And Orders
16. Safety And Security
17. Movable Heritage Items

²⁸ *Heritage Act* 1977 Part 4, Division 2, Section 57.

The Minister may also approve specific statutory exemptions for works undertaken by state agencies, in certain conditions. Any works outside the parameters of the Standard Exemptions or Statutory Exemptions require an application under Section 60 of the *Heritage Act* to the NSW Heritage Council.

In relation to the proposed works outlined in the Section 9 above, these works are beyond the threshold for exempt work and therefore the works within and affecting Transport House (only) require Section 60 approval.

Relics provisions – Section 139

The *Heritage Act* provides protection to all relics and deposits identified as having State or local heritage significance. It is illegal in New South Wales to disturb or excavate land to discover, expose or move a relic without a permit issued by the Heritage Council of New South Wales.

Division 9 Section 139 provides the main legislative constraints regarding archaeological remains and states:

1. *(1) A person must not disturb or excavate any land knowing or having reasonable cause to suspect that the disturbance or excavation will or is likely to result in a relic being discovered, exposed, moved, damaged or destroyed unless the disturbance or excavation is carried out in accordance with an excavation permit.*
2. *(2) A person must not disturb or excavate any land on which the person has discovered or exposed a relic except in accordance with an excavation permit.*
3. *(Listed as point 4) The Heritage Council may by order published in the Gazette create exceptions to this section, either unconditionally or subject to conditions, in respect of any of the following:*
 1. *Any relic of a specified kind or description,*
 2. *Any disturbance or excavation of a specified kind or description,*
 3. *Any disturbance or excavation of land in a specified location or having specified features or attributes,*
 4. *Any disturbance or excavation of land in respect of which an archaeological assessment approved by the Heritage Council indicates that there is little likelihood of there being any relics in the land.*

Section 139 and 140 of the *Heritage Act* state that any item identified as an historical archaeological site or relic cannot be impacted upon without an excavation permit. An excavation permit provides an approval from the Heritage Council for permission to disturb a relic or site.

Due to the extensive disturbance of the archaeological material in this area, the potential to uncover or damage relics is negligible and therefore an excavation permit under the *Heritage Act* is not required. However, as Wynyard Park is identified as an item of local significance in the *Sydney LEP 2012*, it would be appropriate to record and photograph the pit, including depths and stratigraphy where possible after the Wynyard Car Park is penetrated, to inform the future management of the area.

11.2 Environmental Planning and Assessment Act 1979

The proposal is subject to assessment under Part 5 of the NSW *EP&A Act*. Transport for NSW is the proponent and determining authority for the proposal under Part 5 of the *EP&A Act*. The proposal is not declared to be State Significant Infrastructure (SSI) and would not require assessment under Part 5.1 of the *EP&A Act* given that:

- The proposal would not have a significant impact on the environment, including critical habitat and threatened species, populations and ecological communities and their habitats.
- The Minister for Planning has not declared the proposal to be SSI by way of an Order made under section 115U of the *EP&A Act*.

As such, a REF has been prepared to fulfil the requirements of Section 111(1) of the *EP&A Act*. This section of the *EP&A Act* requires that a determining authority: “*examine and take into account to the fullest extent possible all matters affecting or likely to affect the environment*” with respect to the proposed works.

The specific requirements of what must be contained in an environmental assessment are set out in Schedule 2 of the *Environmental Planning and Assessment Regulations 2000*. Heritage matters fall within the scope of ‘environment’ in relation to this Act.

This report and its recommendations should be included within the Part 5 environmental impact assessment. This report fulfils the requirements for assessment under Part 5 of the *EP&A Act*.

11.3 State Environmental Planning Policy (Infrastructure) 2007

The *ISEPP* provides that certain types of infrastructure related construction activities may be undertaken without development consent, subject to certain conditions. Environmental and/or heritage assessment is still required as a part of the determination process under the *ISEPP*.

Planning pathway

Division 15 of the *ISEPP* provides for specific categories of development permitted without consent related to railway or rail infrastructure facilities. Subdivision 1, Clause 78 defines rail infrastructure facilities as (relevant infrastructure categories have been underlined for clarity):

78 Definitions

rail infrastructure facilities include:

- (a) *railway tracks, associated track structures, cuttings, drainage systems, fences, tunnels, ventilation shafts, emergency access ways, bridges, embankments, level crossings and roads, pedestrian and cycleway facilities, and*
- (b) *signalling, train control, communication and security systems, and*
- (c) *power supply (including overhead power supply) systems, and*

- (d) railway stations, station platforms and areas in a station complex that commuters use to get access to the platforms, and*
 - (e) public amenities for commuters, and*
 - (f) associated public transport facilities for railway stations, and*
 - (g) maintenance, repair and stabling facilities for rolling stock, and*
 - (h) refuelling depots, garages, maintenance facilities and storage facilities that are for the purposes of a railway, and*
 - (i) railway workers' facilities, and*
 - (j) rail freight terminals, sidings and freight intermodal facilities,*
- but do not include buildings or works that are for residential, retail or business purposes and unrelated to railway purposes.*

This proposal is defined as development “for the purpose of a railway or rail infrastructure facilities” under clause 79 of the *ISEPP* and the works are permissible without development consent, however a determination is required under Part 5 of the *EP&A Act* (and the *Heritage Act* for the components of the works within Transport House).

Consultation

Clause 14 of the *ISEPP* requires consultation with the relevant local council for works impacting local heritage items on environmental planning instruments or being undertaken within a heritage conservation area, unless those works are “minor or inconsequential”.²⁹ If works are not “minor or inconsequential”, a written assessment must be undertaken of the impacts of the proposed works upon the heritage item or conservation area.³⁰

Wynyard Station is not listed on the *Sydney LEP 2012* as a heritage item, nor is it included in any Heritage Conservation Areas. However, Wynyard Park and Transport House (formerly known as Railway House) are local heritage items listed in the *Sydney LEP 2012*.

The works to Wynyard Park are temporary construction impacts only and are therefore considered minor or inconsequential, and the Clause 14 consultation requirements do not apply.

The works impacting Transport House are considered to be greater than “minor and inconsequential” and Clause 14 of the *ISEPP* applies. This document serves as a written assessment of the heritage impacts of the works.

²⁹ Ibid., Section 14(1)(a)

³⁰ Ibid., Section 14(2)(a)-(c)

This report fulfils the assessment requirements under the *ISEPP* and may be provided to City of Sydney Council as a part of Clause 14 consultation under the *ISEPP*.

Approval and Notification Requirements – <i>ISEPP 2007</i>		
Section/Clause	Action Required	Relevant Authority
14	Heritage Impact Assessment required for works considered greater than “minor or inconsequential”. Notification required to local council for works to Transport House	City of Sydney Council

11.4 Sydney Local Environmental Plan 2012

Parts of the subject site, namely Transport House (item # I1975) and Wynyard Park (item # I1971) are listed heritage items under the *Sydney LEP 2012*, Schedule 5 Environmental Heritage, Part 1 Heritage items. As Transport House is also included in the SHR as an item of State significance, controls for this item are addressed in Section 11.1, Statutory Controls, *Heritage Act 1977*, below.

In regards to Wynyard Park, an item of local significance, there are no site or area specific controls identified within the *Sydney LEP 2012*. Clause 5.10 (4) of the LEP requires that council must assess the heritage significance of the item or conservation area and take into account the extent of impact of the proposed works on the heritage significance of the site or area. This clause does not apply, as the proposal is permissible without consent, by virtue of the *ISEPP*. However, this report has been undertaken to assess the impact of proposed works on the heritage significance of the site and area. As such, the intent of the above clause has been fulfilled and this assessment represents compliance with the *Sydney LEP 2012*.

12 Conclusion and Recommendations

The proposed works would have an acceptable impact to the heritage significance of Wynyard Station generally and would facilitate the ongoing and improved operational use of the site. The proposed works are extensive, however the majority involve demolition, reconfiguration of original spaces, re-finishing and alterations to non-significant fabric. In most instances, the works are impacting on fabric that is non-contributory to the overall significance of the site. Significant features generally would be retained and refurbished within the station, with some elements such as lighting reconstructed to an historic configuration in some areas.

The works represent an aesthetic improvement to the station precinct, modernising and de-cluttering the Paid and Unpaid concourses and platform areas to increase customer amenity. Several of the proposed works are also necessary for health and safety compliance reasons. The removal of unnecessary or intrusive fabric has a positive impact from a heritage perspective, representing a partial return to the original streamlined and functional aesthetic qualities of the site. Revealing of the original fabric of the ceilings of the York Street foyer to Transport House and within areas of the Paid Concourse and B4 transition zone is a positive heritage impact, as is reinstatement of the original lighting detail in these areas.

Scope exists for further refinement of proposed finishes and colours within the development of further detailed design for the project, and are matters suitable to be addressed under conditions of consent.

The works within Transport House are largely affecting modified areas and finishes, and utilitarian spaces and, as such, would have minimal heritage impact. As the works would involve some impact to the basement levels of Transport House, an item of State Heritage significance, a Section 60 approval is required. Further to this, archival recording of the site prior to works is required.

Wynyard Station is not listed on the *Sydney LEP 2012* and consultation requirements do not apply. Transport for NSW is required to consult with and provide an assessment of the impacts to Transport House and Wynyard Park to City of Sydney Council under Clause 14 of the *ISEPP*.

The following mitigation measures are recommended prior to the commencement of works:

Identification number	Mitigation and management measure
General	
H1	<p>A Heritage Construction Environmental Management Plan (HCEMP) would be prepared and would be developed in consultation with Sydney Trains (Heritage), the Office of Environment and Heritage (Heritage Division) and City of Sydney prior to the commencement of construction. The HCEMP would be prepared by a suitably qualified heritage specialist.</p> <p>The HCEMP would include (but is not limited to):</p> <ul style="list-style-type: none"> Identification of heritage items, and known heritage fabric. Description of work practices (generic and specific) to be applied to avoid and minimise impacts to heritage fabric. Stop work procedures that would be implemented should original fabric or unexpected historical archaeological relics are discovered during construction. Procedures to monitor works in sensitive areas, including reporting and notification of accidental damage to heritage fabric.
H2	<p>Prior to the commencement of any demolition works (including reconfiguration of spaces), refurbishment works (including painting) and/or re-purposing of spaces within Wynyard Station and Transport House, annotated plans and a photographic recording would be undertaken of all areas to be modified by the proposal. This includes:</p> <ul style="list-style-type: none"> Plans and photographs illustrating the current layout and uses. Plans and photographs of the original art deco ceiling treatment within the former Railway Refreshment Rooms and the preparation of a Reflected Ceiling Plan which would record the remaining features of the ceiling prior to demolition. A historic paint test analysis of heritage fabric that would be impacted by the proposal.
H3	<p>Prior to works commencing, contractors shall be briefed as to the sensitive nature of the site and any recommended mitigation measures or controls required.</p>
H4	<p>In the event that any unexpected historical archaeological relics or original fabric is discovered during construction at the site (including Wynyard Park), works in the affected area(s) would cease and the Office of Environment and Heritage (Heritage division) would be notified. Further assessment, documentation or approval may be</p>

	<p>required before site works could recommence in the affected area(s).</p> <p>Records of any original finishes exposed during works should be lodged with Transport for NSW. This should include photos, location plans and samples of finishes, as appropriate.</p>
H5	<p>In the event that original fabric is discovered in Wynyard Station or Transport House, the following steps and considerations would be undertaken in consultation with a suitably experienced heritage practitioner as part of the stop work procedures:</p> <p>If original finishes are discovered, the finishes are to be recorded and sampled in accordance with relevant OEH (Heritage Division) guidelines.</p> <p>In-situ retention of the features and incorporation into the proposal design, where feasible and reasonable, would be investigated.</p> <p>If in-situ retention is feasible, methods to remove modern finishes that minimise potential damage to the original fabric would be implemented.</p>
H6	<p>A lighting design for the unpaid concourse would be developed during detailed design in consultation a suitably experienced heritage practitioner and in consultation with Sydney Trains. The lighting design would include a sympathetic response and transition between the western unpaid concourse (within Transport House) and other areas of the station.</p>
Transport House	
H7	<p>An approval under Section 60 of the Heritage Act 1977 would be obtained prior to any works associated with the proposal commencing within Transport House.</p>
H8	<p>No structural modifications to the façade of Transport House (along its York Street and York Lane façade) is permitted as part of this proposal.</p>
H9	<p>During detailed design, the refurbishment of the York Street foyer, reconstruction of the western fire staircase and fit out of the basement levels of Transport House (including the western unpaid concourse) would be finalised in consultation with a suitably experienced heritage practitioner and Sydney Trains (Heritage).</p> <p>The design of York Street foyer would investigate opportunities for salvage of any original fabric and/or heritage interpretation associated with Transport House, as well as the integration with Wynyard Walk.</p>
H10	<p>Original tiles and flooring within the western corridor of the York Street foyer are to be conserved and retained in-situ, and protected during construction to prevent damage.</p>

H11	An experienced heritage practitioner would supervise the removal of modern fittings within Transport House, including the York Street foyer.
H12	The position of service utilities (such as power and air conditioning) within the basement levels of Transport House would use existing openings within ceilings/walls where feasible. The location of final openings would be determined in consultation with an experienced heritage practitioner.
H13	<p>Two timber doors located within Basement Level 1 of Transport House, which would be removed (after necessary archival recordings have been completed) to accommodate toilet amenities, would be re-used within Basement Level 1.</p> <p>All other timber doors found in basement levels of Transport House impacted by the proposal would be retained in situ during the works. Where there is a preventative need to remove them, the doors are to be packed and stored to prevent damage to the doors. The door frames and architraves are to be temporarily protected with a localised timber hoarding/casing.</p>
H14	During detailed design, consideration would be given to feasibility of incorporating design features that meet BCA requirements and reflect the original design of the western fire staircase, including the art deco tiling, stair design and re-use of the original balustrade.
H15	Structural changes below stairs and escalators within Transport House is to be subject to a structural assessment prior to any demolition or modification work being undertaken to prevent damage to remaining heritage fabric.
Wynyard Station	
H16	The extent of disturbance to the decorative profiles of original art deco ceiling treatments associated with the Railway Refreshment Rooms (including the former Grill Room) would be minimised. Remaining portions of the ceiling are to be retained and protected within modern ceiling finishes.
H17	<p>Original balustrades would be retained and protected in situ, or retained and re-used within the station design where direct impacts are anticipated.</p> <p>Original remnant timber handrails, that have not been previously painted, are to be kept as exposed timber.</p> <p>Additional treatments to original balustrades that are required to comply with Building Code of Australia would not significantly obstruct views of the railings.</p>

H18	Original tiles that would be impacted would be recorded and salvaged for re-use elsewhere within the station domain. Where original tiles would require removal, methods to remove the original tiles in a manner that avoids or minimises the potential for damage to the tiles would be implemented.
H19	Where repainting works would involve the stripping of painted original metal elements within the station, testing would be undertaken to ensure no original finishes remain. If original finishes are uncovered, the unexpected finds procedure would apply.
H20	Where original timber doors on station platforms are to be removed and replaced with modern fire safety compliant doors, an example of the original doors is to be retained either in storage on site, or relocated to Transport Heritage NSW's movable heritage collection
H21	Where feasible, new doors for back-of-house areas on station platforms would be designed to include a heritage styled sheeting to replicate the appearance of the original panelled doors.
H22	Removal or reduction of the rooms below the staircases on the station platforms is to be subject to a structural assessment prior to any demolition or modification work being undertaken to prevent damage to remaining heritage fabric.
H23	The incorporation of station platform signage that reflects the original signage of Wynyard Station would be considered as part of the wayfinding design on Platforms 3 and 4, and Platforms 5 and 6.
Wynyard Park and the former Wynyard Tram Tunnels	
H24	All remnant sandstone and landscape elements within Wynyard Park in proximity to the compound and access route are to be protected during construction works within the curtilage of Wynyard Park (if the compound is required).
H25	The area excavated for the temporary construction hoist at Wynyard Park is to be photographically recorded prior to the demolition of the car park surface, and following the establishment of the cross section, recording of depths and stratigraphy, to inform the future management of the area.

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