

Transport for NSW

Turrella Station Upgrade

Statement of Heritage Impacts



Artist's impression of the Turrella Station upgrade subject to detailed design.



STATEMENT OF HERITAGE IMPACTS

Transport Access Program

FINAL

Prepared by
Umwelt (Australia) Pty Limited
on behalf of
Transport for NSW

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1.0 Introduction

1.1 Background

Transport for NSW propose to undertake accessibility upgrades at Turrella Station (the Proposal) located 8.5 km southwest of Sydney CBD. The proposal forms part of the Transport Access Program (TAP), which seeks to provide accessible, safe, secure and integrated transport infrastructure across the existing rail network. Transport for NSW has engaged Umwelt to assess the potential heritage impacts of the proposal on the heritage significance of the Turrella Station (herein referred to as the Proposal Area).

This Statement of Heritage Impacts (SoHI) has been prepared as part of the Review of Environmental Factors (REF) for the concept design for the Proposal. The reference design will form the basis for the development of detailed design and construction documentation following receipt of Determination Approval from the consent authority.

In addition to the impact assessment contained in this SoHI, a set of recommendations in **Section 8.2** have been provided to guide the ongoing development of the detailed design and construction methodology. These recommendations aim to minimise or mitigate (where possible) the potential impacts of the Proposal on Turrella Station.

1.2 The Proposal

The Proposal involves upgrades to Turrella Station as part of the TAP, which will improve accessibility and amenities for customers. The Proposal includes the following key features:

- a new lift providing access from the Reede Street bridge to the station platform
- a new station entrance from the Reede Street bridge. The new station entrance would include:
 - demolition of the existing station access and a section of the eastern platform canopy in order to accommodate the new stairs, lift and landing
 - construction of a new station entrance landing area
 - construction of new stairs between the proposed landing and the station platforms
- internal modifications to the existing station building including
 - provision of a new unisex ambulant toilet in the location of the existing female toilets
 - provision of a new family accessible toilet in the location of the existing male toilets
- modifications to kerb and line markings to accommodate the construction of a new kiss and ride bay along Turrella Street
- upgrades along the footpath located on Reede Street and Turrella Street to provide an accessible and safe path of travel to the new station entrance including installation of traffic barriers
- localised platform regrading and the installation of new tactiles along the platforms
- provision of three new bike hoops accommodating up to six bicycles located on Henderson Street

- ancillary works including improvements to station lighting and CCTV to improve safety and security, electrical upgrades for the new infrastructure (new padmount transformer located on Henderson Street), landscaping work and adjustments to wayfinding signage.

A detailed description of the Proposal is included in **Section 5.4**.

1.3 Proposal Area Overview

The Proposal Area includes Turrella Railway Station and the surrounding public domain on Turrella Street, Reede Street and Henderson Street. Turrella Station is located between Henderson and Turrella Streets in the suburb of Turrella. It is in the Bayside Local Government Area, approximately 8.5km southwest of the Sydney CBD. **Figure 1.1** shows the location of the Proposal Area.

1.4 Methodology

This SoHI has been prepared in accordance with the *Burra Charter: The Australian ICOMOS Charter for Places of Cultural Significance* (Burra Charter) (2013) and the best practice standards set out by Heritage NSW.

Best practice guidelines that have been followed in this report include *Assessing Significance* (Heritage Office (former), 2001), and *Statements of Heritage Impact* (Heritage Office and Department of Urban Affairs and Planning (former), 1996, revised 2002). Reference has also been made to the *Sydney Trains Canopies and Shelters Design Guide for Heritage Stations* 2016 and the *Transport for NSW Managing Heritage Issues in Rail Projects*.

1.5 Limitations

The SoHI was prepared with the following limitations:

- The description of the Proposal Area has been prepared based on information available within the NSW State Heritage Inventory (SHI) citation for the 'Turrella Railway Station Group', desktop review of the Proposal Area (including a review of available aerial and streetscape imagery) and information provided by Transport for NSW.
- It should be noted that the project architect DesignInc have engaged a separate heritage consultant (GML Heritage Pty Ltd) to provide iterative design advice during the preparation of the SDR Design. As such, the discussion of options and heritage advice outlined in this report has been based on information provided to Umwelt from Transport for NSW, and may not include all information provided to the design team by other consultants in the preparation of the design.

1.6 Author Identification

This SoHI has been prepared by Melissa Moritz, Senior Heritage Consultant. Review and input have been provided by Karyn Virgin, Principal Heritage Consultant.

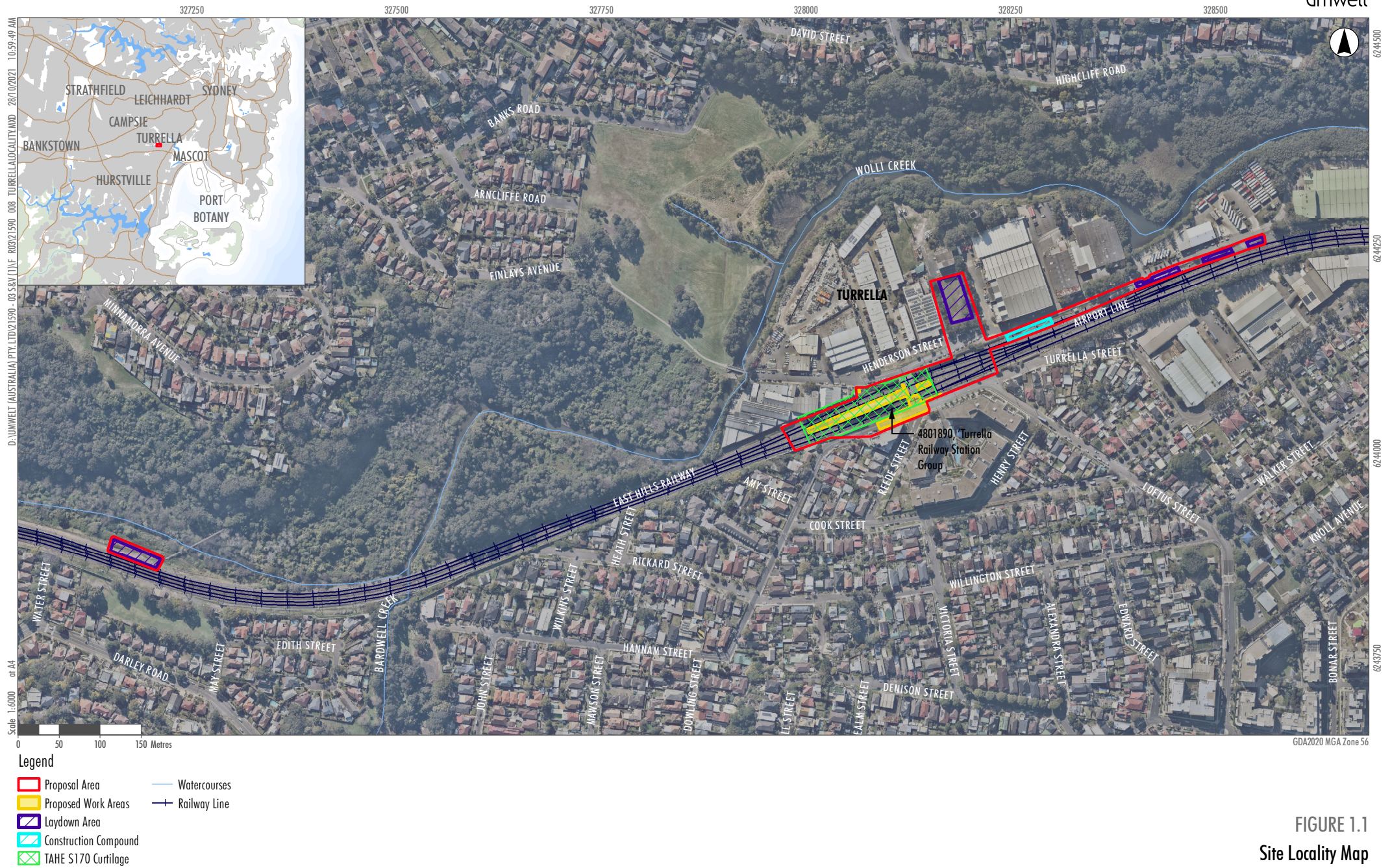


FIGURE 1.1
Site Locality Map

2.0 Statutory Context

The following section provides an overview of the legislative framework relating to the protection and management of historic heritage in NSW. The management and conservation of non-Aboriginal heritage items, relics, archaeological sites and places is subject to a range of statutory provisions in the NSW government legislation. The relevant statutory and non-statutory heritage listings are discussed in this section, along with an overview of the heritage listings that are applicable to the Proposal Area.

2.1 The Heritage Act 1977 (NSW)

The *Heritage Act 1977* (Heritage Act) affords automatic statutory protection to items of heritage significance which form part of the heritage record of NSW (except where these provisions are suspended by other prevailing legislation). The Heritage Act defines a heritage item as a place, building, work, ‘relic’, moveable object or precinct.

The Heritage Act describes a ‘relic’ as any deposit, object or material evidence that is:

- related to the settlement of the area that comprises NSW, not being Aboriginal settlement
- is of State or local heritage significance.

The Heritage Council of NSW (Heritage Council), appointed by the Minister, is responsible for heritage in NSW, as constituted under the Heritage Act. The Heritage Council is a cross-section of heritage experts with the Heritage NSW being the operational arm of the Heritage Council.

Under Section 170 of the Heritage Act, all State government agencies must establish, keep and maintain a list of assets within their ownership and/or management that have been identified as having heritage significance—this can be local or State heritage significance.

The obligations for government agencies, including Transport for NSW, under Section 170 are outlined within:

- NSW Heritage Council, *State Agency Heritage Management Guidelines* (endorsed 1 December 2004)
- *State-Owned Heritage Management Principles* (approved 16 December 2004) under Section 170A of the NSW Heritage Act 1977.

2.2 Environmental Planning and Assessment Act 1979

The *Environmental Planning and Assessment Act 1979* (EP&A Act) requires that consideration be given to environmental impacts – including heritage – as part of the land use planning process. The provisions of the EP&A Act allow for the implementation of Local Environmental Plans (LEPS) – the standard instrument within which local governments manage impacts to the environment, including heritage.

Part 5 Clause 5.10 of the Rockdale LEP 2015 provides the statutory framework for heritage conservation including the conservation of:

- the environmental heritage of Rockdale
- the heritage significance of heritage items and heritage conservation areas, including associated fabric, setting and views

- archaeological sites and
- Aboriginal objects and Aboriginal places of heritage significance.

2.3 State Environmental Planning Policy (Infrastructure) 2007

State Environmental Planning Policies (SEPPs) are environmental planning instruments designed to address planning issues within NSW. In particular, they aim to simplify the development and approvals process for projects of either a certain type or in a particular precinct or location. The SEPPs often make the Planning Minister (or their delegate) the consent authority for the types of development a SEPP relates to. The *State Environmental Planning Policy (Infrastructure) 2007* (ISEPP 2007) applies to the Proposal. In relation to heritage, Part 2, Division 14 of the ISEPP 2007 applies to the Proposal. Division 14(2) of the ISEPP 2007 specifies that:

A public authority, or a person acting on behalf of a public authority, must not carry out development to which this clause applies unless the authority, or the person has—

Had an assessment of impact prepared, and

Given written notice of the intention to carry out the development, with the copy of the assessment and a scope of works, to the council for the area in which the heritage item or heritage conservation area (or the relevant part of such an area) is located in, and

Takes into consideration any response to the notice that is received from the council within the 21 days after the notice is given

2.4 The Burra Charter: The Australian ICOMOS Charter for Places of Cultural Significance, 2013

The *Burra Charter* is a set of best practice principles and procedures for heritage conservation and investigation. The Charter was developed by the Australian group of the international professional organisation for conservation; the International Council for Monuments and Sites (ICOMOS). Although not a statutory document, the *Burra Charter* sets out the best practice standard for heritage management in NSW and Australia. The policies and legislative guidelines of the Heritage Council and Heritage NSW are consistent with and guided by the *Burra Charter*.

2.5 Relevant Heritage Listings

As part of the preparation of this SoHI, a review of relevant heritage databases was undertaken including:

- Rockdale LEP 2015
- NSW SHI (including the Section 170 registers)
- NSW State Heritage Register (SHR)
- Australian Heritage Database (including Commonwealth and National Heritage lists and the Register of the National Estate (RNE)).

These searches identified that the Proposal Area is listed in the following statutory register:

- Transport Asset Holding Entity (TAHE) (formerly RailCorp), Section 170 Heritage and Conservation Register–ID: 4801890, ‘Turrella Railway Station Group’.

The above listing excludes the public domain immediately adjacent to the station, which is also included in the Proposal Area.

The Proposal Area is not and does not fall within the curtilage of any local or State heritage items. It also does not fall within any local or State listed heritage conservation areas.

The closest heritage items to the Proposal Area are:

- Locally listed ‘Wolli Creek Valley’ (Item ID I237, Rockdale LEP 2015). This item is located 90m northwest of the Proposal Area.
- Local and State listed ‘Cairnsfoot Special School’ (Item ID I233, Rockdale LEP 2015, and SHR ID #00551). This item is located 330m southeast of the Proposal Area.
- Locally listed ‘Victorian House’ (Item ID I387, Rockdale LEP 2015). This item is located 370m southeast of the Proposal Area.

The location of the Proposal Area in relation to these heritage items is shown in **Figure 2.1**.

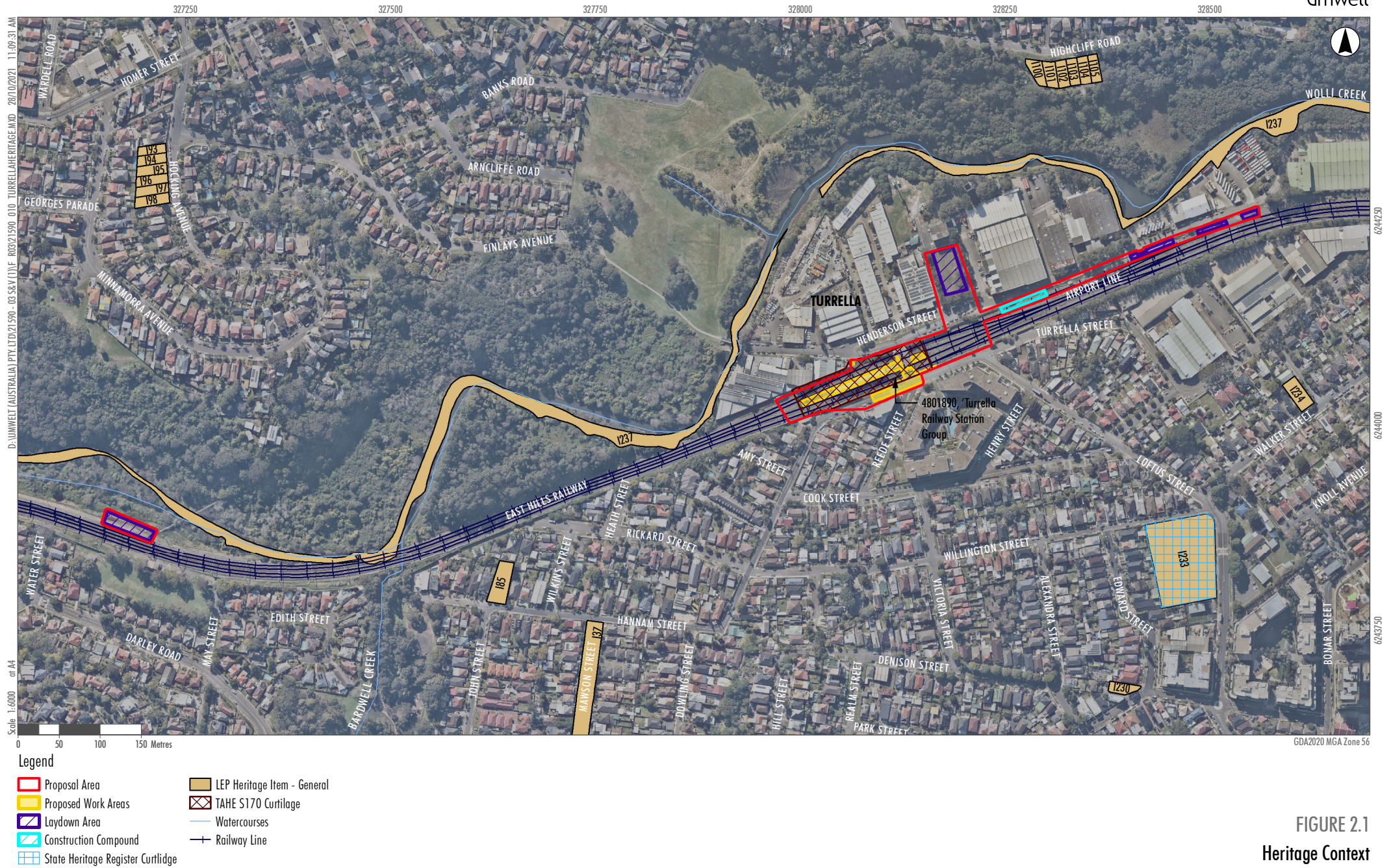


FIGURE 2.1
Heritage Context

3.0 Historical Context

The following historical information has been summarised from the historical notes contained within the SHI citation for the 'Turrella Railway Station Group', with supplementary information provided by Umwelt.

3.1 Early Settlement

The name of the suburb of Turrella derives from an Aboriginal word meaning "a reedy place" or "water weeds"¹. It is located in association with the Cooks River and within the Cooks River valley. By 1798 Cooks River had been named and appeared on the maps sent back to England.² However, the river proved to be a barrier to early expansion of the colony, with swampy ground and extensive mudflats limiting crossing points and slowing settlement of the area.

The first land grant in the area was to emancipist Reuben Hannam, who received 100 acres in 1819³. This was acquired by his son David Hannam after his death in 1852.

By the mid nineteenth century, a small farming population had settled along the river and creeks and sawyers, shingle splitters and charcoal burners were spread over the rest of the Cooks River valley, wherever the trees were available. The Proposal Area is located in this broader context.

As shown in the 1889 Parish Map (**Figure 3.1**), the Proposal Area was originally located in the land owned by Hannam, which encompassed land to the south of Wolli Creek valley, extending approximately to the present-day Willington Street (located to the south of the Proposal Area). Following the opening of the East Hills line in 1931, Turrella's farms and orchards were subdivided for residential and industrial development.

3.2 Turrella Railway Station

The main impetus for the construction of the East Hills line was from the real estate industry, which sought to develop the area where the line was proposed following the end of World War 1. Construction of the line was approved in 1924 however commencement was delayed until 1927. It later became an unemployment relief project during the course of its construction due to the onset of the Great Depression in 1929 (Figure 3.2).

The East Hills railway line was opened by Premier Jack Lang in 1931 in Padstow, with the section as far as Kingsgrove being a double track electrified line. This included Turrella Railway Station (the Proposal Area).

All station buildings on the East Hills line were built to the same general design and plan, which was revised after initial planning to include a booking office, Station Master's office and parcels office.

The East Hills line from Turrella to Panania is the only line in Sydney that has retained almost all station buildings from the original construction phase (1927-1931), noting that some of the station buildings have been altered over time. The only exception to this is the former station building at East Hills Station which was demolished in 1987.

¹ NSW State Heritage Inventory 'Turrella Railway Station Group' Accessed 24 August 2021 <<https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?id=4801890>>

² Dictionary of Sydney 'Back Huts and Country Estates' accessed 30 August 2021 <https://dictionaryofsydney.org/entry/bark_huts_and_country_estates#footnote-7>

³ NSW State Heritage Inventory 'Wolli Creek Valley' accessed 27 September 2021 <<https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2330169>>

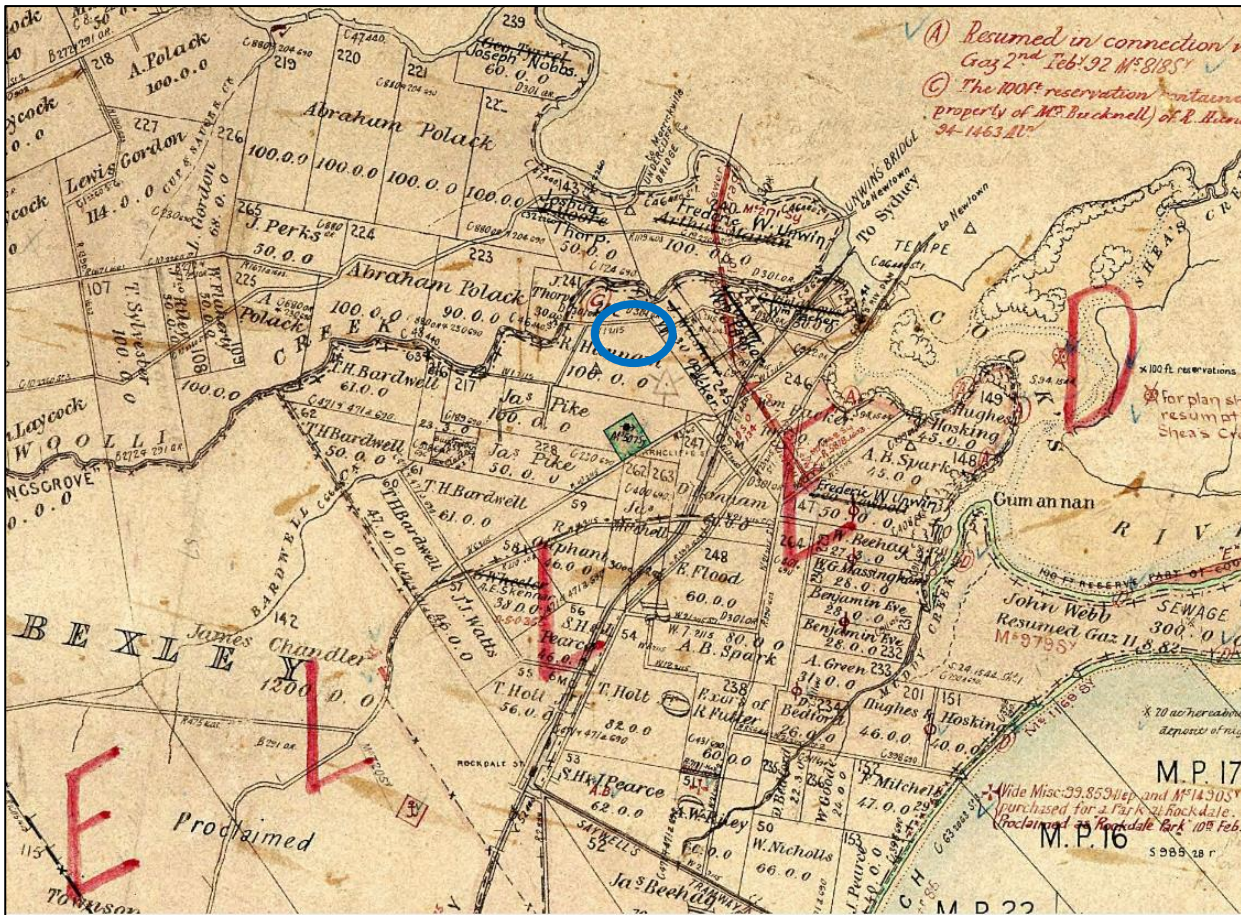


Figure 3.1 1889 Parish Map with the location of the Proposal Area indicated in Blue

Source: NSW Land Registry Services

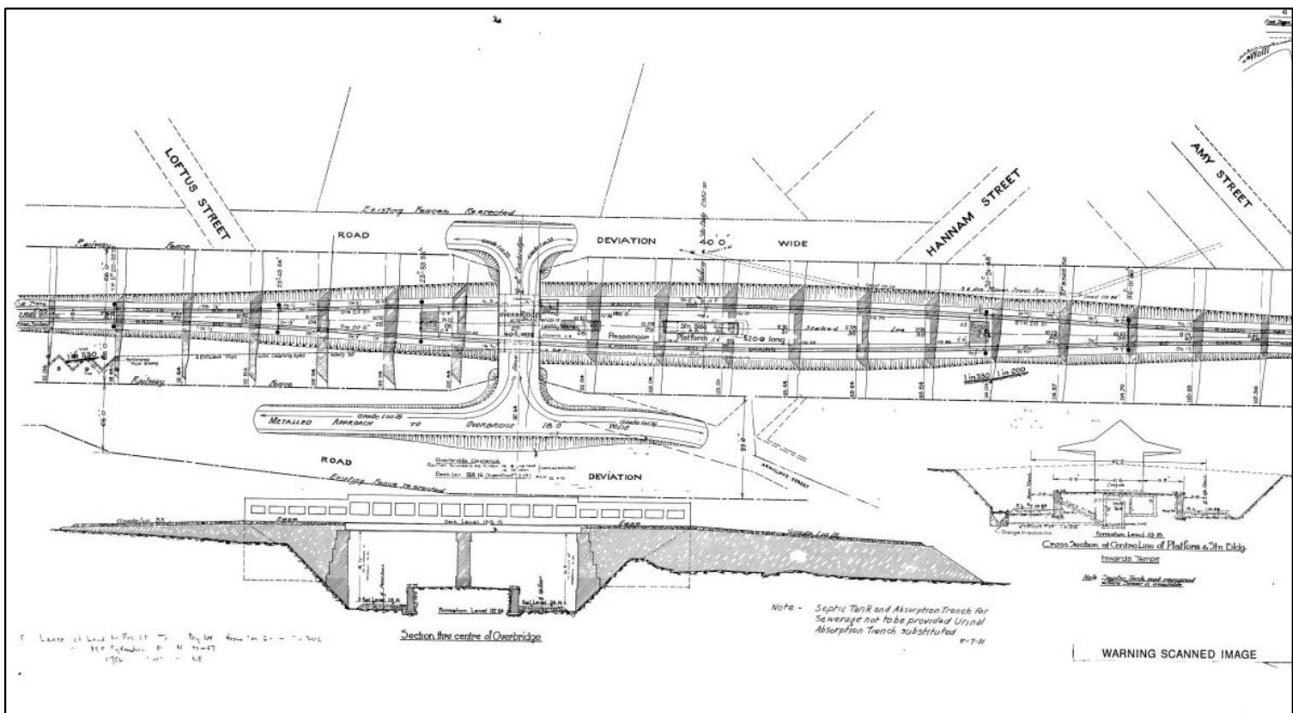


Figure 3.2 1929 plan for Turrella Station showing the (then) proposed configuration

Source: Transport for NSW.

When first built, Turrella Railway Station had one of the smaller brick station buildings along the line, and a (since demolished) separate booking office near the station entry point⁴.

However, the booking office was removed from the base of the stairs in 1980 and the stairway was further altered in 2001 during the widening of the overbridge for quadruplication of the line. This included major modifications to the stairway, replacing all of the structure except for the steel trestle and outside stringers at the west end. The upper section of the stair and the connecting landing to the overbridge were also replaced with a new reinforced concrete slab landing and concrete pillar support.

Construction work continued in 2009 to upgrade the East Hills line generally for quadruplication of the line.

In 2003 a gabled awning with a corrugated steel roof was attached to the east end of the station building, outside of the ticket window. The station building roof was also reclad in 2008, replacing the original corrugated fibro-asbestos sheet roofing with corrugated steel⁵.

The existing butterfly canopies to the east and west side of the station building were added in 2012. Other modifications to the station, such as upgrades to services, installation of furniture, rubbish bins and communications and security equipment, have occurred over time, as with the other stations on the East Hills Line. However, the station remains one of only a few on the line that have not been subject to accessibility upgrades, such as the installation of lifts, ramps, canopies and building upgrades for Family Accessible Toilets and communications equipment. These kinds of accessibility upgrades have been undertaken at Bexley North, Revesby, and Panania stations, amongst others.



Figure 3.3 **Historical photo of Turrella Station**

Opening of East Hills line, locomotive 3105 with end platform cars at Turrella station, Turrella, New South Wales, 21 December 1931

Source: National Library of Australia

⁴ NSW State Heritage Inventory 'Turrella Railway Station Group' Accessed 24 August 2021
<<https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?id=4801890>>

⁵ NSW State Heritage Inventory 'Turrella Railway Station Group' Accessed 24 August 2021
<<https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?id=4801890>>

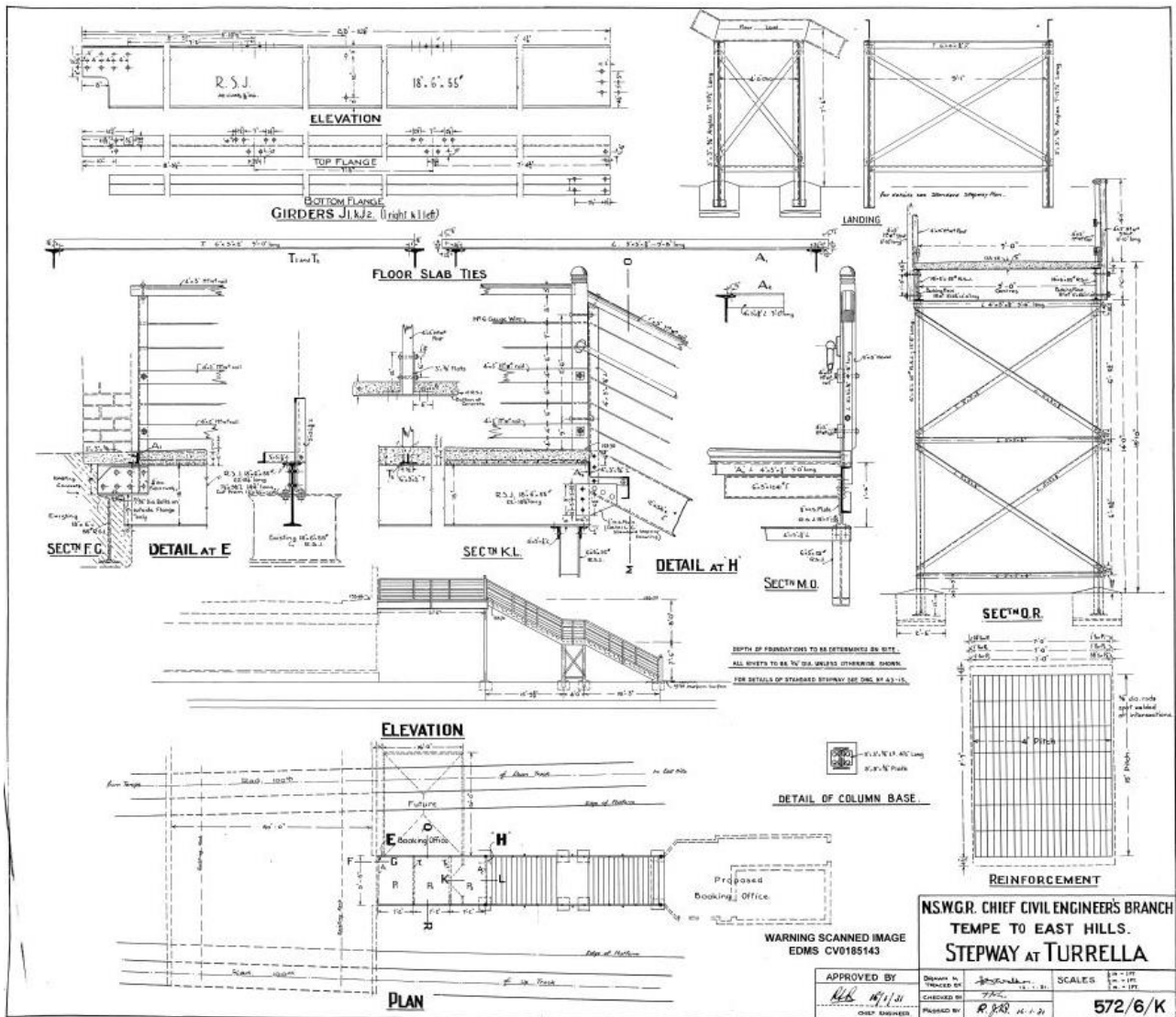


Figure 3.4 1931 plans of the stairway Turrella Railway Station

Source: Transport for NSW

Turrella Station uses the design shown for the station building.

Source: Transport for NSW

4.0 Description

4.1 Setting

The Proposal Area includes Turrella Railway Station and the surrounding public domain on Turrella Street, Henderson Street and the Reede Street Overbridge.

The Proposal Area is located in a densely developed residential setting, with some areas of industrial and commercial buildings to the north. The surrounding streetscape has been upgraded as part of residential developments, with narrow streets, concrete footpaths and standard street lights.

The Station is located in a cutting, between Turrella Street and Henderson Street, with the Reede Street Overbridge providing access stairs down to the station. Henderson Street slopes up substantially to Reede Street Overbridge. Turrella Street also slopes up to the overbridge although at a more gentle slope. The incline across the area means that Turrella Street sits slightly elevated, with the street almost level with the station building awning in some areas.

Due to the location in a rail corridor cutting and the surrounding terrain, views to the station are generally limited and the station no longer forms a dominant element in the landscape. Views of the station are partially available looking north from Turrella Street, with the platform, station building roof, and canopies visible. The previous modifications to the station are also visible in these views. More open views of the north side of the station building are available from Henderson Street, when approaching from the northeast.

The station building is also visible in views looking west from the Reede Street Overbridge (**Photo 4.1**). However, original views to the east elevation of the station building are now obscured by the tall canopies installed in 2012. The safety barriers to the overbridge are wire mesh fencing supported on metal poles. As this fencing is visually permeable, views to the station are still available but important detailing of the station building is obscured by the 2012 canopy.

Overall, the visibility and aesthetic presentation of the building has been severely compromised by existing modifications to the station, with only minimal modified view lines remaining available.

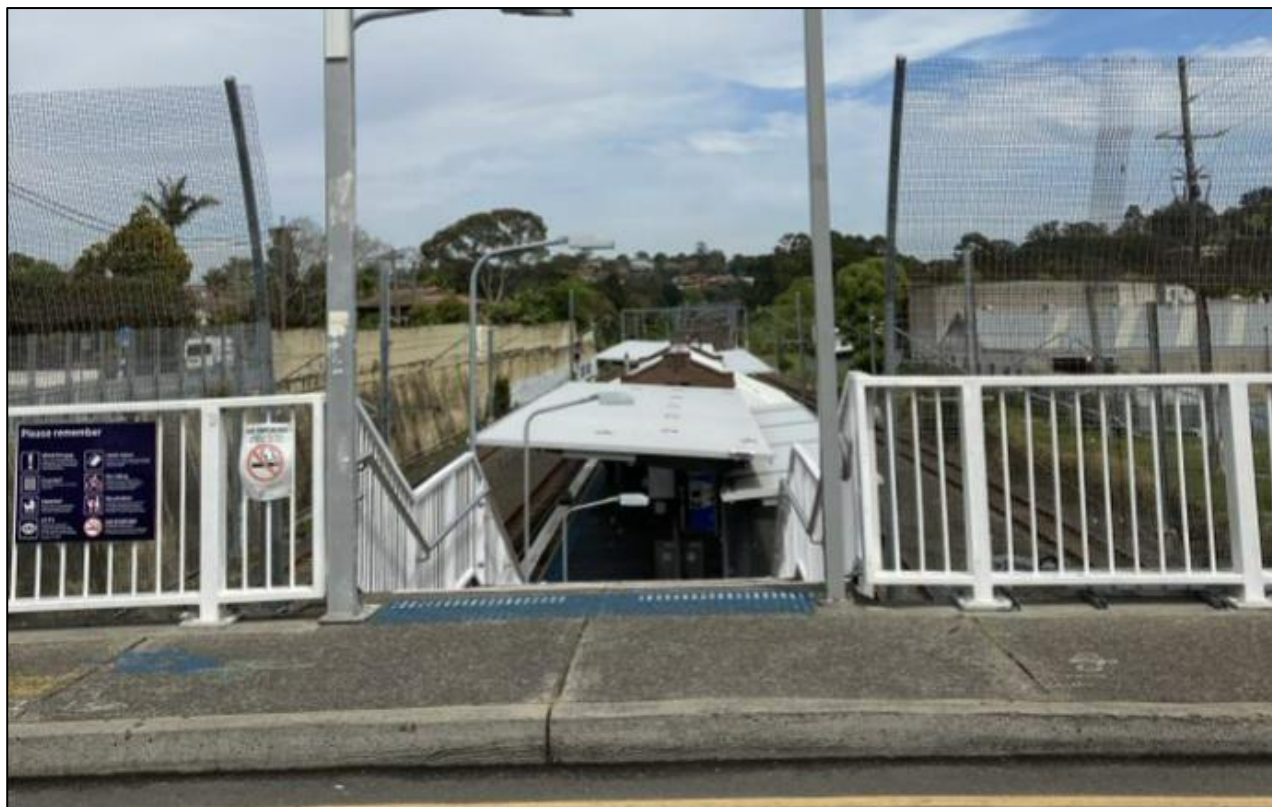


Photo 4.1 Turrella Station entrance from Reede Street Overbridge



Photo 4.2 View to Turrella Station from the corner of Reede and Henderson Streets.

4.2 Station Elements

The following description of Turrella Railway Station has been summarised from the SHI citation.

Turrella Station consists of the following elements:

- Station Building (1931) (Type 13)
- Station Island Platform (1931)
- Overbridge and Stairs (modified in 2001)
- Canopies (2012)

The following detailed description for these elements has been summarised from the SHI citation for Turrella Station⁶. No detailed register of moveable heritage items is known to exist for the station group.

4.2.1 Station Building (1931)

Exteriors:

The station building is a four-bay rectangular dark face brick building of standard stretcher bond monochromatic brickwork with some detailing often seen in Interwar Art Deco style architecture (**Photo 4.3** and **Photo 4.4**). It is a Type 13 station building, which was a standard building design used at all stations along the East Hills Line in the 1930s. The heritage listing for Turrella Railway Station Group ties specifically to the station's Interwar Art Deco style and its use of the Type 13 standard station design.

Most station buildings on the East Hill line are five bays in length, compared to the smaller four bay length at Turrella Station. The bays are defined by simple brick engaged piers to the north and south elevations. The roof is clad in corrugated steel and is gabled at east and west ends against the parapets, with hipped awnings to north and south elevations. The awnings have modern fibro cement sheeting to the undersides. The awning forms an integral part of the roof form. *The building has brick stepped parapets at east and west ends, each featuring a projecting moulded brick capping course and three vertical lines of projecting decorative brickwork, presenting subtle art-deco detailing to the east and west elevations.*

The north and south elevations have six timber-framed double-hung windows. Some retain the original six-paned top sashes, or small timber framed windows with frameless glass or glass louvres. The original window openings have bullnose brick sill detailing and both window and door openings have stop chamfered brickwork. Original door openings have terrazzo thresholds. One doorway on the south elevation of the station has been bricked over however retains evidence of the terrazzo threshold. The other doorway retains its original terrazzo however it is partially obscured by the concrete ramp to the doorway. The station retains metal boot scrapers which have been set into concrete ramps to the north and south doorways.

All the doors have been replaced with modern timber flush doors. The building has one modern aluminium framed window and an aluminium framed glazed panel to the ticket window at the east end of the platform.

⁶ NSW State Heritage Inventory 'Turrella Railway Station Group' Accessed 24 August 2021
<<https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?id=4801890>>

Two canopies have been added to the east and west end of the station building (**Photo 4.3**). Constructed in 2012, the two canopies are reverse pitched or ‘butterfly’ design, with a single central column supporting the cantilevered beams stretching over both sides of the platform. Both canopies are split in height, with one side of the canopy set higher on the central columns, giving a staggered appearance to the canopy. The canopy to the west end of the station building is a consistent height. They are set high on the supports, obscuring the parapets to east and west elevation of the station building.

Interiors:

Internally the Station Building is divided into five spaces. From east to west the spaces include the station office and adjoining staff and communications room, the female toilet, the cleaner’s storeroom and the male toilet.

The station office and adjoining staff and communications room are located in the former booking and parcels office and Station Master’s room. Entered from the south elevation, the station office is a small rectangular room with painted masonry walls, original timber batten and cement sheet ceilings and moulded timber cornices. A metal framed ticket window has been added to the east wall of the office (**Photo 4.7**).

The window on the north wall has been removed, and replaced with a smaller metal framed window and air-conditioning unit. The office has a contemporary fit out with desks and office equipment. There is a range of electrical and communications equipment fixed to the walls. A door has been installed into the masonry wall leading from the station office into the former Station Master’s room, which now holds the staff amenities and communications equipment. The floor in the station office is lower than the adjacent room, with a concrete step providing access to the staff and communications room (**Photo 4.8**).

The staff and communications room retains the original timber batten and cement sheet ceiling (**Photo 4.9**). The timber framed sash windows remain to both the north and south elevations. The floors are laminate over a concrete slab. A doorway in the northwest corner of the room, formerly connecting to the entry vestibule of the female toilet, has been enclosed with plasterboard sheet, however the location of the opening is identifiable from the gaps in the skirting boards (**Photo 4.10**). A contemporary kitchenette has been added to the room along with a communications rack and other electrical equipment. Scratches on the wall in both rooms show the layers of paint previously used on the wall (**Photo 4.12**).

The female toilet is accessed from a door on the north elevation of the station building with a small entry vestibule formerly connected to the staff and communications room (**Photo 4.13**). The toilet and entry vestibule both retain the original cement sheet and timber batten ceiling, with moulded timber cornices. The floors are painted cement. The female toilet retains the two original masonry and timber partitions to the cubicles, with modified or replaced timber doors (**Photo 4.14**). The windows to this room retain their timber architraves (**Photo 4.15**), with the original glazing remaining, albeit painted over on two of the three windows (**Photo 4.16**). The toilet and sink have been replaced; however some redundant pipes remain fixed to the walls (**Photo 4.16**).

The male toilet, located at the west end of the building, is fitted out similarly to the female toilet. It also retains the original ceiling, windows and timber architraves and painted cement floors. There are two masonry and timber cubicles, retaining early / original timber doors with four panels (**Photo 4.17**). One of these doors has glazed panels to the upper section of the door, matching the glazing to the windows (**Photo 4.19**). A metal and rendered masonry urinal remains in the male toilet with original cistern located above. As with the female toilet, the toilet has been replaced with a combination of recent and redundant pipes fixed to the walls.

The cleaner's storeroom is located in a small room between the male and female toilets. This is accessed from a timber door on the north side of the station building, and contains modern removable shelving, cleaning supplies and a communications rack. This room retains the original ceiling as seen in the other internal spaces, with timber architraves retained to the window (**Photo 4.20**). The floor in this space is unpainted concrete.

Original elements are found in the male and female toilets, and the cleaners' storeroom, which retain fit out, ceilings, skirting boards and windows from the original building design. The station office and staff and communications room have some original fabric, being the ceiling, a window and some skirting boards, however generally original elements have been replaced with modern fit out and equipment within these spaces.



Photo 4.3 Turrella Station platform and building, viewed from the southeast in the rail corridor

The tall canopies added in 2012 sit almost flush to the parapets of the station building.

Source: Umwelt 2021



Photo 4.4 West elevation of the Station Building, looking east

The parapet of the station building is obscured by the 2012 canopy and the screens to the male toilet have been removed as is evident by the discolouration of brickwork at the door entrance.

Source: Umwelt 2021.



Photo 4.5 East elevation, Station Building

Showing the painted original windows to the female toilet and air-conditioning installed to the station office window.

Source: Umwelt 2021



Photo 4.6 **Boot scraper set into concrete step to the Station Building**

Source: Umwelt 2021



Photo 4.7 **Station Office**

Photo taken looking at the east wall, showing the ticket window, modern desk fit out and examples of the cables fixed to the wall internally.

Source: Umwelt 2021

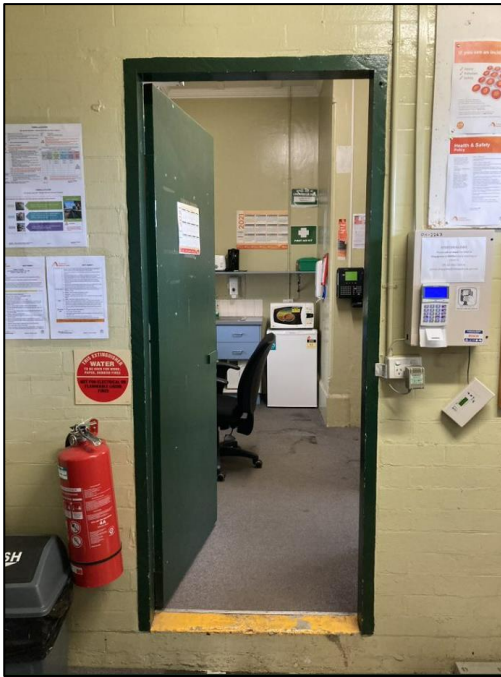


Photo 4.8 **Doorway between the two spaces in the station office**

A door has been cut into the masonry wall between the station office and the former waiting room, with a step between the two rooms due to the difference in floor levels.



Photo 4.9 **Timber batten and cement sheet ceiling with added lattice panel in station building**

The ceiling is an original detail to the station building and is present in all of the internal spaces.

Source: Umwelt 2021

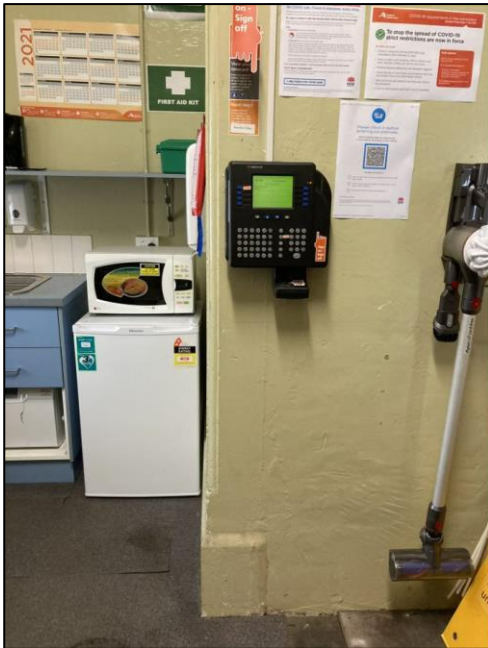


Photo 4.10 Station office and former waiting room

The infilled doorway between the former waiting room and entry vestibule now within the female bathroom is visible at the lower section of the wall

Source: Umwelt 2021

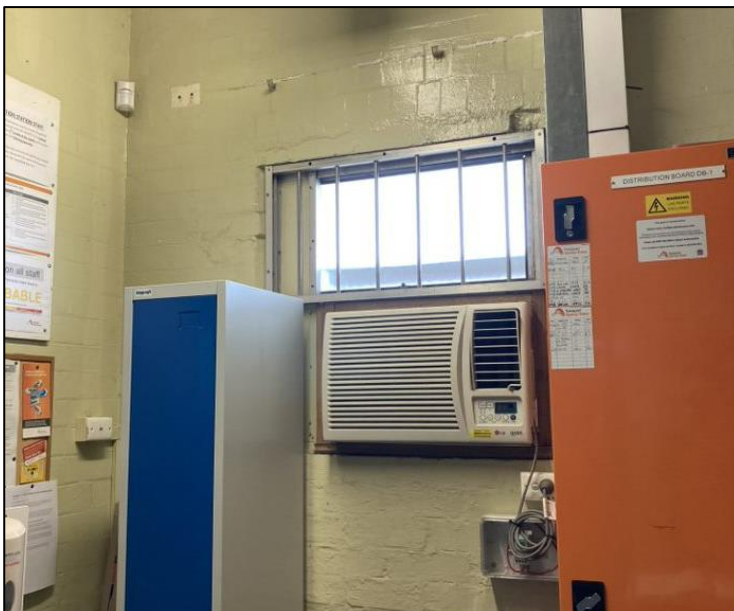


Photo 4.11 Infilled doorway to the station office, with metal window frame and air-conditioning unit

Source: Umwelt 2021



Photo 4.12 Station office paint detailing

A scratched area of wall in the station office showing the different layers of paint colours to the internal space.

Source: Umwelt 2021



Photo 4.13 Entry vestibule to the female toilet

A doorway to the station office has been bricked over, with the doorway and lintel visible in the wall.



Photo 4.14 Female Bathroom cubicle

Source: Umwelt 2021



Photo 4.15 Female bathroom showing the original timber framed sash window



Photo 4.16 Female Toilet

The inside of the toilet cubicles is consistent in both male and female toilets with original windows and a mixture of redundant and current pipes fixed to the wall.

Source: Umwelt 2021



Photo 4.17 Male Toilet, showing the original masonry and timber cubicle

Source: Umwelt 2021



Photo 4.18 Male Toilets, Station Building

Photo showing masonry and metal urinals associated with the original / early modification of the station

Source: Umwelt 2021



Photo 4.19 Original timber door retain glazing to the upper panels



Photo 4.20 Store room within the Station Building

The original ceiling, wall vent and joinery to the window are visible in this image.

Source: Umwelt 2021

4.2.2 Platform (1931)

The platform at Turrella Railway Station is a 158m long brick island platform. It is slightly curved in shape and tapers at each end. The platform retains the original brick face with stepped brick coping to the upper edge. The surface is asphalt with modern tactile indicators added to the edges of the platform.

Contemporary seating, rubbish bins and information points have been installed along the length of the platform. The light poles to the west and east ends (to the east of the overbridge) of the platform are modified original lights, with the luminaries and upper section of the poles having been modified (**Photo 4.5**). The canopies and lights attached to the station building awnings are modern additions to the station.

4.2.3 Overbridge and Stairs (1931/2001)

The Reede Street Overbridge and stairs consists of a concrete bridge structure with central support located on the east end of the Turrella Station Platform. The Overbridge, although original to the station, was heavily modified during the completion for the line quadruplication in 2001.

The stairs from the Reede Street Overbridge are concrete treads and risers on an open trestle metal frame, with concrete columns added at the junction with the Overbridge (**Photo 4.22**). Modifications in 2001 included the replacement of large areas of the stairs with new concrete treads, concrete landing to the upper section and new handrail and lights added to the sides. The steel trestle and outside stringers are all that remain of the original stairway structure. The area below the stairs is exposed, with views to the trestle structure and Overbridge footing available from the east end of the platform. Use of this simple trestle structure for stairs and footbridges was common at stations built in the Interwar and Post-War periods. However, the stairs at Turrella have been heavily reworked, during the quadruplication of the line and other modifications to the station entrance.



Photo 4.21 Reede Street Overbridge and entry stairs to Turrella Station

Source: Umwelt 2021



Photo 4.22 Turrella Station Platform looking west, showing the early light posts to the west end of the platform

The light poles are indicated by red arrows. The luminaries and upper section of the lights has been replaced.

Source: Umwelt 2021.

5.0 Heritage Significance

As identified in **Section 2.5**, the Proposal Area includes the Turrella Railway Station Group, and the public domain adjacent to the station. The public domain to Henderson Street and Turrella Street is not included in the listing for the station group and is not included in any heritage listings or heritage conservation areas. The surrounding public domain has not previously been identified as significant, and so is not considered within this section of the report.

The following section outlines the significance of listed heritage items located within the Proposal Area, being the Turrella Railway Station Group. As heritage items in the vicinity are all physically and visually separated and would not be impacted by the Proposal, discussion of their heritage significance is not included below.

5.1 Assessment of Significance – Turrella Railway Station Group

The Turrella Railway Station Group is listed on the TAHE S170 register for its historical, aesthetic, research potential and representative heritage values. **Table 5.1** contains the significance assessment prepared as part of TAHE S170 heritage and conservation register entry for Turrella Railway Station Group on the NSW SHI.⁷ It is replicated here without change.

Table 5.1 TAHE Section 170 Heritage Significance Assessment

Criterion	Application of Criteria
Criterion A – Historical Significance	<i>Turrella Railway Station is of historical significance as part of the East Hills line, a major Depression period public work undertaken under the controversial Premiership of Jack Lang and through its relationship to the development of the suburb of Turrella and the broader East Hills region. The austere design of the station building is reflective of the completion of the East Hills line as a Depression period unemployment relief works project.</i>
Criterion B – Associative Significance	No associative significance was identified as part of the listing on the TAHE S170 heritage register.
Criterion C – Aesthetic or Technical Significance	<p>Turrella Railway Station is of aesthetic significance as an example of a small Inter-War period suburban railway building. Although the station has been modified through the installation of modern canopies, services and furniture, the station building remains generally consistent with other East Hills line railway station buildings in design and style.</p> <p>The building externally retains its original design elements, and is very austere in style, <i>with Inter-War Art Deco style touches (for example brick strapwork detail to parapets) and is competently executed, exhibiting fine workmanship in its brickwork. The building is noted for its use of monochromatic brickwork, stepped parapets, irregular fenestration and engaged piers.</i> Although the placement of the modern canopies has reduced the visibility and therefore legibility of these significant design elements, it still retains a level of significance under this criteria.</p>
Criterion D – Social Significance	<i>The place has the potential to contribute to the local community's sense of place and can provide a connection to the local community's past.</i>

⁷ NSW State Heritage Inventory 'Turrella Railway Station Group' Accessed 24 August 2021
<https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?id=4801890>

Criterion	Application of Criteria
Criterion E – Research Potential	<i>Turrella Station is of research significance for its ability to demonstrate design and construction techniques of the Inter-War period. The building provides insights into NSW Railway's experimentation with styles of architecture and adaptation to Depression period economic conditions.</i>
Criterion F – Rarity	<i>The Turrella Station building is not rare, as it is part of a cohesive group of 10 similar to identical Inter-War suburban station buildings completed in 1931 between Turrella and East Hills.</i>
Criterion G – Representativeness	<p>Turrella Railway Station is a good, representative example of an East Hills line railway station, with the platform and station building generally intact. However the modern canopies introduced to the station in 2012 obscure views of the station building, and demonstrates the effects of the economic Depression of 1929-1930s on railway station construction. Although modified with modern canopies installed, the Station is one of the least modified along the East Hills Line, with accessibility upgrades and other modifications having been made to the majority of other stations along the line as Transport for NSW improve accessibility to stations across the network.</p> <p>Turrella Railway Station is representative of the cohesive collection of East Hills line railway stations from Turrella to East Hills.</p>

5.2 Statement of Significance

The following statement of significance is included in the SHI inventory citation for Turrella Railway Station Group.

Turrella Railway Station - including the 1931 platform and station building - is of local heritage significance. Turrella Railway Station is of historical significance as a major public work completed as an unemployment relief project during the Great Depression, and as a major transport hub for the suburb of Turrella since 1931. Turrella Railway Station building is of aesthetic significance as an austere 1930s railway building with simple Art Deco detailing and fine brick workmanship that is evocative of the effects of the Depression on building programs for large organisations such as the NSW railways⁸.

Turrella Railway Station generally remains representative of the cohesive collection of ten East Hills line railway stations from Turrella to East Hills. It is noted, however, that modifications undertaken to the station obscure key elements of the station building, and impact the legibility of the aesthetic significance of the Station Group.

5.3 Significance Gradings

Different components of a place can contribute to its heritage value in different ways. This includes not just the fabric of the item, but its setting, views and the grouping of individual elements. Elements can be graded on how they individually contribute to the overall heritage values of a place. The gradings of significance developed by the heritage council of NSW have been adapted for use in this report (Table 5.2).

⁸NSW State Heritage Inventory 'Turrella Railway Station Group' Accessed 24 August 2021
<<https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?id=4801890>>

Table 5.2 Gratings of Significance Definitions

Grading	Significance
Exceptional (E)	Elements of exceptional significance are rare or outstanding elements that directly contribute to the place's overall heritage significance. They demonstrate a key aspect of the place's overall heritage significance and should be retained and conserved in-situ. Any change should be minimal and retain significant values or fabric.
High (H)	Elements of high significance have a high degree of original fabric. They demonstrate a key aspect of the place's overall heritage significance and should be retained and conserved. Retention should be considered in-situ. Minor change is allowed so long as significant values and fabric are retained and conserved.
Moderate (M)	Elements of moderate significance have been altered or modified or do not demonstrate a key aspect of the significance of the place. They contribute to the place's overall heritage significance; however, change is allowed so long as it does not adversely affect values and fabric of exceptional or high significance.
Little (L)	Elements of little significance do not substantially add to the significance of the place in a positive way, though neither do they detract from its overall significance. Elements of little significance may also reflect fabric that may have been substantially altered or modified or may reflect non-significant phases of development. Changes are allowed so long as it does not adversely affect values and fabric of exceptional or high significance.
Neutral (N)	Elements identified as neutral do not contribute or detract from significance. The attribution of 'neutral' typically applies to introduced new or utilitarian fabric that does not relate to a significant historical period or use. Changes are allowed so long as they do not impact on associated fabric of higher significance.
Intrusive (I)	Intrusive elements are damaging to the place's overall heritage significance; they should be considered for removal or alteration.

An examination of the elements of Turrella Railway Station Group and a review of the significance assessment contained in **Table 5.1** have provided the background to provide a grading of contributory elements within the Railway Station Group. **Table 5.3** below provides the grading of contributory elements present at Turrella Station.

Table 5.3 Grading of Contributory Elements

Element	Grading	Discussion
Island platform: form, layout, brick coping (excluding modern asphalt surface)	Exceptional	The island platform is of exceptional contributory significance as part of the original station design. It is consistent with the standard station design across the East Hills line and provides tangible evidence for the historical and aesthetic significance of Turrella Railway Station Group (TRSG). This excludes the modern asphalt surfaces which has been added to the station platform.
Station Building – form, fabric and design elements (excluding modern fit out)	High	<p>The Station Building is of high contributory significance as an original, albeit modified element of the station design. Although some modifications have occurred, such as the infill of doorways and installation of modern fitout and cable routes, the station building still conforms to the standard Type 13 station building design, and displays the key Art-Deco design elements.</p> <p>Intact elements from the original station design include the ceiling, timber window frames and glazing and the timber and masonry partitions to both male and female toilets.</p> <p>The station building and the original internal fabric it contains contributes to the aesthetic and historical significance of the TRSG.</p>

Element	Grading	Discussion
Stairs from Reede Street Overbridge	Little	The stairs from the Reede Street overbridge are a heavily modified original element of the station group. They provide some evidence for the former stairs, retaining a similar simple trestle structure common across Interwar and Post War stations, however the level of modification and utilitarian nature of the structure can provide a little contribution to the overall significance of the TRSG.
Freestanding Station lights	Little	The freestanding station lights at the west end and extreme east end of the station building retain the early base design to the light poles. However, as the upper sections of the lights have been heavily modified, they are of little contributory significance to the TRSG.
Reede Street Overbridge	Neutral	The Reede Street Overbridge, although part of the early station design, has been heavily modified and does not provide any contribution to the overall heritage significance of the TRSG.
Station Building - modern internal fit out	Neutral	The modern internal fit out within the station group is a utilitarian addition to the internal spaces and is not associated with key historical periods of development at the station. It does not offer any contribution to the overall heritage significance of the TRSG.
Platform surface and furniture including seats and rubbish bins	Neutral	<p>The asphalt platform surface is a replacement of the original concrete deck which has been installed and likely replaced during one of the phases of upgrades to the station. This does not detract from any of the significant station elements nor does it contribute to the overall heritage significance of the TRSG.</p> <p>The existing station platform furniture, including timber seats and rubbish bins are recent additions to the station curtilage. They do not detract from or obscure any significant elements of the station group, nor do they provide any contribution to the overall significance of the TRSG.</p>
Platform canopies (2012 additions)	Intrusive	<p>The two contemporary canopies installed adjacent to the Station Building are of an incompatible scale and design which reduces the prominence of the Station Building within the setting of the station group. They have been placed at a height that obscures views of the significant station building, particularly at the east and west ends of the building where the parapet displays key Inter War Art Deco details, for which the station building is considered to be of aesthetic significance.</p> <p>The 2012 platform canopies are therefore considered to be Intrusive elements within the TRSG.</p>

5.4 Historical Archaeological Potential

Historical archaeology in Australia generally relates to the study of the past using physical evidence in conjunction with historical sources. Historical archaeology is generally defined as comprising the period since European arrival in Australia in 1788. An archaeological resource is the physical evidence of the past and may comprise sub-surface evidence including building foundations, occupation deposits, features and artefacts. Archaeological resources are irreplaceable and have the potential to contribute to our knowledge and understanding of early history using information that is unavailable from other sources (DUAP 1996:2). As outlined in **Section 2.1**, archaeological remains or ‘relics’ are defined as:

deposit, object or material evidence that is:

- *related to the settlement of the area that comprises NSW, not being Aboriginal settlement*
- *of State or local heritage significance.*

As discussed in **Section 3.1** the first European settlement in the area was by emancipist Reuben Hannam, who received 100 acres of land in 1819. This land was gradually cleared for farming and orchards, with areas of subdivision occurring in the 1850s. The Proposal Area remained within the name of Hannam in Parish Maps from 1899, with little information available about the use of this land beyond farming.

The operation of farms would generally have included construction of fence lines and sheds or other ancillary structures as necessary within the landscape. Historical archaeological remains of these structures would not be substantial structures or foundations; rather if surviving it would consist of post holes or shallow foundations. No substantial dwellings or domestic structures are recorded as being built in the Proposal Area during this phase of historical development.

The construction of the East Hills line in the 1929-1930 would have required widescale clearing, leveling of the terrain and demolition of any orchards, farms and associated fence lines and sheds along the length of the rail alignment, including within the Proposal Area. This would have resulted in the complete removal of any historical archaeological remain associated with early land use within the Proposal Area.

The construction and ongoing use of Turrella station has included the installation of necessary subsurface infrastructure, such as stormwater management, sewerage, electricity cables and water. As these have been upgraded or replaced, redundant services or subsurface infrastructure may have been left in situ, below the existing ground surface both on the platform and within the rail corridor. Remains of subsurface infrastructure at Turrella Station would not meet the definition of historical archaeological remains or 'relics' as defined by the Heritage Act.

6.0 The Proposal

6.1 Scope of Works

The following scope of work, and subsequent assessment of impacts in **Section 7.0**, have been prepared in reference to the following design documentation:

- 150447-TRL-PM-RPT-00001 – Turrella Station Upgrade Design Report, System Definition Review Submission, Prepared By Aurecon for Transport for NSW, Revision A, dated 27/8/2021
- 150447-TRL-AR-DRG – TAP 3 Turrella Station Upgrade – Architectural Design Package, System Definition Review Submission, drawn by Keira M-K (Aurecon/DesignInc), Revision A, Dated 27/8/2021. This includes the architectural drawings outlined in the tables below.

ARCHITECTURAL DRAWING LIST	
DOCUMENT NUMBER	DRAWING TITLE
000 - GENERAL	
150447-TRL-AR-DRG-00001	COVER SHEET
150447-TRL-AR-DRG-00002	DRAWING LIST
150447-TRL-AR-DRG-00005	NOTES, SYMBOLS & ABBREVIATIONS
150447-TRL-AR-DRG-00070	3D VIEWS
100 - CONTEXT	
150447-TRL-AR-DRG-00110	DEMOLITION SITE PLAN
150447-TRL-AR-DRG-00120	PROPOSED SITE PLAN
200 - GA PLANS	
150447-TRL-AR-DRG-00210	PLATFORM PLAN - ZONE 1
150447-TRL-AR-DRG-00211	PLATFORM PLAN - ZONE 2
150447-TRL-AR-DRG-00220	STREET LEVEL PLAN - ZONE 1
150447-TRL-AR-DRG-00230	STREET LEVEL ROOF PLAN - ZONE 1
150447-TRL-AR-DRG-00250	PLATFORM REFLECTED CEILING PLAN - ZONE 1
250 - GA SECTIONS & ELEV	
150447-TRL-AR-DRG-00260	ELEVATIONS - SHEET 01
150447-TRL-AR-DRG-00261	ELEVATIONS - SHEET 02
150447-TRL-AR-DRG-00270	SECTIONS - SHEET 01
150447-TRL-AR-DRG-00271	SECTIONS - SHEET 02

ARCHITECTURAL DRAWING LIST	
DOCUMENT NUMBER	DRAWING TITLE
600 - VERTICAL CIRCULATION DETAILS	
150447-TRL-AR-DRG-00600	STAIR - PLANS AND SECTIONS
150447-TRL-AR-DRG-00610	LIFT PLANS
150447-TRL-AR-DRG-00615	LIFT - ELEVATIONS
150447-TRL-AR-DRG-00616	LIFT - SECTIONS
150447-TRL-AR-DRG-00630	STREET LEVEL CANOPY - PLANS
150447-TRL-AR-DRG-00645	STREET LEVEL LANDING - SECTION DETAILS
150447-TRL-AR-DRG-00655	PLATFORM CANOPY - SECTION DETAILS
150447-TRL-AR-DRG-00670	RAMP - PLANS & SECTIONS
700 - INTERIORS	
150447-TRL-AR-DRG-00701	PLATFORM BUILDING - DEMOLITION PLANS
150447-TRL-AR-DRG-00702	PLATFORM BUILDING - PROPOSED PLANS
150447-TRL-AR-DRG-00705	F.A.T AND CLEANERS
150447-TRL-AR-DRG-00706	STAFF AND AMBULANT TOILET
900 - SCHEDULES	
150447-TRL-AR-DRG-00990	MATERIAL BOARD

6.1.1 Station Upgrade

The station upgrade works include:

- upgrades to the existing footpath between Reede Street, Turrella Street and the station entrance on the Reede Street Overbridge
- demolition of the existing stairs connecting the station platform to Reede Street Overbridge and replace with a new stairs
- installation of a new lift and associated lift landing between Reede Street Overbridge and the station platform
- demolition of the existing modern butterfly canopy to the east end of the station building and replacement with a new canopy between the new lift to the existing station building.

To maintain existing pedestrian access to the station during construction of the new station entry, temporary stairs would be constructed to allow continued access to the station platform. The design and staging of the temporary access would be determined during the detailed design phase of the Proposal.



Figure 6.1 Proposed station layout (subject to detailed design)

Source: Cambium Group, October 2021

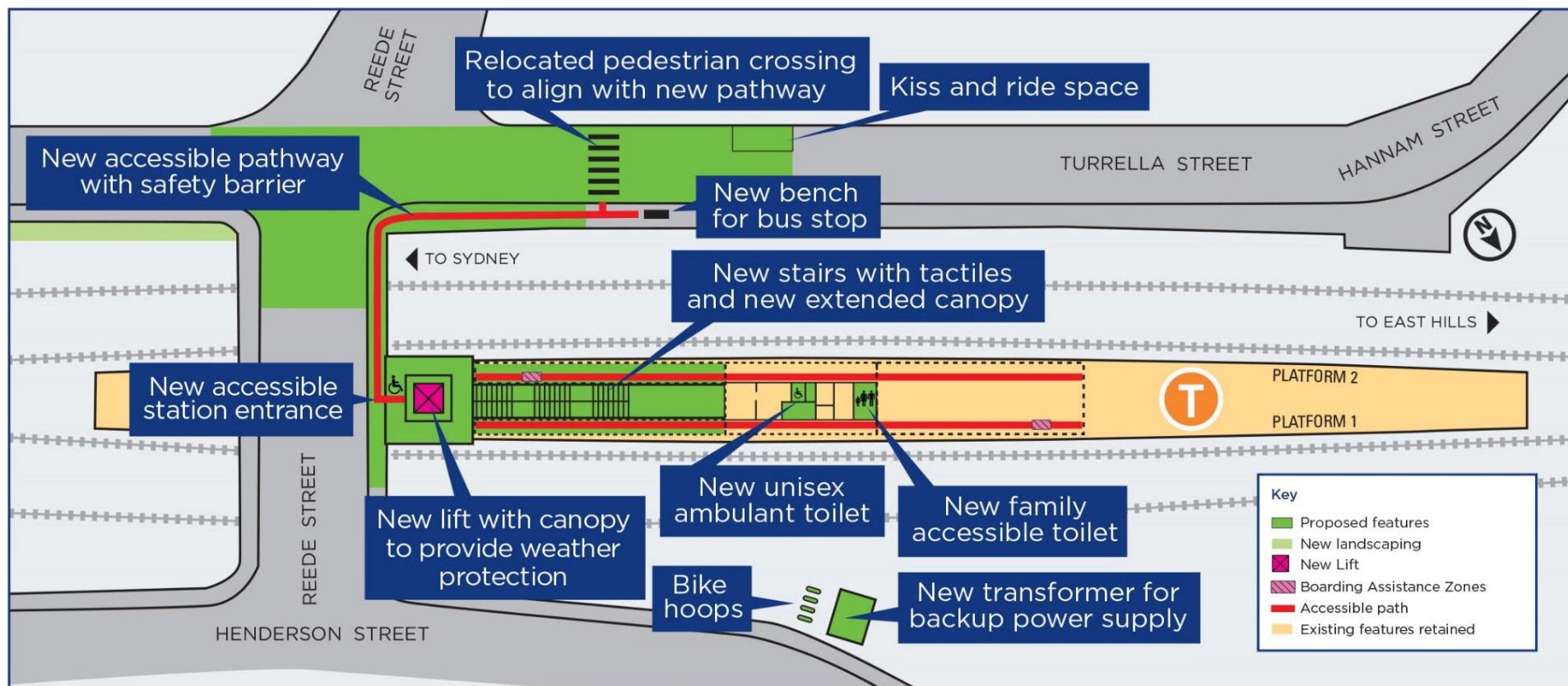


Figure 6.2 Plan showing the proposed new layout of the Proposal Area

Source: Transport for NSW



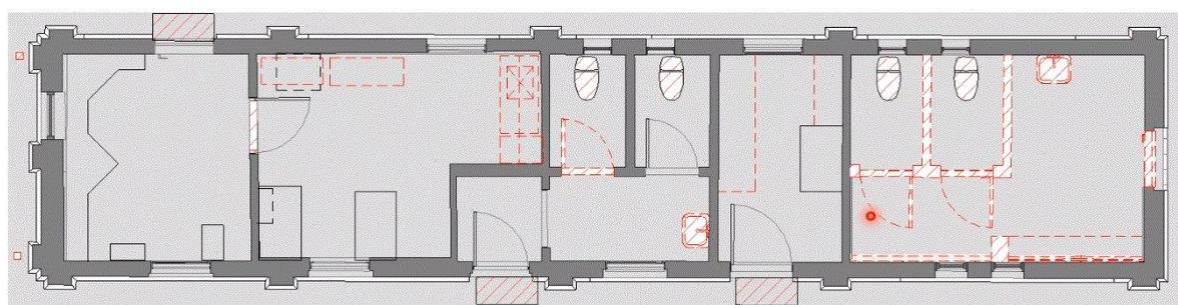
Figure 6.3 Indicative rendering of the Proposal at Turrella Station

Source: Transport for NSW

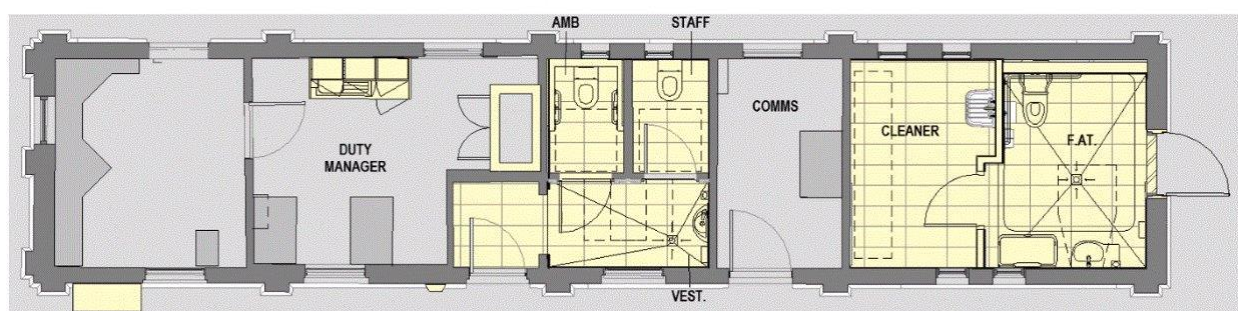
6.1.2 Station Building Modifications

The Proposal includes the following modifications to the station building:

- refurbishment of existing male toilets located within the station building to create a new family accessible toilet. This work would include the demolition of existing toilets, installation of fittings, finishes, services connections, bathroom fixtures (including toilets, sinks, and a changing table), plumbing and widening of the existing brick opening to allow for a new compliant door
- designation of a new single staff toilet to be located in the existing female toilets
- construction of a new unisex ambulant toilet at the location of the existing female toilets. This would include the demolition of existing internal partitions and fittings and installation of new fittings, fixtures, finishes, and services connections
- relocation of the communications room to the existing cleaning room
- relocation of the cleaning room to the existing male bathroom. This would include the removal of existing internal partitions, fixtures and fittings and the installation of service connections, fixtures and fittings.



EXISTING LAYOUT



PROPOSED LAYOUT

Figure 6.4 Plan showing the existing (top) and proposed (bottom) layout of Turrella Station Building

Source: Transport for NSW

6.1.3 Interchange Facilities

The following upgrades to the interchange facilities provided at Turrella Station are proposed:

- relocation of the road centreline along Turrella Street to provide space for the proposed kiss and ride bay
- modifications to the median strip on Reede Street and provision of new road line markings to allow vehicles to turn left onto Reede Street from Turrella Street more safely and reduce the likelihood of a vehicle mounting the kerb
- relocation of the pedestrian crossing on Turrella Street approximately 10m west to allow easier access from the kiss and ride bay
- construction of a new ramp and path located near the corner of Turrella Street and Reede Street including the installation of new compliant handrails and traffic barriers.

6.1.4 Ancillary Work

The following ancillary work is required to take place at and around the station to facilitate accessibility upgrades:

- relocation of services including lighting and communications systems (e.g., CCTV), stormwater drainage, retaining walls, and overhead wiring

- improvements to station power supply which may include an upgrade to the existing transformer or the installation of a padmount substation, and earthing/bonding provisions (specific power requirements to be determined during detailed design)
- improvements to station security and communication systems, including CCTV modifications, public address system upgrades, modification to station passenger information systems and new hearing induction loops within the station platforms
- new wayfinding signage in relation to the new lifts and parking spaces
- regrading and resurfacing of the station platforms to provide compliant paths of travel between the lift, boarding assistance zones, family accessible toilet and other facilities on the platforms
- lighting upgrades
- relocation of station furniture and rubbish bins
- temporary site compounds for storage of materials and equipment
- temporary work (where required) during construction in order to maintain existing pedestrian 'level of service' and access to the station
- relocation and protection of existing underground services including sewer, telecommunications, water, power and gas.

6.2 Options Analysis – Lift and Stair Design

During the development of the reference design Transport for NSW (and their design team) undertook an options analysis to identify the best design solution for providing Disability Discrimination Act (DDA) compliant access to Turrella Station. The different options, outlined in the design report prepared by Aurecon (27 August 2021), were required to provide a design outcome that '*respected the existing heritage building while tying in with the more contemporary surroundings*'⁹. The preliminary options investigated providing additional DDA access to Wolli Creek Reserve from Henderson Street as well as relocating the primary access point away from the Reede Street Overbridge. Figure 6.5 shows a selection of preliminary options considered.

⁹ Aurecon, TAP 3 Turrella Station Upgrade Design Report, prepared for Transport for NSW. Revision A, dated 27 August 2021.

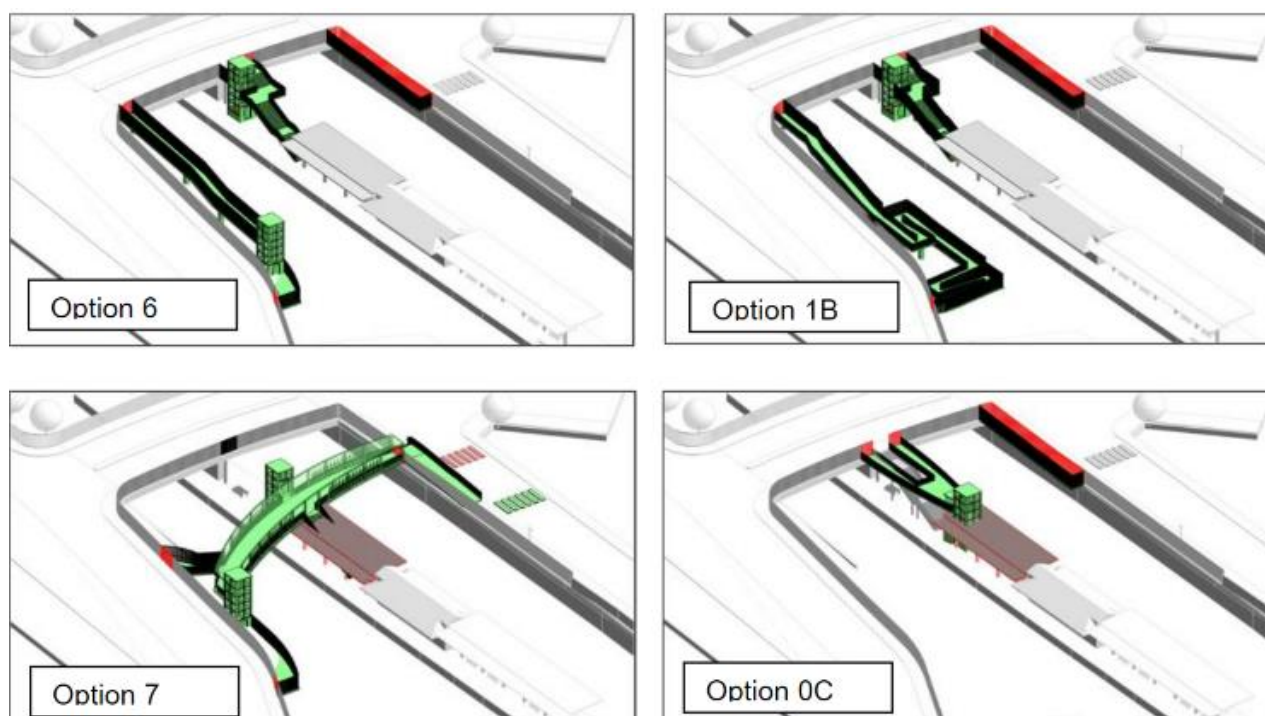


Figure 6.5 Selection of preliminary options for the Proposal

Source: Aurecon

Following stakeholder review, a second round of options analysis was undertaken, centred around improvements to the existing entrance from the Reede Street Overbridge. This identified option 0D and 0C (**Figure 6.6**). Option 0C retained the existing stairs, with a two sided overbridge extending west from Reede Street Overbridge to connect to a lift located midway between the station building and Reede Street Overbridge; this was based on design options used elsewhere, including Hazelbrook Station.

Option 0D would construct the new lift directly off the overbridge, with a lift landing on all sides of the new lift, and stair access from the west side of the landing. This design is modelled off the design approach to Bexley North Station, located two stops west on the East Hills line.

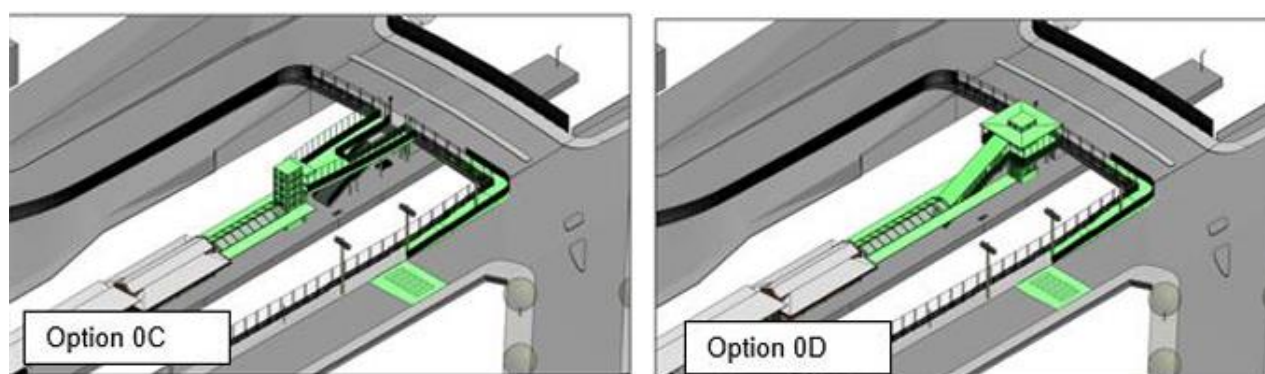


Figure 6.6 Plan showing Options 0C and 0D

Source: Aurecon

Option 0D was identified as the preferred option based on the following advice provided by GML Heritage (27 August 2021) and Transport for NSW heritage specialists.

- By setting the lift structure at a maximum distance from the single-storey station building, the scale difference would be reduced, thereby minimising adverse physical and visual heritage impacts¹⁰.
- Option 0D has previously been implemented along the East Hills line. The consistent treatment of Depression-era stations along the same railway line was considered imperative by Transport for NSW heritage experts, in order to retain the design of the stations along the line as a cohesive group¹¹.

¹⁰ GML Heritage Pty Ltd '21-0148Amhm1 – Transport Access Program, Turellia Station Upgrade Works – SDR Stage – Heritage Review' prepared for Design Inc, 27 August 2021.

¹¹ GML Heritage Pty Ltd '21-0148Amhm1 – Transport Access Program, Turellia Station Upgrade Works – SDR Stage – Heritage Review' prepared for Design Inc, 27 August 2021.

7.0 Impact Assessment

The purpose of this section is to assess the potential for the Proposal to impact on the heritage values of the Turrella Railway Station Group and any heritage items in the vicinity of the Proposal.

7.1 Impacts to Turrella Railway Station Group

Table 7.1 below provides an assessment of the impacts of the Proposal on the heritage significance of the Turrella Railway Station Group.

Table 7.1 Assessment of Impacts to Turrella Railway Station Group

Scope of Works	Assessment of Impact
Station Upgrades	
Upgrades to the existing footpath between Reede Street, Turrella Street and the station entrance on the Reede Street Overbridge	<p>Upgrades to the footpath within the public domain surrounding the station would occur in association with contemporary, non-significant fabric that is located outside of the curtilage of the TRSG.</p> <p>All of these works would be at-grade, and would result in a standard public domain appearance common to comparable settings.</p> <p>Ultimately, the works would improve the accessibility and amenity of the station in line with contemporary standards.</p> <p>Overall, this component of the works would not result in any physical or visual impacts to the TRSG.</p>
Demolition of the existing stairs connecting the station platform to Reede Street and replacement with a new covered stairs	<p>As shown in Photo 4.2, the existing stairs providing access to the station from the Reede Street Overbridge is vernacular in its design and construction; it presents as a typical, standardised set of stairs common to train stations within the local and regional area. The heritage significance of the stairs is assessed in this report to be 'little', predominately based on the extent to which the stair has been modified over time.</p> <p>The stairs would be replaced in entirety at the east end of the platform, located slightly further to the west. The proposed new stairs would be of a slightly narrower width to allow access to the proposed new lift shaft, the stairs would be concrete treads similar to existing with an open steel structure below. The new stairs would be enclosed with a canopy for weather and sun protection, with anti-climb screens to the sides.</p> <p>The overall design is relatively similar to the existing stairs, with the major proposed change being the introduction of the canopy.</p> <p>As the existing stair does not form part of any significant views or vistas to or from the station (rather, it presents as a utilitarian and expected element within the immediate context of the station) modifications to its appearance would not significantly alter the overall presentation of the station, which has already been modified by contemporary changes (particularly those undertaken in 2001 and 2012).</p> <p>The new canopy would somewhat limit the visibility of the station building when entering the station from Reede Street, however it is noted that the visibility of the building from this vantage point is already significantly compromised and obscured by the existing 2012 canopy and the relative scale of the station building compared to surrounding structures. The design seeks to improve on the existing visual impacts with the west end of the proposed canopy to the stairs open, allowing views through the canopy to the upper parapet wall of the station building.</p>

Scope of Works	Assessment of Impact
	<p>Overall, the proposed new stairs and associated canopy do represent a visual change within the context of the station, but given the already compromised visual presentation of the station and its component elements, the visual impacts of this change are assessed to be minor.</p> <p>Ultimately, the introduction of the new stair and canopy would improve accessibility and would enhance the useability of the station by providing additional protection from inclement weather.</p> <p>The proposed design of the new stair and canopy has been selected following a comprehensive consideration of alternate options, and based on heritage advice provided by GML (27 August 2021) it has been determined that this design option presents the least amount of heritage impact whilst achieving the desired project objectives (refer to Section 6.2).</p>
<p>Installation of a new lift and associated lift landing between Reede Street and the station platform</p>	<p>The new lift and landing at the east end of the station would introduce a relatively large new structural element to the station group.</p> <p>This work would require demolition of a small section of the platform, for the construction of the lift core. This would be located near the centre of the platform, away from the brick facing and coping, and would remove the non-original platform surface and fill from the platform. This would not require physical intervention to the station building or significant parts of the station platform (being the brick face and coping, general form and alignment). The proposed scope of works would not result in physical impacts to fabric of significance.</p> <p>It is acknowledged that the proposed new lift, landing and canopy would be of a larger scale than existing structures that are present, including the Overbridge. However, the structure would be located to the east end of the station and away from the station building, resulting in an appropriate degree of visual and physical separation. The new lift and landing (with canopy) would be located in proximity to large scale elements, such as the Overbridge and stair, rather than in proximity to smaller scale elements such as the station building.</p> <p>The proposed design of the new lift, landing and canopy has been selected following a comprehensive consideration of alternate options, and based on heritage advice provided by GML it has been determined that this design option presents the least amount of heritage impact whilst achieving the desired project objectives (refer to Section 6.2).</p> <p>At present, views of the eastern elevation of the station building are still available from the Reede Street Overbridge; the proposed new lift structure and associated landing/canopy would obscure direct views from the centre of the bridge and atop the stairs / lift landing. It is noted however, that the existing 2012 canopy has already obscured views to this elevation to some extent. Oblique views to the station from the overbridge would remain available, with the upper section of the east parapet wall and the north and south elevations visible.</p> <p>The newly proposed works would therefore result in a relatively limited degree of additional visual impact, noting that views to the eastern elevation of the station building are already compromised and currently are not particularly contributory to the heritage significance of the station group.</p> <p>The new lift landing would be screened on three sides with 3m high safety screens. These screens would also extend the length of the overbridge to either side of the lift landing. As the materiality of the proposed screen has not been confirmed the extent of visual impact is unknown. However, it is</p>

Scope of Works	Assessment of Impact
	<p>assumed that the screens would be visually permeable, meaning that views through them would remain available, albeit in a clearly altered state.</p> <p>Given the extent to which these views have already been compromised by contemporary changes (particularly being the introduction of the canopies in 2012), and the visual and physical distance being maintained between the proposed new structures and the station building, this would result in a minor degree of additional visual impact.</p> <p>The proposed new lift and associated landing to the Reede Street Overbridge would have a minor adverse impact on the TRSG.</p>
Demolition of the existing platform canopy and replacement with a new canopy extending from the lift to the existing station building	<p>The existing canopy at the east end of the station was added in 2012 and spans approximately 20m from the east end of the station building to near the entry stairs, running down the centre of the platform. It is an intrusive element within the setting of the station group and obscures significant views to the station building from the east. The canopy is not physically connected to the station building; removal of the canopy would therefore not require intervention to significant elements of the station building and would not have an adverse physical impact.</p> <p>The proposed new canopy at the platform level would extend the full length of the east end of the platform, connecting to the lift and new covered stairs at the east end. A 600mm gap at the west end of the canopy is proposed between the canopy and the station building. The installation of the new canopy would therefore not physically impact the station building.</p> <p>The proposed new canopy would be a larger structure than the existing in both length and width. However, the central section of the awning, with a low gable profile, would be constructed of glazed panels supported by a metal structure. The glazed panels would allow partial views to the station building as passengers descend into the station. Although the full expression of the east elevation, and art-deco detailing would still largely be hidden by the canopy's roofline horizontal supports, the glazed panel and lower height of the proposed new canopy would reinstate some views to the parapet of the station building which are currently hidden.</p> <p>The proposed works would remove an intrusive element to the station group, with the replacement canopy designed so as to provide better, albeit still partially obstructed, views of Interwar Art Deco detailing to the eastern elevation of the station building. The proposed canopy would not have any impacts to significant fabric however would have a minor adverse visual impact and overall minor impact on the TRSG.</p>
Construction of temporary access stairs during construction works. The design and staging of the temporary access would be determined during detailed design phase of the Proposal.	<p>It is not anticipated that the temporary access stairs would require interaction with the significant fabric of the station group. Any visual impact of the proposed access stairs would be temporary and their installation entirely reversible.</p> <p>Given the temporary and reversible nature of the works, the temporary access stairs would not have an impact on the significance of the TRSG.</p>
Station Building Modifications	
Refurbishment of existing male toilets located within the station building to create a new family accessible toilet. This work would include:	<p>The male toilets within the Turrella station building retain several original elements, including the masonry and timber cubicle partitions, masonry and steel urinal with original cistern, timber architraves and glazing to the windows and painted concrete slab flooring.</p> <p>The proposed works for the installation of the family accessible toilet would include the complete removal of the original elements of this space, including partitions and urinal from the 1930s construction of the building.</p>

Scope of Works	Assessment of Impact
<ul style="list-style-type: none"> • demolition of existing toilets and fittings • installation of fittings, services connections, bathroom fixtures (including toilets, sinks, and a changing table) • plumbing and • widening of the existing brick opening to allow for a new compliant door 	<p>Installation of new service connections and plumbing would also require penetrations to the original structure of the building, with any associated ventilation works requiring removal of sections of the original ceiling. The new family accessible toilet would be in a reconfigured into a smaller space, allowing for the proposed new cleaners room at the east end of the existing toilet.</p> <p>The Proposal would completely remove evidence of this component of the station building's original design, both through the removal of the original fit out and reconfiguration of the space. This would result in a major adverse impact to the station building. Penetrations to the original finishes to the walls and ceilings would result in a minor adverse physical impact.</p> <p>Widening of the existing opening in the brick wall would require removal of original brickwork to areas of the west elevation. The works would be limited to the opening and would not require removal of large areas of brickwork, but would require the removal of a small amount of original fabric (brickwork). This would have a minor adverse physical impact.</p> <p>The widening of the door would also alter the scale of the opening in comparison to the detailing on the west elevation and the remaining unaltered openings to the station building. This would have a minor visual impact.</p> <p>The replacement of the door would not result in any adverse impacts as the existing door is contemporary and non-significant.</p> <p>Overall, the proposed refurbishment works of the male toilet would have a moderate adverse impact to the TRSG. This is associated with the reconfiguration of the space and removal of the original elements with the space. It should be noted that these works would be confined to the male toilets and would not impact on the presentation and aesthetic significance of the TRSG as a whole.</p>
<p>Relocation of the cleaning room to the existing male toilet. This would include:</p> <ul style="list-style-type: none"> • removal of existing internal partitions, fixtures and fittings <p>installation of service connections, fixtures and fittings.</p>	<p>The relocation of the cleaner's room to the east half of the male toilet would require the re-configuration of walls and openings within the station building. Although some modifications have occurred to the fixtures within the internal spaces, the configuration of the station building has remained largely consistent since construction in 1931.</p> <p>Reconfiguration of the west bay of the station building, where the male toilet is located, would have a moderate adverse impact as it would be a small departure from the standard building design used for all stations along the East Hills line, and result in a change in use of the westernmost bay. However, the remaining rooms of the station building would remain generally consistent with the standard design.</p> <p>Installation of new services connections, fixtures and fittings in the new cleaner's room would require penetrations or fixings to the walls, floors or ceilings of the station building. These are generally original elements to the station design. New penetrations within this space have the potential to result in a minor to moderate adverse physical impact, depending on the size and location required, and extent of intervention to original fabric required.</p> <p>The proposed relocation of the cleaning room has the potential to result in a moderate adverse impact on the TRSG. Although the works would require the intervention with significant fabric, these penetrations are likely to be localised and not require total removal of the ceiling or significant elements of the station group. Additionally the proposed works would generally not be visible to the public and would not impact on the</p>

Scope of Works	Assessment of Impact
	significant views of the station building, nor detract from the overall contribution of the building to the significance of the TRSG.
<p>Designation of a new single staff toilet and construction of a new unisex ambulant toilet at the location of the existing female toilets. This would include:</p> <ul style="list-style-type: none"> • demolition of existing internal partitions and fittings and • installation of new fittings, fixtures, finishes, and services connections 	<p>Similar to the male toilets, the female toilets retain the masonry and timber partitions from the original station design. Demolition of the existing internal partitions would remove original elements of the station group. This would have a moderate adverse physical impact</p> <p>Installation of new fittings, fixtures and services would require new penetrations and fixings to the station building walls and ceilings. This would be into areas of original fabric such as the walls and ceilings. This would have a minor to moderate impact, depending on the size and location of penetrations required.</p> <p>The proposed refurbishment works of the female toilet to form a unisex ambulant toilet therefore would have a moderate adverse impact to the TRSG. This impact is associated with the removal of and intervention with the some of the original elements within the existing female toilets. Although this would remove original fabric, it would not compromise the overall contribution of the station building to the aesthetic significance of the TRSG.</p>
<p>Relocation of the communications room to the existing cleaner's room and demolition of kitchenette and other fittings from existing communications room.</p>	<p>Removal of the kitchenette and other modern fittings from the communications room would not result in an adverse visual or physical impact as this constitutes non-original and non-significant fabric.</p> <p>The relocation of communications equipment to the existing cleaner's room would require the installation of new connections and services for the communications equipment. This is likely to require penetrations to the floor, ceiling or walls of the cleaner's room. The cleaner's room retains its original ceiling, skirting, walls and the floor slab for the building. Any new penetrations within this space have the potential to result in a minor to moderate adverse physical impact, depending on the size and location required. The nature of these works would be localised removal of the original fabric within the cleaners' room, however the Station Building generally would retain its ability to contribute to the overall significance of the station.</p> <p>The proposed works have the potential to have a minor adverse impact on the TRSG. This should be confirmed at detailed design stage to ensure greater impacts would not result from the proposed works.</p>
Ancillary Works	
<p>Relocation of and improvement to services including:</p> <ul style="list-style-type: none"> • lighting and communications systems (e.g., CCTV and public address system upgrades) • stormwater drainage • retaining walls • overhead wiring 	<p>Relocation of services within the TRSG has the potential for localised impacts on significant fabric of the station group.</p> <p>New services installed within existing cable routes or where installation does not remove significant fabric or add penetrations or fixings to significant fabric, would not result in adverse impact.</p> <p>However, where new penetrations to the station building or platform face are required, this would have a minor to moderate adverse physical impact, depending on the extent of works required.</p> <p>Introduction of new cable routes to the exterior walls of the station building platform face or rock cuttings may result in a minor adverse visual impact and should be avoided.</p> <p>Detailed design should confirm the extent to which services are required for relocation to determine the level of impact to the TRSG.</p>

Scope of Works	Assessment of Impact
Improvements to station power supply which may include an upgrade to the existing transformer or the installation of a padmount substation, and earthing/bonding provisions (specific power requirements to be determined during detailed design)	<p>Power supply upgrades as part of the Proposal include the installation of a new padmount substation, located at the north boundary of the rail corridor, at the Henderson Street entrance to the rail corridor. This is located to the north of the TRSG, on an embankment opposite the station building.</p> <p>The new substation would be a new rectangular structure, approximately 1.5m high by 2m wide by 2.5m long on a concrete pad footing. This would sit at eye level from the station platform, due to the embankment at the Henderson Street Entrance.</p> <p>The construction of the substation would not require impacts to the significant elements of the station (identified in Section 5.3), however, this would alter the views from the station to Henderson Street. As identified in Section 4.1, limited views to the station building's north elevation remain available from Henderson Street. Construction of the substation in the proposed location would obscure some views of the station building from the north; some views would remain, albeit at smaller scale. This would have a minor adverse visual impact. The works would not have an adverse physical impact.</p> <p>Overall the proposed padmount substation would have a minor adverse impact on the significance of the TRSG.</p>
New wayfinding signage in relation to the new lifts and parking spaces	<p>New wayfinding signage would be located in the public domain and separate from the significant elements of the station. No works would be required to significant fabric of the TRSG.</p> <p>Signage would be of small scale and would introduce minor additional elements to the station along with updates to the present wayfinding signage. This would have a negligible visual impact. New and updated signage would be cohesive in presentation and materiality.</p> <p>The proposed signage would not result in an adverse impact on the heritage significance of the TRSG.</p>
Regrading and resurfacing of the station platforms to provide compliant paths of travel between the lift, boarding assistance zones, family accessible toilet and other facilities on the platforms	<p>Regrading of the existing platform surface would remove the current surface and replace with new asphalt to match existing. The platform surface has been extensively altered since the construction of the station and is not considered a significant part of the platform.</p> <p>The platform surface would be reinstated to match existing upon completion of the works.</p> <p>Overall, the proposed regrading to the platform would not result in an adverse visual or physical heritage impact on the TRSG. The regrading works would result in more level surfaces and enable the installation of compliant paths and accessibility aids.</p>
Relocation of station furniture and rubbish bins	<p>The existing station furniture and rubbish bins are contemporary additions to the station platform and have been graded as neutral. Relocation of these items along the platform is unlikely to require intervention into the significant elements of the TRSG. This would not have an adverse physical impact.</p> <p>Relocation of existing items within the TRSG would not result in significant changes to the setting of the station. This would not have an adverse visual impact.</p> <p>No adverse heritage impact would result from relocation of existing furniture or rubbish bins.</p>

Scope of Works	Assessment of Impact
Temporary construction compound and laydown areas for storage of materials and equipment	<p>The construction compound and laydown areas for storage would be temporary. Any visual impacts associated with this would be temporary, with the works being fully reversible. The proposed construction compound and laydown areas would be located unobtrusively to minimise temporary visual impacts.</p> <p>The temporary construction compound and laydown areas would result in negligible temporary impacts, which would be reversible upon completion of the Proposal.</p>
Relocation and protection of existing underground services including sewer, telecommunications, water, power and gas	<p>The relocation of services using non-destructive search methods would not result in any adverse physical or visual impacts as no physical works are required.</p> <p>Protection works would occur to the services, only if required. Any services exposed for this work would be re-buried (or similar) with associated surfaces made good, to match existing surrounding surfaces. This would not result in any visual impacts and would result in a negligible degree of physical impact.</p> <p>This would not result in adverse impact on the TRSG.</p>
Interchange Facilities	
Relocation of the road centreline along Turrella Street to provide space for the proposed kiss and ride bay and relocation of the pedestrian crossing on Turrella Street approximately 10 metres west	<p>The proposed kiss and ride bay, and associated road line modifications, including relocation of the pedestrian crossing are located outside the boundary for TRSG and are not located within a heritage conservation area. The works would be at-grade, and would result in a standard public domain appearance common to comparable settings.</p> <p>The works would improve the accessibility and amenity of the station in line with contemporary standards.</p> <p>Overall, this component of the works would not result in any physical or visual impacts to the TRSG.</p>
Modifications to the median strip on Reede Street and provision of new road line markings for improving traffic safety.	<p>Modifications to the median strip and associated road line modifications are located outside the boundary for TRSG and are not located within a heritage conservation area. The proposed works, located at ground level, would not require modification or removal of significant fabric, and would not have an adverse physical impact. The works would be consistent with the standard public domain works in the vicinity.</p>
Construction of new ramp and path located near the corner of Turrella Street and Reede Street including the installation of new compliant handrails and traffic barriers	<p>The construction of a new ramp and pedestrian path would occur outside of the Turrella Station group and would not require physical impacts to any significant elements of the station group. It would not alter the setting of the station or impact on views to or from the station.</p> <p>Depending on the height, scale and materiality of new handrails and traffic barriers, there may be small, localised areas that partially interrupt views to the station. Due to the small nature of these impacts, this would likely have a negligible to minor visual impact.</p> <p>The proposed upgrades to the public domain including ramp and footpath are anticipated to have a negligible to minor impact on the TRSG This should be confirmed following completion of the detailed design.</p>

7.2 Items in the Vicinity

The Proposal Area is not located within a conservation area and does not interface with the listed heritage items in the vicinity of the Proposal. The nearest heritage items are located 90m and 330m away from the Proposal Area and are visually separated by buildings and sloping terrain, and do not have discernible visual or physical connections to the Proposal Area. The Proposal would not require physical impacts to any heritage items in the vicinity, and as the surrounding built form provides a visual barrier to the heritage items, the Proposal would not have any adverse visual impacts to heritage items in the vicinity. There would be no adverse heritage impacts to the heritage items in the vicinity as a result of the Proposal.

7.3 Statement of Heritage Impact

The Proposal would have a **minor** adverse impact on the heritage significance of the Turrella Railway Station Group.

The platform and building design and layout were developed using standard design for stations along the East Hills railway line. The reconfiguration of the platform setting and modifications to the internal spaces of the station building would be a departure from the original design, which forms a part of the heritage value of the station. However, as there have been a series of modifications previously which have reduced the integrity of the station group, the changes introduced by the Proposal would be minor within the immediate context of the station.

The Proposal includes the removal of the existing intrusive canopy to the east end of the station, and although a replacement canopy would be larger in length and width, the design articulation of the new canopy would help to partially reinstate views to the east end of the station building and the Art Deco detailing of the building which is key to its aesthetic significance. As the new canopy does not fully reinstate the views, the visual impact would be minor.

As identified throughout this assessment proposed works to the inside of the station building would result in physical impacts to the station building, ranging from minor to major impacts. These impacts are associated with the demolition of the original partitions within the male and female toilets, as well as new penetrations to the ceiling and walls for the proposed toilet upgrades and the relocation of the communications room to the cleaners' room. Overall, the combined impacts of the proposal to original fabric at the station group would have a moderate adverse impact.

As identified in **Table 7.1**, the proposal would result in impacts of variable degree to elements of the Turrella Railway Station Group. These impacts are relative to the contribution of the element within the station group, as well as the contribution of the item as a whole. Cumulatively the impacts outlined in **Table 7.1** would have a minor overall impact on the Turrella Railway Station Group.

Although it is acknowledged the Proposal would be likely to result in minor adverse impacts on the significance of the station group, the Proposal forms part of a suite of accessibility upgrades currently being undertaken or completed on the East Hills line and across the wider Sydney railway network. These upgrades would allow for greater access and use of the station by passengers, ensuring the continued viability of the station for its intended use – a transportation hub for the community and wider region. Therefore, adverse impacts could be considered acceptable as they have been designed to provide a level of consistency with other works previously completed along the East Hills line, and would ensure the ongoing viability of the Turrella Railway Station Group.

7.4 Impacts to Historical Archaeology

As identified in **Section 5.3**, the Proposal Area does not have the potential to retain historical archaeological remains associated with the earlier phases of land use. The ground disturbing works included in the Proposal therefore are unlikely to impact on historical archaeological remains or 'relics'.

8.0 Conclusion and Recommendations

8.1 Conclusions

The Proposal has been assessed as having a minor adverse impact on the Turrella Railway Station Group. This is associated with the alterations to the visual presentation and configuration of the station, and particularly the visual impacts of the new lift, stair and canopy structures. The Proposal would also result in the demolition of the original partitions within the male and female toilets, as well as new penetrations to the ceiling and walls for the proposed toilet upgrades and the relocation of the communications room to the cleaner's room. Those impacts to the original fabric at the station group would have a moderate adverse impact overall.

These impacts are relative to the contribution of the element within the station group, as well as the contribution of the item as a whole. Cumulatively the impacts the Turrella Railway Station Group would have a minor overall impact.

8.2 Recommendations

The following recommendations should be implemented to mitigate or potentially minimise the impacts of the Proposal on the Turrella Railway Station Group.

Detailed Design

The following should be implemented during the development of the detailed design:

- Options should be explored to retain a representative example of the original partitions in one of the station toilets. This should be done in consultation with the Transport for NSW heritage team and heritage consultant to find a sensitive design approach that also meets the BCA and proposal requirements.
- The final design for the toilets and any works to original fabric should be prepared in consultation with a heritage consultant and Transport for NSW. This should include endorsement from Transport for NSW heritage team on the 75% complete design (Critical Design Review [CDR]) to ensure all opportunities to minimise heritage impacts have been considered and incorporated into the design where appropriate.
- The glazing to the toilet windows should have the paint removed and other options investigated to reinstate visibility to the original glazing panes, noting that privacy screening measures will likely be required.
- The design and finishes of new station elements, including the new toilets should respond to the Inter-War architecture and design of the station.
- An appropriate materials and colour palette should be selected for the new elements included in the Proposal. This should be done in consultation with Sydney Trains standard colour schemes for Inter-War railway stations.
- Opportunities to repaint the station elements in accordance with the Sydney Trains standard colour schemes for Inter-War railway stations should be explored within the proposal scope. This provides an opportunity to mitigate some of the impacts of the proposal on the Turrella Railway Station Group.

- Opportunities should be explored to reinstate removed original fabric where possible. This includes the use of sympathetic new elements or replicate the details such as doors and windows as specified in the original designs.
- The Inter-War Art-Deco detailing on the original building should be emphasised or highlighted in the proposed new works. This could include lighting placed to spotlight significant elements or use of similar detailing or patterns in the new work which reflect the original architectural style of the station building.
- Lift canopy structure – options should be explored to reduce the size of the horizontal beam supporting the lift canopy. This would help to minimise the dominance of the lift structure in views to and from the station.
- Enclosed stairway – the detailed design should explore options for high transparency materials to the enclosed canopy to allow better views to the station building as well as to lighten the appearance of the structure against the other proposed modifications to the east end of the station.
- Power supply upgrades – screening of the proposed padmount substation should be considered to reduce the impact on the views to and from the station building. This could seek to use interpretive devices or public art to improve the setting of the north side of the station, near Henderson Street entrance.
- Upgrades to any services should seek to use existing cable routes or penetrations through significant elements of the station group. This should be undertaken in accordance with the Sydney Trains Heritage Technical Note: Installation of New Electrical and Data Services at Heritage Sites.
- Details of the temporary enabling works should be confirmed and reviewed to ensure no additional impacts may result from the required works. This should include reviewed visual and physical impacts.
- Preparation of a heritage interpretation plan in accordance with *Sydney Trains Interpretation Guideline*. This should be implemented as part of the detailed design and construction documentation for the Proposal.

Construction

The following should be implemented during construction phase of the project, including temporary enabling works:

- All project team members, including contractors, should be provided with a heritage induction as part of the general site induction package. This should identify why the Turrella Station Group is significant, their obligations under the Heritage Act 1977 and environmental management process relating to unexpected finds, design revisions identified during construction and protection methods to be used during construction to prevent accidental damage during construction works.
- Significant elements of the station group should be adequately protected during the works. This could include physical barriers, exclusion zones or other methods as appropriate to ensure accidental damage does not occur during the works.
- Regrading of the platform should be completed so that no significant or original features along the platform are impacted. This includes the boot scrapers and the light poles, which are contributory elements to the station group.

- The regrading should also ensure adequate drainage and ventilation to the station building is maintained. This includes ensuring any vents to the lower section of walls are free from debris at the end of the regrading works, and that asphalt or other materials are not built up directly against the station building.
- A Photographic archival recording should be prepared for Turrella Station prior to the commencement of works, including any temporary works or site investigations. This should capture the areas of the station affected by the Proposal.

Section 170 Heritage Act

Section 170 of the Heritage Act and the State Agency Heritage Management Guidelines require a state agency to notify Heritage NSW of the demolition of a heritage asset, or substation portion of that asset.

The proposal includes the demolition of a significant original element, being the removal of the stairs to the station platform and the demolition of the original fitout in the male toilet. Therefore, Heritage NSW should be notified of the proposed works to Turrella Railway Station Group.

9.0 References

Dictionary of Sydney 'Back Huts and Country Estates' accessed 30 August 2021

<https://dictionaryofsydney.org/entry/bark_huts_and_country_estates#footnote-7>

GML Heritage Pty Ltd '21-0148Amhm1 – Transport Access Program, Turellla Station Upgrade Works – SDR Stage – Heritage Review' prepared for Design Inc, 27 August 2021.

NSW State Heritage Inventory 'Turrella Railway Station Group' Accessed 24 August 2021

<https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?id=4801890>

Transport Sydney Trains *Railway Footbridges Heritage Conservation Strategy: Interim Review*, August 2021

