



Australian Government

BUILDING OUR FUTURE



Transport for NSW

Pacific Highway improvements at Hexham (Hexham Straight)

Community update – December 2020



Have your say on the strategic design for the Hexham Straight widening project.

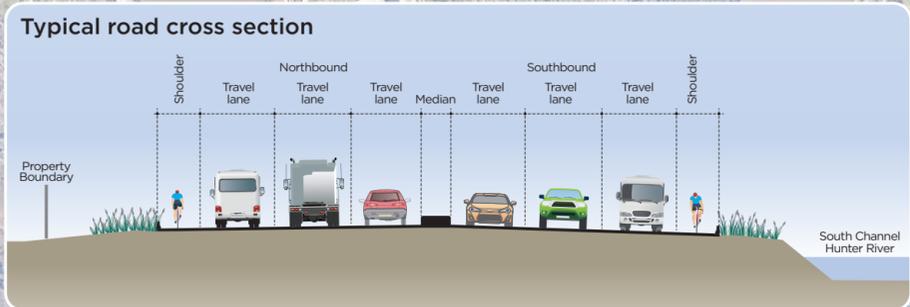
December 2020

Hexham Straight Widening project area

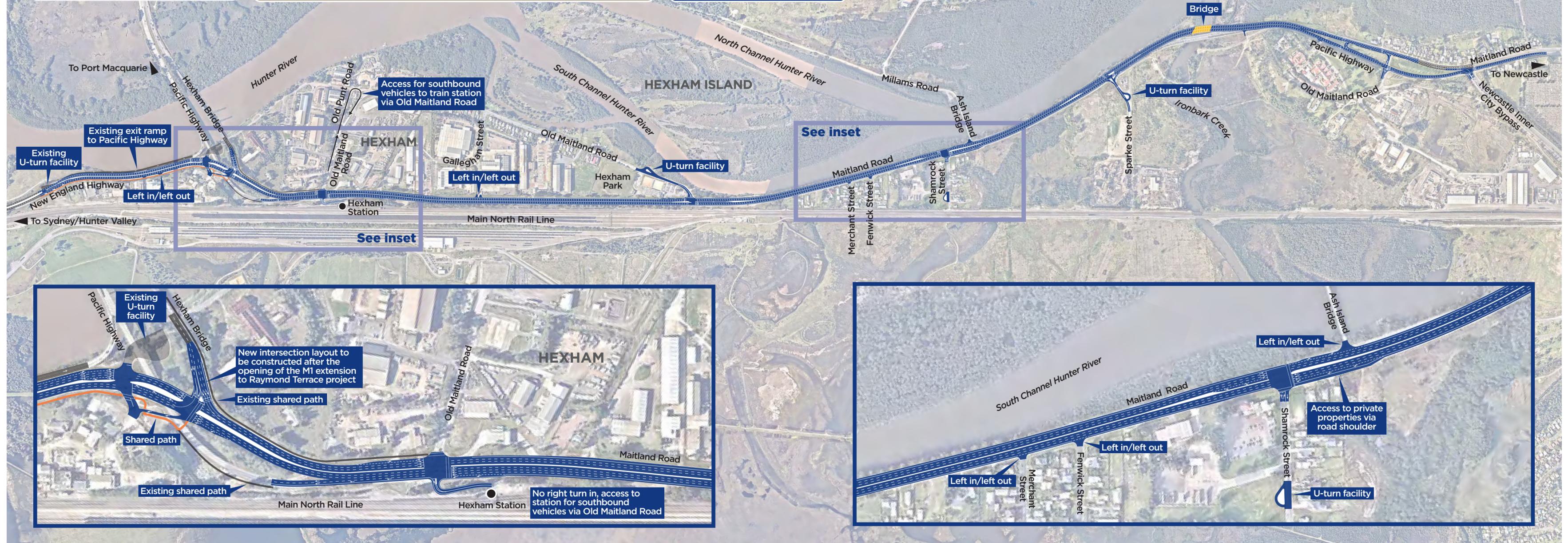
0 200 400 600
Metres

Legend

- Proposed new road alignment
- Existing road facilities
- Proposed shared path
- Existing shared path



- Key features**
- Widening six kilometres of Maitland Road from four to six lanes
 - Demolition and replacement of the existing bridge over Ironbark Creek
 - Relocation of utilities
 - U-turn facilities on Sparke Street, Shamrock Street and Old Maitland Road
 - Modifications to existing intersections along the route
 - Improved cyclist and pedestrian connectivity





Consultation starts on strategic design

The Australian and NSW governments are investing in the upgrade and development of the section of the Pacific Highway (Maitland Road) known as Hexham Straight between the Newcastle Inner City Bypass and Hexham Bridge.

Transport for NSW is planning the six kilometre widening project, which will see Maitland Road widened to three lanes in both directions between the Newcastle Inner City Bypass and Hexham.

This is the first chance for you to have your say on the initial designs for the project. Your feedback will help us understand what is important to you and deliver the best outcome for the community.

Around 50,000 motorists use this section of highway daily and this upgrade will ensure the Pacific Highway at Hexham is prepared to handle future growth and reduce congestion during peak periods.



Project benefits

- Improved connection between Newcastle and the New England Highway, M1 Motorway and the Pacific Highway
- Improved traffic flow for motorists and freight for more reliable travel times
- Improved access to the surrounding road network
- Opportunities to facilitate economic growth to and from key employment areas such as the Port of Newcastle and Greater Newcastle
- Improved safety for all road users.



Key features of the project

The proposed Hexham Straight widening project would include:

- Widening six kilometres of Maitland Road from four to six lanes
- Demolition and replacement of the existing bridge over Ironbark Creek
- Relocation of utilities
- U-turn facilities on Sparke Street, Shamrock Street and Old Maitland Road
- Modifications to existing intersections along the route
- Improved cyclist and pedestrian connectivity.



The project will provide new **U-turn facilities**



Improvements to existing intersections

Have your say

Transport for NSW is inviting feedback from the community on the initial designs for the proposed Hexham Straight widening project. The community is invited to provide comments by **5pm on Friday 5 February 2021** to:

Website:

nswroads.work/hexham

Email:

hexhamstraight@transport.nsw.gov.au

Mail:

Transport for NSW
Locked Bag 2030,
Newcastle NSW 2300

Individual phone meetings can be booked with project team representatives by calling **1800 515 141** (during business hours) to make an appointment.



Next steps

We will consider all feedback received during the consultation period in the next stage of the project.

We are planning to finalise and publicly display the Review of Environmental Factors (REF) and Concept Design for the Hexham Straight project in late 2021.

Any proposed changes to the current strategic design will be publicly displayed for comment at the same time as the REF in 2021.

Timing for construction is not confirmed and is subject to approval and funding availability.

We will keep the community and key stakeholders informed as the project progresses.

Contact us:

If you have any questions or would like more information on the upgrade please contact our project team:

 1800 515 141 (business hours)

 hexhamstraight@transport.nsw.gov.au

 nswroads.work/hexham

 Hexham Straight Project Manager
Transport for NSW,
Locked Bag 2030,
Newcastle NSW 2300



Existing bridge over Ironbark Creek

Project development process*

* timing subject to approval



Better and more reliable trips
for people, businesses and freight

Easing congestion

Improving safety

Improving freight productivity



This document contains important information about road projects in your area. If you require the services of an interpreter, please contact the Translating and Interpreting Service on 131 450 and ask them to call Transport Projects on 02 43 946 215. The interpreter will then assist you with translation.

Pub No. 20.417 | Pacific Highway improvements at Hexham (Hexham Straight) Community Update | December 2020

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Australian Government

BUILDING OUR FUTURE



April 2021

Pacific Highway improvements at Hexham (Hexham Straight) – Community feedback on strategic design

The Australian and NSW governments are planning for an upgrade of the section of the Pacific Highway (Maitland Road) known as Hexham Straight between the Newcastle Inner City Bypass and Hexham Bridge.

Transport for NSW is developing the six kilometre widening project, which would see Maitland Road widened to three lanes in both directions between the Newcastle Inner City Bypass and Hexham. The proposal would improve traffic flow along this critical route, deliver more reliable travel times for freight, and increase safety for all road users, including cyclists and pedestrians.

The Hexham Straight widening will be delivered as part of the Australian and NSW governments' commitment to deliver the M1 Pacific Motorway extension to Raymond Terrace and surrounding upgrades. The Australian Government has committed \$1.6 billion and the NSW Government \$400 million to build both projects, delivering safer and more efficient journeys for thousands of motorists.

The Pacific Highway (Maitland Road) is a critical link in the National Land Transport Network, providing access to and from the Port of Newcastle, and is among the busiest transport corridors carrying some of the highest traffic volumes in the Hunter.

Key features

- Widening six kilometres of Maitland Road from four to six lanes
- Demolition and replacement of the existing bridge over Ironbark Creek
- Relocation of public utilities
- U-turn facilities on Sparke Street, Shamrock Street and Old Maitland Road
- Modifications to existing intersections along the route
- Improved cyclist and pedestrian connectivity.

Community feedback

Stakeholders and the community were invited to provide their feedback on the strategic design between December 2020 and February 2021. We would like to thank everyone who took the time to provide feedback.

We received 45 submissions about a range of issues including:

- Suggestions about improving access for residents and motorists along Shamrock Street, Fenwick Street, Merchant Street and Old Maitland Road
- Suggestions about improving traffic flow at the Newcastle Inner City Bypass intersection with Maitland Road
- Suggestions for off-road cycle paths including dedicated separated shared pathways within the project as well as improving access for northbound cyclists over the Hunter River Bridge at Hexham
- Concerns about construction and operational impacts such as noise, flooding and dust impacts.

Project status

We received a high number of submissions about the status and progress of the M1 Pacific Motorway extension to Raymond Terrace project. The Australian Government has committed \$1.6 billion and the NSW Government \$400 million to build the extension and improve Hexham Straight, delivering safer and more efficient journeys for thousands of motorists.

The Environmental Impact Statement for the M1 Pacific Motorway extension to Raymond Terrace is scheduled to be displayed for community and stakeholder comment in mid-2021.

What happens next?

We will continue developing the concept design and environmental assessment, considering the community feedback received. We plan to display the environmental assessment for community feedback later this year. We will continue keep the community informed as the project progresses.

Timing of construction is not confirmed and is dependent on a range of factors including planning approval.

Contact

If you have any questions, please contact our Project team on 1800 515 141 (during business hours) or hexhamstraight@transport.nsw.gov.au.

For more information about this project, visit nswroads.work/Hexham.



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Transport for NSW Media Release

31 JANUARY 2020

Contract awarded for Pacific Highway improvements at Hexham

Jacobs Group Australia was today awarded the contract for the development of the concept design and environmental assessment to improve the section of the Pacific Highway known as Hexham Straight.

Transport for NSW Regional Director Hunter Anna Zycki said the next step will be to develop a preferred option to display for community comment in the second half of 2020 for the stretch of highway between the Newcastle Inner City Bypass and Hexham Bridge.

“The Pacific Highway (Maitland Road) is a critical link from the Port of Newcastle to the National Land Transport Network and is among the busiest transport corridors carrying some of the highest traffic volumes in the Hunter,” Ms Zycki said.

“The proposed six kilometre-long project would improve traffic flow along this critical route, deliver more reliable travel times for freight, and increase safety for all road users including cyclists and pedestrians.

“It is essential that we fully understand what the public wants from this project so we can strike the right balance between their needs and the demands of other users.”

Feedback received from the community will then be considered in finalising the preferred option and developing the concept design and environmental assessment for the project.

The proposed scope of the Hexham Straight project involves the addition of an extra lane in both directions, including replacement of the existing bridge over Ironbark Creek.

The work would also involve potential adjustments to connecting roads, utility relocation and consideration of project staging.

Timing for construction is not confirmed and is dependent on consideration of the project’s potential staging, planning approval and funding availability.

More information is available at nswroads.work/hexham.

Media contact: (02) 9462 6299



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April 2021

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Pacific Highway Hexham (Hexham Straight)

Community Consultation Report

May 2021

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1 Introduction

1.1. Background

The Pacific Highway (Maitland Road) is a critical link from the Port of Newcastle to the National Land Transport Network and is among the busiest transport corridors carrying some of the highest traffic volumes in the Hunter.

The proposed six kilometre-long project would improve traffic flow along this critical route, deliver more reliable travel times for freight, and increase safety for all road users, including cyclists and pedestrians.

The proposed scope of the Hexham Straight project involves the addition of an extra lane in both directions, removal of the existing bridges and construction of two new bridges at Ironbark Creek, adjustments to connecting roads as well as significant utility relocation.

Timing for construction is not confirmed and would be dependent on planning approval, future traffic needs and funding availability.

Consultation for the initial designs for the proposed Hexham Straight widening project was carried out from 2 December 2020 to 5 February 2021.

1.2. The proposal

The proposed safety improvements included:

- Upgrades to the six kilometre section of the Pacific Highway between the Newcastle Inner City Bypass and Hexham Bridge.
- Widening of the dual carriageway from two lanes to three lanes in each direction
- Demolition of the existing Ironbark bridge and construction of two new bridges at Ironbark Creek
- Minor intersection upgrades and property adjustments
- Utility relocation.

2 Consultation approach

2.1 Consultation objectives

We consulted with the community and key stakeholders on the proposal to:

- Inform the community and stakeholders of the proposal and possible impacts
- Seek feedback on the proposal and issues of concern for consideration in developing the design and method of construction. The feedback will be incorporated into the environmental impact assessment.
- Build a database of interested and concerned community members with whom we can continue to engage with during the proposal's development.

2.2 Values

Our values underpin our decisions and behaviours when working with customers, colleagues, stakeholders and partners.

- **Customer focus** - We place the customer at the centre of everything we do
- **Collaboration** - We value each other and create better outcomes by working together
- **Solutions** - We deliver sustainable and innovative solutions to NSW's transport needs
- **Integrity** - We take responsibility and communicate openly
- **Safety** - We prioritise safety for our people and our customers.

2.3 How consultation was done

We consulted with the community in December 2020 on the proposed design. Community members were encouraged to provide their feedback via mail, email, online or by phone contact with the Project Manager.

Our key consultation tools are listed below:

Project Update	Delivered to 240 residents and businesses in the Hexham area. A copy of the project update is available at Appendix A .
Webpage	Project webpage (nswroads.work/Hexham) created and updated with latest project information.
Briefings	Arranged with key stakeholders such as City of Newcastle, and as requested by businesses and residents.

3 Consultation summary

3.1 Overview

Comments on the proposed upgrade closed on Friday 5 February 2021, with 45 submissions received about a range of issues. Some submissions included multiple comments and raised multiple issues.

The feedback received in formal submissions raised a number of issues including flooding and noise, improving access and safety on side streets, reducing number of traffic lights, increased congestion and impact to the local road network and request for off-road cycle paths.

After considering all responses, along with the proposal's aims and design requirements, we have completed additional investigations and design work to help inform the next steps for the proposal.

Issue category	Number issue raised	Issues raised
Design proposal	1	Request to add left turn slip and acceleration lane on south side of Maitland Road
	4	Concerns around the design of the proposal
	5	Suggestions to improve traffic flow at the Newcastle Inner City Bypass intersection
	2	Suggestions about additional access to Hexham Station
	4	Request for flyover onto the Newcastle Inner City Bypass
	1	Request to extend widening
	6	Request to reduce traffic lights on Hexham Straight
Access and safety Improvements	1	Merchant Street
	1	Fenwick Street
	2	Shamrock Street
	4	Old Maitland Road
	4	Sparke Street
	1	Concerns of safety and accessing businesses

	1	Suggested alternatives to remove vehicles in accidents without closing lanes on Hexham Straight
	1	Request for upstream pre-warning hazard lights in 80km/h zone
	1	Request to reduce speed on Hexham Bridge on-ramp
	1	Suggestion to close access road from the Calvary St Joseph's Retirement Community on to Maitland Road and redirecting traffic into the Old Maitland Road intersection
Traffic	1	Newcastle Inner City Bypass intersection
	1	Traffic issue with widening dual carriageways
	7	Suggestions to improved traffic flow on Hexham Bridge
	1	Hexham Straight
	2	Shamrock Street
Cyclist provision	1	Suggestion to improve access for northbound cyclists over the Hunter River Bridge at Hexham
	5	Suggestions for dedicated shared pathway or off-road cycle paths and safety improvements
Flooding	2	Hunter River
	1	Hexham Straight
	1	Shamrock Street
Heavy vehicles	1	Ironbark Creek Bridge
	2	Shamrock Street
General	1	Speed limit after upgrade
	1	U-turn facility at Sparke Street
Construction impacts	1	Disruptions on Hexham Straight
	1	Demolition of Iron Bark Creek bridge
Noise	2	Shamrock Street

Public transport	2	Suggestions on improved public transport sign posting and locations
Dust	1	Shamrock Street
Fishing	1	Retention or extension of Fishing or Tourism alcoves along the Hunter River edge

3.2 Recommendations

A majority of feedback related to improvements to access and traffic flow. We are unable to address all of these issues with the current funding and project scope, however Transport for NSW will consider this feedback for potential future funding nominations.

Based on the feedback received, Transport for NSW has carried out further investigations and has made some changes to the design of the project. We will be proceeding with the project with some changes, including:

- Additional capacity added at the Newcastle Inner City Bypass intersection. A third left turn lane will be included on the approach to Maitland Road for about 300 metres.
- An off road cycle path will be added on the approach to Maitland Road from the Newcastle Inner City Bypass, connecting cyclists to Maitland Road and bypassing the traffic lights
- Revised the project design to cater for B-double vehicles up to 30 metres long
- Improvements to existing bus stops and facilities
- Improvements to the Hexham Station access.

For submissions which related to scope outside of the project proposed, we:

- Contacted Hunter Water about requests for a sewer connection
- Contacted City of Newcastle about timing of the Richmond Vale Rail Trail.

Please see **Appendix B** for a copy of the revised design.

We will work closely with local residents and stakeholders in relation to the potential impacts of this project. We will do this by:

- Informing the community and stakeholders of consultation outcomes and the next steps for the proposal via publication of this report
- Continuing to carry out communication activities as the project progresses and informing the community through the Transport for NSW website and project notifications.

3.3 Next steps

We have considered all submissions and will ensure our team is aware of concerns raised by the community. We will ensure issues highlighted by stakeholders and the community that are unable to be addressed by this project will be considered for future funding.

We will continue to keep the community informed of the project progress.

Appendix A – Project update (December 2020)

Appendix B – Revised design

24 February 2021

Attention: Geoff Mansfield
gmansfield@ncc.nsw.gov.au
Officalmail@ncc.nsw.gov.au
The City of Newcastle
12 Stewart Avenue
PO Box 489
Newcastle, NSW 2300
Australia

Dear Geoff,

RE: State Environmental Planning Policy (Infrastructure) 2007 - Consultation regarding the proposed widening of the Pacific Highway/Maitland Road/New England Highway (A43) at Hexham

Transport for NSW (Transport) proposes to widen about six kilometres of the Pacific Highway/Maitland Road/New England Highway (A43) from four lanes to six lanes, from the intersection with the Newcastle Inner City Bypass at Sandgate, through to Hexham Bridge, in Hexham, NSW (the overall proposal). The section of road known as the 'Hexham Straight' is located in the suburb of Hexham within the Newcastle local government area (LGA).

The proposal is subject to assessment under two planning pathways, a review of environmental factors (REF) under Part 5 of *Environmental Planning and Assessment Act 1979* (EP&A Act) and an environmental impact statement (EIS) under Part 4 of the EP&A Act. The majority of the proposal is subject to approval under Part 5 of the EP&A Act (known as the REF proposal). However, a small part of the proposal (3.13 hectares) falls on land mapped as "coastal wetlands" under State Environmental Planning Policy (Coastal Management) 2018 (CM SEPP). As such, that part of the proposal (known as the EIS proposal) is subject to approval under Part 4 of the EP&A Act and will be assessed within an EIS that will be submitted separately to the City of Newcastle. Further detail on the proposal is included in **Attachment A** and in Transport's December 2020 Community Update included as **Attachment B**.

The purpose of this letter is to formally consult with the City of Newcastle about the REF proposal (as described in **Attachment A**) under clauses 13 - 15 of the State Environmental Planning Policy (Infrastructure) 2007 in relation to potential impacts on Council infrastructure, flood liable land and local heritage. The REF proposal involves:

- Excavation of the surface and footpath of local roads (for which the City of Newcastle is the roads authority under the *Roads Act 1993*) that is not minor or inconsequential. Existing drainage structures would be modified to suit the widened carriageways
- The installation of a temporary structure on, or the enclosing of, a public place that is under Newcastle City Council management or control that is likely to cause a disruption to pedestrian or vehicular traffic that is not minor or inconsequential
- Development on flood liable land that is susceptible to flooding by the probable maximum flood event
- Construction activities within close proximity of the following local heritage items listed under the Newcastle Local Environmental Plan 2012 (that are not also listed as State heritage items):
 - Oak Factory (I178)
 - Goninans Administration Building (I186)
 - Minmi To Hexham Railway (I332)
 - J & A Brown'S Hexham Workshop (I183)

- Former Hexham Public School (I185)
- Hexham Railway Station (I176)
- Former Glen Lovett Hall (I184)
- Former Uniting Church And Hall (I182)
- Former Travellers Rest Hotel (I177)
- 2Hd Studio (I519)
- Sandgate Cemetery (I516)
- Sangate Cemetery Railway Spur (I517)
- Sandgate Cemetery Office (I518).

A Project REF is currently being prepared by Jacobs on behalf of Transport, to identify potential environmental planning issues associated with the REF proposal and to provide appropriate management measures to minimise environmental impacts. Issues to be considered within the REF will include flooding, coastal processes, Aboriginal and non-Aboriginal heritage, biodiversity, noise and vibration, traffic and access, soils, and contamination, socio-economic impacts, landscape character, visual aesthetics, urban design and cumulative environmental effects. A Statement of Heritage Impact (SOHI) will be prepared and included in the REF to address any direct or indirect impacts on locally and State listed heritage items.

To assist in our investigation and to ensure all potential issues are addressed, it would be appreciated if you could provide any comments regarding this proposal to the undersigned within 21 days from receipt of this letter. Any response from the City of Newcastle within that period will be considered by Transport in its consideration of this proposal.

Transport would be pleased to provide further information if required. Should you have any queries about this matter, please do not hesitate to contact the Project Development Manager, Andrew Thompson on 02 4908 7630 or by email Andrew.J.THOMPSON@transport.nsw.gov.au

Yours sincerely,



Andrew Thompson
Project Development Manager
Technical & Project Services
Infrastructure & Place
Transport for NSW
T 02 4908 7630 | M 0427 535 448
Level 7 266 King Street Newcastle NSW 2300

Regulatory, Planning and Assessment.MBisson/GMansfield
Reference: PB2021/01977
Phone: 02 4974 2000

12 April 2021

Andrew Thompson
Project Development Manager
Technical & Project Services
Infrastructure & Place
Transport for NSW
NEWCASTLE NSW 2300

Reply by Email: Andrew.J.Thompson@transport.nsw.gov.au

Dear Mr Thompson

RE: NOTIFICATION PROPOSED WIDENING OF THE PACIFIC HIGHWAY/MAITLAND ROAD/NEW ENGLAND HIGHWAY HEXHAM

I refer to your letter dated 24 February 2021 advising it is proposed to widen about six kilometres of the above roads from four to six lanes at Hexham under the provisions of *State Environmental Planning Policy (Infrastructure) 2007* and seeking comments from the City of Newcastle (CN) regarding potential impacts on CN infrastructure, flood liable land and local heritage items for consideration in the preparation of the Review of Environmental Factors (REF). I also refer to the meeting of 24 March 2021 between officers from Transport for NSW (TfNSW) and CN regarding the project.

It is noted that the proposal is subject to assessment under two planning pathways, a REF under Part 5 of *Environmental Planning and Assessment Act 1979* (EP&A Act) and a development application under Part 4 of the EP&A Act. The majority of the proposal is subject to approval under Part 5. However, a small part of the proposal involves land mapped as 'coastal wetlands' under *State Environmental Planning Policy (Coastal Management) 2018*. Accordingly, that part of the proposal is subject to approval under Part 4 and a development application will be submitted to CN.

The following advice is provided for your consideration in the preparation of the REF:

1. CN Infrastructure

Stormwater Assets

A CN Asset Engineer has provided the following comments:

- *TfNSW is to determine the location of CN's stormwater assets in the project area. To assist you in this regard the attached map shows most of CN's stormwater assets in the project area, but it may not be complete or accurate.*
- *Where the existing stormwater assets are located within the project area or likely to be impacted by the works, assess for retention/renewal/upgrade. This assessment must include consideration to pipe condition, capacity, cover, location and accessibility. CN can provide condition assessments on CN owned infrastructure if given sufficient notice.*

- *Where existing outlets are impacted by the widening, extend the drainage system to CN's standards (to be approved by CN). This includes outlet headwalls, tide gates etc.*
- *Safe access from the road is required for each headwall for maintenance (i.e. safe location to stop, removable barriers to move machinery to headwall). If a Road Occupancy Licence is required to maintain assets that benefit the road, the assets will become TfNSW owned. TfNSW will also need to ensure appropriate access is provided to perform routine maintenance. CN can provide details on specific access requirements for each site i.e. truxor, backhoe or manual labour/hand tools.*
- *Investigate and implement WASTOP tide gates or similar to protect the upstream catchment from inundation where appropriate.*
- *Runoff is directly connected to Hunter River. Treat all drainage systems prior to discharge to meet water quality targets.*
- *A solid redirective kerb will impede flood water and adversely impact properties. Investigate impacts to overland flow paths.*
- *Safely maintain stormwater conveyance through minor (10% Annual exceedence probability (AEP)) and major (1% AEP) events.*
- *Works as Executed (WAE) documentation is required all new CN stormwater assets and dilapidation for all retained assets within the project area as per attached CN document 'Stormwater WAE Requirements'.*

Traffic and Roads

A CN Development Officer (Engineer) has provided the following comments:

'It is requested that TfNSW address how the proposed works may impact on development along either side of the corridor. Concern is raised specifically in relation to managing vehicular access for existing sites and the proposal potentially limiting the future development potential of the properties along the road corridors.'

The potential traffic impact on the local roads network is also a matter of concern as additional traffic will be directed to these roads.'

CN's Transport & Traffic Coordinator has provided the following comments:

'It is requested the design of the project is amended to provide a right turn in (right turn lane) to Old Maitland Road from the Pacific Highway. (Refer to attachment)

This turning facility is required because presently trucks travelling from the east to the industrial properties turn right at Old Maitland Road at the Hexham Bowling Club end. The trucks are travelling through residential areas and causing vibration to residents' homes. The road was not design for heavy vehicles and is deteriorating over time.

While the proposed U-turn facility is helpful it will not assist the drivers of the trucks if their destination is west of Galleghan Street because they cannot run right at Old Maitland Road (western end).

CN did not approve as a heavy vehicle route that section of Old Maitland Road from Gallegan Street to the eastern end (near Hexham Bowling Club). As a result, heavy vehicles heading west are continuing along the Pacific Highway to find a turning area to turn around to turn left into Old Maitland Road.'

2. Flooding

A CN Development Officer (Engineer) has provided the following comments:

- *The full length of the proposed works is subject to Ocean and River flooding during the 1% AEP and PMF flooding events.*
- *It is recommended that TfNSW obtain a site/locality specific flood study to demonstrate that the proposed works will not have an adverse flooding impact on neighbouring developments or downstream allotments.*
- *Once a flood study has been conducted for the site, TfNSW can liaise with CN in relation to any identified impacts that the proposal may have in relation to flooding.*

3. Heritage

A CN Development Officer (Heritage) has provided the following comments:

'The proposed works are in the vicinity of 13 listed heritage items on Schedule 5 of the Newcastle Local Environmental Plan 2012 (Refer to attachment).

Generally, the proposed works do not encroach into the curtilage of any of the identified heritage items as the works are within the public road reserve. A Statement of Heritage Impact should be prepared to assess the impact of the project on the setting of all heritage items along the route and in the vicinity.

The works include replacement of the existing Ironbark Creek bridge. These works would include significant excavation. Appropriate due diligence with respect to Aboriginal cultural heritage values will need to be undertaken, particularly noting the proximity to Hexham Swamp and the Hunter River. These are known to be key locations in Aboriginal occupation of the region as identified in CN's Aboriginal Heritage Management Strategy 2018-21.'

If you have any questions in relation to the various matters raised in this letter, please contact Geof Mansfield Principal Planner (Development) on 4974 2767 or by email on gmansfield@ncc.nsw.gov.au.

Yours faithfully



Michelle Bisson
MANAGER REGULATORY, PLANNING AND ASSESSMENT

Atts

Attachment - Heritage Items

Item	Address	Item No.
Railway Station	Maitland Road, Hexham	I176
Former Travellers Rest Hotel	23 Maitland Road, Hexham	I177
Oak Factory	189 Maitland Road, Hexham	I178
Former Uniting Church and Hall	63 Old Maitland Road, Hexham	I182
J & A Brown's Hexham Workshops	100 Old Maitland Road, Hexham	I183
Former Glen Lovett Hall	187 Old Maitland Road, Hexham	I184
Former Hexham Public School	277 Old Maitland Road, Hexham	I185
Goninans Administration Building	230 Old Maitland Road, Hexham	I186
Minmi to Hexham Railway	Minmi to Hexham	I332
Sandgate Cemetery	108 Maitland Road, Sandgate	I516
Railway Spur – Sandgate Cemetery	108 Maitland Road, Sandgate	I517
Office – Sandgate Cemetery	116 Maitland Road, Sandgate	I518
2HD Studio	173 Maitland Road, Sandgate	I519

Stormwater WAE Requirements



The following is a summary of City of Newcastle's (CN) requirements for Works As Executed (WAE) documentation for all stormwater drainage assets. All deliverables as defined below shall be submitted to CN's Principal Representative and assessed for conformance.

1. Dilapidation Survey - Pre and Post CCTV Inspection of CN Stormwater Assets

Pre and post CCTV inspection of CN owned drainage assets in the project area that is to be retained (project area defined as area likely to be subject to construction loading throughout the projects life span). The pre and post CCTV survey shall act as the dilapidation survey. This CCTV is to be undertaken in accordance with Appendix 11 – Specification of Drainage Inspection of CN's Stormwater and Water Efficiency for Development Technical Manual April 2019 (attached).

2. CCTV Inspection and Reporting for New Drainage Assets

All new stormwater drainage assets to be dedicated to the City of Newcastle shall be inspected in accordance with Appendix 11 – Specification of Drainage Inspection of CN's Stormwater and Water Efficiency for Development Technical Manual April 2019 (attached).

CCTV inspection to be undertaken after risk of damage to the assets due to construction loading is negligible.

3. Defect Assessment and Rectification for Acceptance

All new stormwater drainage assets to be dedicated to the City of Newcastle shall be assessed to identify any defects in accordance with Appendix 10 – Specification of Acceptance of Drainage Defects CN's Stormwater and Water Efficiency for Development Technical Manual April 2019 (attached).

Council's minimum condition standards for new drainage infrastructure are set out by this Specification which references WSA 05-2008 2.2 for acceptability of defects. Defects that are unacceptable will require remediation in order to achieve the minimum standard for Council to accept as public assets within the Defects Liability Period.

4. WAE Engineering Survey

WAE drawings shall be prepared and certified by a Registered Land Surveyor in the State of New South Wales or a suitably qualified Engineer.

The WAE Engineering Survey of all stormwater drainage assets to be dedicated to the City of Newcastle shall comply with the following:

- Survey to be delivered as 12D compatible to CN
- Drainage Asset Engineering Survey to be completed and capture data outlined the table below.
- Survey to be undertaken after risk of damage of the assets due to construction loading is negligible.
- PDF plan of WAE survey to be supplied to CN

Table 1 -Required Data for Drainage Asset Engineering Survey

Stormwater Pipes and Culverts
Pipes - All U/S and D/S invert levels of pipe. Link matching U/S and D/S inverts with a line in suitable drainage style. List pipe length as a string attribute.
Pipe Material: Steel Reinforced Concrete etc
Pipe Class: Class 1, 2, 3, 4, 5, 6, 7, 8, 9, 10
Pipe Length: m
Pipe Diameter: mm
Pipe Height: For culverts only mm
Pipe Width: For culverts only mm
JointType: Rubber Ring Joint/Butt
CoverType: Standard or saltwater
US InvRL: m
US InvDepth: mm
DS InvRL: m
DS InvDepth: mm
Source: R Plan Number or project Plan Number
Stormwater Pits
Pit Type: (Bike Safe Kerb Inlet, junction etc.)
Pit Material: Concrete etc
Pit Construct: Precast or Cast in Situ
Lid Type: Cast Iron, Galv grate, etc
Sag: Yes or No
StepIrons: Yes or No
Pit Length: mm
Pit Width: mm
Pit Depth: Check with Design to include in chain (Cover RL-Inv RL)
Pit Diameter: mm
Pit Surface RL: m
Pit InvertRL: m
LintellLength: mm Select from 1200, 1800, 2400, 3000, 3600, 4200
Grate Length: mm
Grate Width: mm
Source: R Plan Number

Hexham Straight Widening Project Stormwater Plan 15



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Scale: 12,597.96

at A4

Hexham Straight Widening Project Stormwater Plan 14



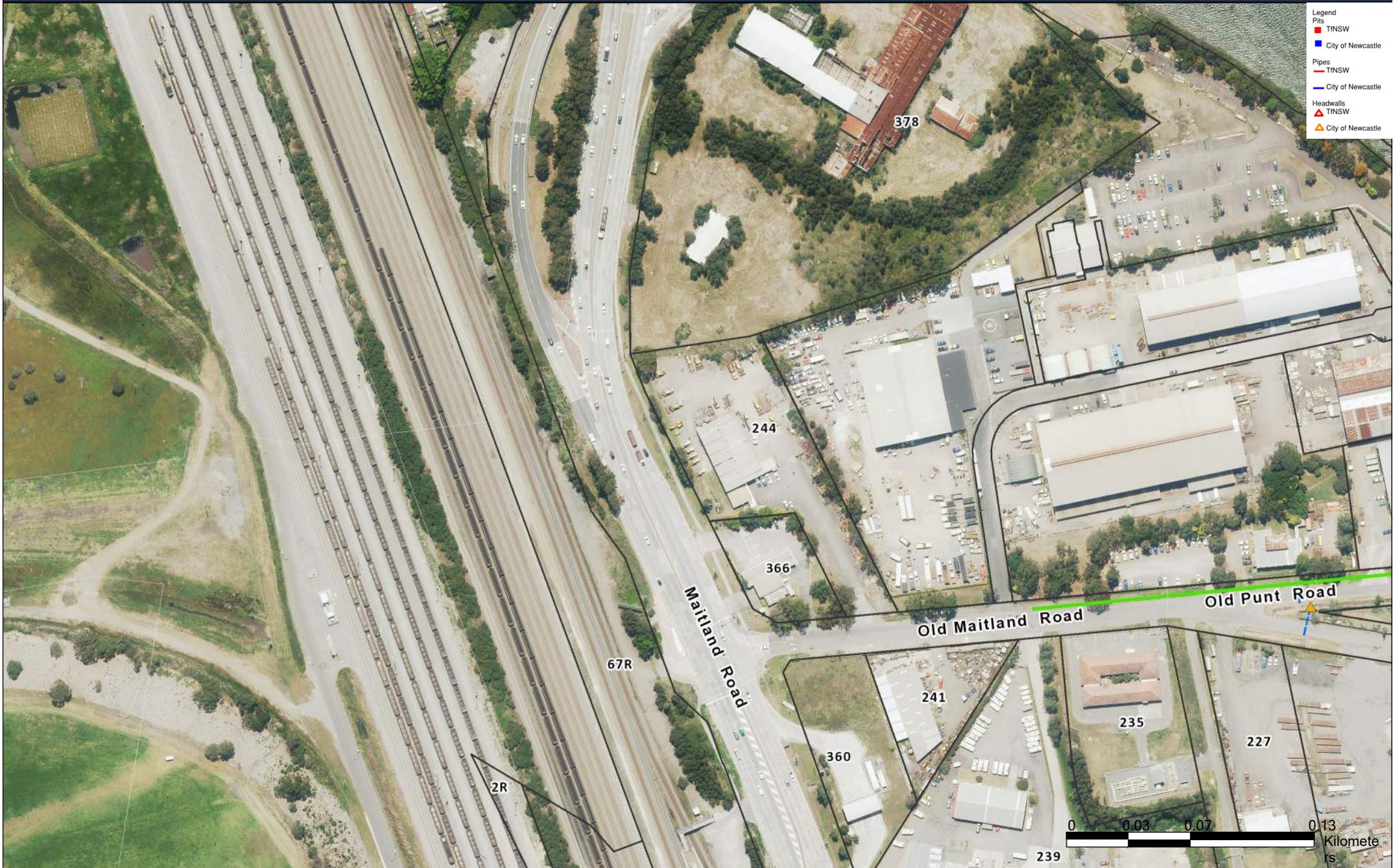
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Hexham Straight Widening Project Stormwater Plan 13



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Scale: 12,597.96

at A4

Hexham Straight Widening Project Stormwater Plan 12



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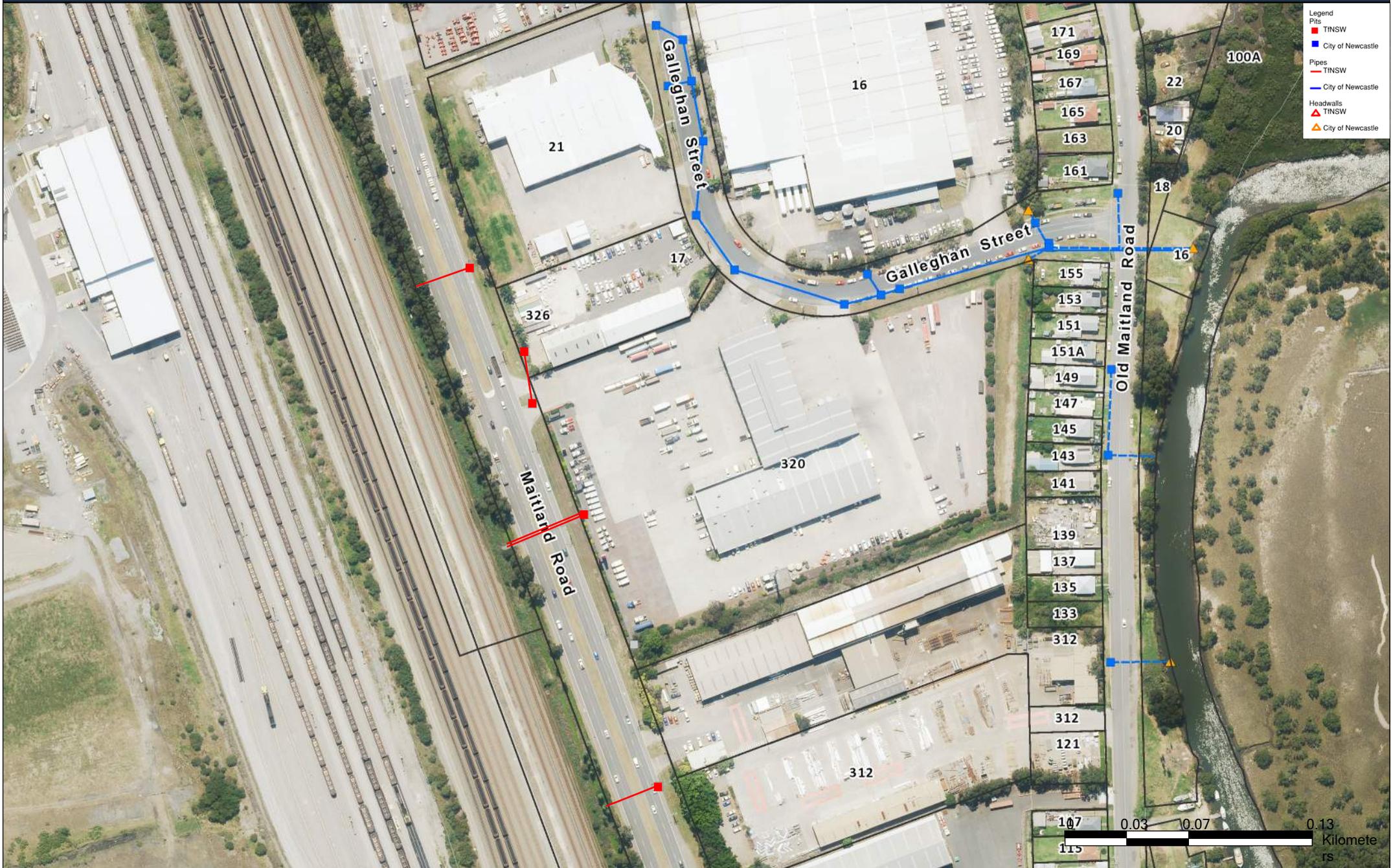
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Scale: 12,597.96

at A4

Hexham Straight Widening Project Stormwater Plan 11



- Legend
- Pits
 - TINSW
 - City of Newcastle
 - Pipes
 - TINSW
 - City of Newcastle
 - Headwalls
 - ▲ TINSW
 - ▲ City of Newcastle

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at A4

Hexham Straight Widening Project Stormwater Plan 10



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at A4

Hexham Straight Widening Project Stormwater Plan 9



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Hexham Straight Widening Project Stormwater Plan 8



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Hexham Straight Widening Project Stormwater Plan 7



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Hexham Straight Widening Project Stor Plan 6



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at A4

Hexham Straight Widening Project Stormwater Plan 5



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Scale: 12,597.96

at A4

Hexham Straight Widening Project Stormwater Plan 4



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Scale: 1:2,597.96

at A4

Hexham Straight Widening Project Stormwater Plan 3



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Scale: 12,597.96

at A4

Hexham Straight Widening Project Stormwater Plan 2



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Scale: 12,597.96

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Hexham Straight Widening Project Stormwater Plan 1



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Scale: 12,597.96

at A4

24 February 2021

Attention: R Philips / Scott Carter
DPI Fisheries
1243 Bruxner Highway
Wollongbar, NSW 2477

To whom it may concern,

RE: Consultation regarding the proposed widening of the Pacific Highway/Maitland Road/New England Highway (A43) at Hexham

Transport for NSW (Transport) proposes to widen about six kilometres of the Pacific Highway/Maitland Road/New England Highway (A43) from four lanes to six lanes, from the intersection with the Newcastle Inner City Bypass at Sandgate, through to Hexham Bridge, in Hexham, NSW (the overall proposal). The section of road known as the 'Hexham Straight' is located in the suburb of Hexham within the Newcastle local government area (LGA).

The proposal is subject to assessment under two planning pathways, a review of environmental factors (REF) under Part 5 of *Environmental Planning and Assessment Act 1979* (EP&A Act) and an environmental impact statement (EIS) under Part 4 of the EP&A Act. The majority of the proposal is subject to approval under Part 5 of the EP&A Act (known as the REF proposal) and a REF is currently being prepared by Jacobs on behalf of Transport for the REF proposal. However, a small part of the proposal (3.13 hectares) falls on land mapped as "coastal wetlands" under State Environmental Planning Policy (Coastal Management) 2018 (CM SEPP). As such, that part of the proposal (known as the EIS proposal) is subject to approval under Part 4 of the EP&A Act and will be assessed within an EIS. Further detail on the proposal is included in **Attachment A** and in Transport's December 2020 Community Update included as **Attachment B**.

The purpose of this letter is to formally consult with the NSW Department of Primary Industries (DPI) Fisheries about the REF proposal (as described in **Attachment A**), under section 199 of the *Fisheries Management Act 1994* (FM Act). The REF proposal is located next to the Hunter River and sections of the proposal are on habitat mapped as Type 1 and Type 2 Key Fish Habitat and in a waterway identified as Class 1 Major Key Fish Habitat according to the *Policy and Guidelines for Fish Habitat Conservation and Management* (Department of Primary Industries, (DPI) 2013). In addition, construction activities would be undertaken in the Ironbark Creek channel from a barge, that would include the installation of bridge piers into the bedrock beneath. The waterways next to the REF proposal also form part of the Hunter Estuary Wetlands and Hunter Wetlands National Park. Ironbark Creek also flows into and out of Hexham Swamp Nature Reserve.

Construction activities within key fish habitat areas, such as dredging or reclamation works, or those resulting in any harm to marine vegetation or any obstruction to fish passage would normally require permits under the FM Act. Section 199 of the FM Act allows public authorities to undertake dredging or reclamation works without the need for a permit. However, under Part 7, Division 3, Section 199 of the FM Act, Transport must consult with NSW Fisheries before carrying out or authorising dredging or reclamation work.

To assist in our investigation and to ensure all potential issues are addressed, it would be appreciated if you could provide any comments regarding this proposal to the undersigned within 21 days from receipt of this letter. Any response from Fisheries NSW within that period will be considered by Transport in its consideration of the REF proposal.

Transport would be pleased to provide further information if required. Should you have any queries about this matter, please do not hesitate to contact the Project Development Manager, Andrew Thompson on 02 4908 7630 or by email Andrew.J.THOMPSON@transport.nsw.gov.au

Yours sincerely,



Andrew Thompson

Project Development Manager
Technical & Project Services
Infrastructure & Place
Transport for NSW
T 02 4908 7630 | M 0427 535 448
Level 7 266 King Street Newcastle NSW 2300

Our Ref: C21/119

Your Ref: A36263802

12 March 2021

Andrew Thompson
Project Development Manager
Transport for NSW
266 King St
Newcastle NSW 2300

Dear Andrew,

Proposal: Proposed widening of the Pacific Highway/Maitland Road/New England Highway (A43)

Property: Sandgate to Hexham

Thank you for your referral of 24 February 2021 seeking comments on the proposal from the NSW Department of Primary Industries – Fisheries (DPI Fisheries).

DPI Fisheries is responsible for ensuring that fish stocks are conserved and that there is no net loss of key fish habitats upon which they depend. To achieve this, DPI Fisheries ensures that developments comply with the requirements of the *Fisheries Management Act 1994* (FM Act) (namely the aquatic habitat protection and threatened species conservation provisions in Parts 7 and 7A of the Act, respectively), and the associated *Policy and Guidelines for Fish Habitat Conservation and Management (2013)*. DPI Fisheries is also responsible for ensuring the sustainable management of commercial, recreational and Aboriginal cultural fishing, aquaculture, Marine Parks and Aquatic Reserves within NSW.

The proposal requires the removal of areas of mangroves and saltmarsh, which are considered as Key Fish Habitat and are also listed as Coastal wetlands under the Coastal Management SEPP.

Harm to marine vegetation requires a permit under s 204-5 the *Fisheries Management Act*, while works of dredging and reclamation require a s199 approval.

In issuing a permit for the works to proceed and harm marine vegetation the Department has a policy that requires avoidance, mitigation or offsets for any marine vegetation harmed. This is calculated on a 2:1 ratio.

Consequently, the Department would require:

- Where possible avoid harm by minimising the footprint of the development area. This would most likely be the use of bridging over those areas around Ironbark Creek and the wetlands to the west between Sparke and Shamrock Sts., rather than filling. This would reduce the impact on the wetland areas to the approximate footprint of the bridge piles.
- If this is not feasible then any walls to contain fill for abutments etc should be as vertical as possible to minimise the footprint of the roadway.
- An assessment of the area impacted must be made, quantifying the vegetation being affected.
- An assessment of potential offsets based on the final design of the works. This may identify areas that could be enhanced along the bank of the Hunter River beside the works (eg rock fillets) or potential environmental works further afield (eg offset area identified for the M1 Hexham Bypass).

Should the proposal be modified, the applicant will be required to resubmit the proposal and updated aquatic habitat surveys and related information as advised by the relevant officer of Fisheries NSW.

If you, Crown Lands or Council require any further information, please do not hesitate to contact me on 4916 3931.

Yours sincerely,



Scott Carter

Senior Fisheries Manager – Coastal Systems Central/Metro
Authorised delegate of the Minister for Primary Industries



24 February 2021

Attention: Joanna Erksine
Jo.erskine@environment.nsw.gov.au
Hunter Wetlands National Park Ranger
NSW National Parks & Wildlife Service
1 Wetlands Place
Shortland, NSW 2307
PO Box 351, Jesmond NSW 2299

Dear Joanna,

RE: State Environmental Planning Policy (Infrastructure) 2007 - Consultation regarding the proposed widening of the Pacific Highway/Maitland Road/New England Highway (A43) at Hexham

Transport for NSW (Transport) proposes to widen about six kilometres of the Pacific Highway/Maitland Road/New England Highway (A43) from four lanes to six lanes, from the intersection with the Newcastle Inner City Bypass at Sandgate, through to Hexham Bridge, in Hexham, NSW (the overall proposal). The section of road known as the 'Hexham Straight' is located in the suburb of Hexham within the Newcastle local government area (LGA).

The proposal is subject to assessment under two planning pathways, a review of environmental factors (REF) under Part 5 of *Environmental Planning and Assessment Act 1979* (EP&A Act) and an environmental impact statement (EIS) under Part 4 of the EP&A Act. The majority of the proposal is subject to approval under Part 5 of the EP&A Act (known as the REF proposal) and a REF is currently being prepared by Jacobs on behalf of Transport for the REF proposal. However, a small part of the proposal (3.13 hectares) falls on land mapped as "coastal wetlands" under State Environmental Planning Policy (Coastal Management) 2018 (CM SEPP). As such, that part of the proposal (known as the EIS proposal) is subject to approval under Part 4 of the EP&A Act and will be assessed within an EIS. Further detail on the proposal is included in **Attachment A** and in Transport's December 2020 Community Update included as **Attachment B**.

The purpose of this letter is to formally consult with National Parks and Wildlife Service (NPWS) about the REF proposal (as described in **Attachment A**). As the proposed works would be carried out adjacent to land reserved under the *National Parks and Wildlife Act 1974* in Zone E1 as part of the Hunter Wetlands National Park, consultation is required under clause 16(2)(b) of the State Environmental Planning Policy (Infrastructure) 2007.

To assist in our investigation and to ensure all potential issues are addressed, it would be appreciated if you could provide any comments regarding this proposal to the undersigned within 21 days from receipt of this letter. Any response from NPWS within that period will be considered by Transport in its consideration of this proposal.

Transport would be pleased to provide further information if required. Should you have any queries about this matter, please do not hesitate to contact the Project Development Manager, Andrew Thompson on 02 4908 7630 or by email Andrew.J.THOMPSON@transport.nsw.gov.au

Yours sincerely,



Andrew Thompson

Project Development Manager
Technical & Project Services
Infrastructure & Place
Transport for NSW
T 02 4908 7630 | M 0427 535 448
Level 7 266 King Street Newcastle NSW 2300



DOC 21/163265
5 March 2021

Andrew Thompson
Project development Manager
Technical & Project Services
Infrastructure & Place
Transport for NSW
Level 7 266 King Street
NEWCASTLE NSW 2300

By email: Andrew.J.THOMPSON@transport.nsw.gov.au

Attention: Andrew Thompson, Project Development Manager

Dear Andrew

State Environmental Planning Policy (Infrastructure) 2007- Consultation regarding proposed widening of the Pacific Highway/Maitland Road/New England Highway(A43) at Hexham

Thankyou for your letter dated 23 February 2021, regarding the Transport for NSW (TfNSW) proposed widening of the Pacific Highway at Hexham and seeking comments from the National Parks and Wildlife Service (NPWS).

NPWS acknowledges that this represents statutory consultation under clause 16 of the State Environmental Planning Policy (Infrastructure) 2007 as a section of the Pacific Highway project is located on the interface with Hunter Wetlands National Park.

NPWS would like to clarify as part of this response that no works are directly proposed on land reserved under the *National Parks and Wildlife Act 1974* as part of the initial project delivery and that TfNSW remains committed to avoiding impacts to park.

On review of the material presented and following on from the project brief delivered by TfNSW on 23 February 2021 NPWS requests that the following matters be addressed in the preparation of the project Review of Environmental Factors (REF).

In summary NPWS recommends:

1. Consideration of environmental matters in accordance with the [Guidelines for Developments Adjoining Lands Managed \(OEH 2013\)](https://www.environment.nsw.gov.au/topics/parks-reserves-and-protected-areas/development-guidelines). <https://www.environment.nsw.gov.au/topics/parks-reserves-and-protected-areas/development-guidelines>
2. Clear instructions are provided as part of the REF, and any operation documents produced by TfNSW will ensure no unauthorised works, access or encroachments occurs to park.
3. Adequate sediment and erosion controls are in place, and operational especially in environmentally sensitive areas to protect the park and its interface during project works.
4. Public safety is considered and managed accordingly, where the park entry points intersect with the highway corridor and the project works in particular at the parks, Ash Island entrance.

5. Hygiene protocols for machinery, vehicle and material are established, and delivered throughout the project to limit propagule and pathogen transmission on the park interface.
6. Park fire and management trail access remains operational and unobstructed, unless subject to prior arrangement with the NPWS Lower Hunter Area office.
7. Post works monitoring and maintenance of weed incursion and remediation works occurs to protect areas where significant modification of the canopy, embankments or soil surface has occurred.
8. Communication between TfNSW and NPWS continues to ensure the project is delivered in a safe, efficient and environmentally sensitive manner.

As discussed on the phone with Ranger Jo Erskine, NPWS would like to arrange a joint inspection of Hunter Wetlands NP near Iron bark Creek which may be directly impacted by the widening of the Highway. NPWS would also like to discuss the proposed changes to vehicle access to Ash Island.

NPWS would also like to explore the potential for a gate at or near the bridge entrance to Ash Island, an action in the Hunter Wetlands NP Plan of Management. The gate would close the island vehicle access at night, to limit illegal behaviour that occurs after hours, including the burning and dumping of stolen vehicles. Erecting the gate at this location has the potential for NPWS to limit gate vandalism, as it would potentially be in direct view from the highway and CCTV cameras. Specific location details would need to be discussed following field inspection and consider road / park visitor safety.

If you require any further information or wish to discuss NPWS requirements further, please contact Jo Erskine Lower Hunter on 4946 4104 or via email jo.erskine@environment.nsw.gov.au.

Yours sincerely



Mitchell Carter
Manager - Lower Hunter Area
NATIONAL PARKS AND WILDLIFE SERVICE



15 March 2021

Attention: Kieran Black
subsidenceadvisory@customerservice.nsw.gov.au
Subsidence Advisory NSW
117 Bull Street
PO Box 488G
Newcastle, NSW 2300
Australia

Dear Kieran,

RE: Consultation regarding the proposed widening of the Pacific Highway/Maitland Road/New England Highway (A43) at Hexham

Transport for NSW (Transport) are currently preparing an Environmental Impact Statement (EIS) and a Review of Environmental Factors for the Hexham Straight Widening and Ironbark Creek Bridge replacement Project (the proposal). The EIS assessment relates only to those areas of the proposal which are mapped State Environmental Planning Policy (Coastal Management) 2018 (CM SEPP). A Review of Environmental Factors (REF) is being prepared for the remaining areas of the proposal (refer to figure 1.3a & 1.3b). The purpose of this letter is to consult with Subsidence Advisory NSW on issues relevant to the preparation of both the EIS and REF.

The EIS and REF includes the assessment of potential impacts of the proposed development on the existing environment, during construction and operation (including cumulative impacts where necessary), and develop appropriate measures to avoid, minimise, mitigate offset, manage and/or monitor these potential impacts.

We are planning to finalise and display the EIS and REF for the proposal in late 2021.

Transport for NSW would like to offer the Subsidence Advisory NSW the opportunity to meeting to discuss the proposal and provide the current status of the environmental assessments.

It would be appreciated if you could provide any written comments regarding this proposal to the undersigned within 21 days from receipt of this letter. Any response from Subsidence Advisory NSW within that period will be considered by Transport in its consideration of this proposal.

Should you have any queries about this matter, please do not hesitate to contact the Project Development Manager, Andrew Thompson on 02 4908 7630 or by email Andrew.J.THOMPSON@transport.nsw.gov.au

Yours sincerely,

Andrew Thompson
Project Development Manager
Technical & Project Services
Infrastructure & Place
Transport for NSW
T 02 4908 7630 | M 0427 535 448
Level 7 266 King Street Newcastle NSW 2300

Attention: Andrew Thompson

Via email: Andrew.J.THOMPSON@transport.nsw.gov.au

Dear Andrew

Enquiry – Proposed widening of the Pacific Highway/Maitland Road/New England Highway (A43) at Hexham – EOTH21-00068

Thanks for your enquiry regarding the above project.

It is understood from your letter that the project involves the widening six kilometres of Maitland Road from four to six lanes, the demolition and replacement of an existing bridge over Ironbark Creek, the relocation of utilities, the construction of U-turn facilities on Sparke Street, Shamrock Street and Old Maitland Road and the modification to existing intersections along the route.

SA NSW records indicate that historical mine workings are not present within the project area and that the project is located entirely outside of a declared mine subsidence district (MSD). SA NSW approval is not required for infrastructure that is located outside of an MSD.

SA NSW would not require a mine subsidence assessment or remedial works to be carried out if the project were to be assessed under our current Merit Assessment Policy.

Please contact me on (02) 4908 4300 or at subsidedevelopment@customerservice.nsw.gov.au if you have any questions or wish to discuss.

Yours sincerely



Kieran Black
Technical Specialist

16 April 2021



16 March 2021

Attention: Annemarie Hopcroft
annemarie.hopcroft@epa.nsw.gov.au
Environmental Protection Authority
4 Parramatta Square
Locked Bag 5022
Parramatta, NSW 2124
Australia

RE: Consultation regarding the proposed widening of the Pacific Highway/Maitland Road/New England Highway (A43) at Hexham

Transport for NSW (Transport) are currently preparing an Environmental Impact Statement (EIS) and a Review of Environmental Factors for the Hexham Straight Widening and Ironbark Creek Bridge replacement Project (the proposal). The EIS assessment relates only to those areas of the proposal which are mapped State Environmental Planning Policy (Coastal Management) 2018 (CM SEPP). A Review of Environmental Factors (REF) is being prepared for the remaining areas of the proposal (refer to figure 1.3a & 1.3b). The purpose of this letter is to consult with Environmental Protection Authority on issues relevant to the preparation of both the EIS and REF.

The EIS and REF includes the assessment of potential impacts of the proposed development on the existing environment, during construction and operation (including cumulative impacts where necessary), and develop appropriate measures to avoid, minimise, mitigate offset, manage and/or monitor these potential impacts.

We are planning to finalise and display the EIS and REF for the proposal in late 2021.

Transport for NSW would like to offer the EPA the opportunity to meet to discuss the proposal and the current status of the environmental assessments in further detail.

It would be appreciated if you could provide any written comments regarding this proposal to the undersigned within 21 days from receipt of this letter. Any response from Environmental Protection Authority within that period will be considered by Transport in its consideration of this proposal.

Should you have any queries about this matter, please do not hesitate to contact the Project Development Manager, Andrew Thompson on 02 4908 7630 or by email Andrew.J.THOMPSON@transport.nsw.gov.au

Yours sincerely,

Andrew Thompson
Project Development Manager
Technical & Project Services
Infrastructure & Place
Transport for NSW
T 02 4908 7630 | M 0427 535 448
Level 7 266 King Street Newcastle NSW 2300



DOC21/278103-1 - EF15/1355

Transport for NSW
Hexham Straight Widening and Ironbark Creek Bridge
Replacement Project Team
Locked Bag 2030
NEWCASTLE NSW 2300

By email: Elle.Hutchinson@transport.nsw.gov.au
Andrew.J.THOMPSON@transport.nsw.gov.au

15 April 2021

Dear Mr Thompson & Ms Hutchinson

**Hexham Straight Widening and Ironbark Creek Bridge Replacement Project
Consultation regarding the preparation of an Environmental Impact Statement and
Review of Environmental Factors**

I refer to your email to the Environment Protection Authority (EPA) received 16 March 2021, inviting comment on the Environmental Impact Statement (EIS) and Review of Environmental Factors (REF) being prepared for the proposed Hexham Straight Widening and Ironbark Creek Bridge replacement Proposal (the Proposal) by Transport for NSW (TfNSW).

The EPA has considered the Proposal and has identified in **Attachment A** the information it requires to assess the Proposal.

The EPA's key information requirements for the project include an adequate description and assessment of:

- Impacts on water quality and site water management, with specific reference to potential impacts on the Hunter River and an assessment of background water quality.
- Potential noise impacts due to construction and operation with specific reference to proposed community consultation and management measures during the construction phase.

It is also noted that the project area is adjacent to the Hunter Wetlands National Park which makes up part of the Hunter Estuary Wetlands Ramsar site of international importance, the coastal wetlands of Hexham Swamp Nature Reserve and Shortland Wetlands.

In carrying out the assessment, TfNSW should refer to the relevant guidelines listed in **Attachment B** and any relevant industry codes of practice and best practice management guidelines.

TfNSW should also be aware that any commitments made in the EIS and REF may be formalised as environment protection licence conditions. Pollution control measures should not be proposed if they are impractical, unrealistic or beyond the financial viability of the

development. It is important that all conclusions are supported by adequate data.

If you require any further information regarding this matter, please contact Annemarie Hopcroft on 02 49086861.

A handwritten signature in cursive script, appearing to read 'Staylor'.

SIMON TAYLOR
A/ Unit Head Regulatory Operations
Regional North
Environment Protection Authority

ATTACHMENT A

EPA's Recommended EIS & REF Requirements - Hexham Straight Widening and Ironbark Creek Bridge replacement Project

1 Environmental impacts of the project

Impacts related to the following environmental issues need to be assessed, quantified and reported on;

- Air Quality
- Noise and Vibration
- Water and Soil Quality and Management
- Waste Management
- Dangerous Goods, Chemical Storage and Bunding

The EIS and REF should address the specific requirements outlined under each heading below and assess impacts in accordance with the relevant guidelines mentioned. A full list of guidelines is at Attachment B.

2 Licensing requirements

The EIS & REF should confirm if the proposal will involve activities listed in Schedule 1 of the Protection of the Environment Operations Act 1997 (POEO Act). If scheduled activities are to be undertaken as part of the Proposal, the scale of the activity should be clearly stated.

3 The Proposal and Premises

The objectives of the Proposal should be clearly stated and refer to:

- The size and type of the operation;
- The nature of the processes and the products, by-products and wastes produced;
- The types and quantities of any chemicals to be used and stored onsite;
- Proposed operational hours, including any heavy vehicle movements;
- Proposed maximum and average annual production rates that will occur at the premises; and
- Proposed staging and timing of the proposal.

The EIS & REF will need to fully identify all the processes and activities intended for the site over the life of the development. This will include details of:

- The location of the proposed facility and details of the surrounding environment;
- The proposed layout of the site;
- Appropriate land use zoning;
- Ownership details of any residence and/or land likely to be affected by the proposed operations;
- Maps/diagrams showing the location of residences and properties likely to be affected and other industrial developments, conservation areas, wetlands, etc. in the locality that may be affected by the facility;
- All equipment proposed for use at the site;
- All chemicals, including fuel, used on the site and proposed methods for their transportation, storage, use and emergency management;
- Clearly detail the boundary of the premises; and
- Methods to mitigate any expected environmental impacts of the development.

4 Air Issues

4.1 Air quality

The EIS & REF should include an air quality impact assessment (AQIA) in accordance with the Approved Methods for the Modelling and Assessment of Air Pollutants in NSW. The AQIA must identify and describe in detail all possible sources of air pollution and activities/ processes with the potential to cause air pollutants including odours and fugitive dust emissions beyond the boundary of any premises proposed to be licenced by an EPL. The AQIA should cover both the construction and operational phases of the development. The AQIA should include cumulative impacts associated with existing developments and any developments having been granted development consent but which have not commenced.

The EIS & REF should demonstrate that the Proposal will operate within EPA's objectives which are to minimise adverse effects on the amenity of local residents and sensitive land uses and to limit the effects of emissions on local, regional and inter-regional air quality.

The EIS & REF should describe in detail the measures proposed to mitigate the impacts and quantify the extent to which the mitigation measures are likely to be effective in achieving the relevant environmental outcomes.

The AQIA must describe the methodology used and any assumptions made to predict the impacts. Air pollutant emission rates, ambient air quality data and meteorological data used in the assessment must be clearly stated and justified.

5 Noise and Vibration

The following matters should be addressed in relation to noise and vibration impacts associated with the proposal. This includes identification of the hours of operations, assessment of all activities where proposed, and impacts on sensitive receivers associated with the proposed hours of operation. The following matters should be addressed as part of the EIS & REF.

General

- Construction noise associated with the proposed development should be assessed using the Interim Construction Noise Guideline (DECC, 2009).
- Vibration from all activities (including construction and operation) to be undertaken on the premises should be assessed using the guidelines contained in the Assessing Vibration: a technical guideline (DEC, 2006).
- Blast impacts should be demonstrated to be capable of complying with the guidelines contained in Australian and New Zealand Environment Council - Technical basis for guidelines to minimise annoyance due to blasting overpressure and ground vibration (ANZEC, 1990). If an alternative methodology or guidelines are presented in the EIS & REF, justification must be provided.

Road

- Noise on public roads from increased road traffic generated by land use developments should be assessed using the guidelines contained in the NSW Road Noise Policy (DECCW, 2011).
- Noise from new or upgraded public roads should be assessed using the NSW Road Noise Policy (DECCW, 2011).

Monitoring

- Detailed monitoring that will be conducted to assess the impacts of the proposal.

6 Water and Soils

6.1 Water Quality

Describe Proposal

- Describe the proposal including position of any intakes and discharges, volumes, water quality and frequency of all water discharges.
- Demonstrate that all practical options to avoid discharges have been implemented and environmental impact minimised where discharge is necessary.
- Where relevant include a water balance for the development including water requirements (quantity, quality and source(s)) and proposed storm and wastewater disposal, including type, volumes, proposed treatment and management methods and re-use options.

Background Conditions

- Describe existing surface and groundwater quality. An assessment needs to be undertaken for any water resource likely to be affected by the proposal. Issues to be discussed should include but are not limited to:
 - a description of any impacts from existing industry or activities on water quality.
 - a description of the condition of the local catchment e.g. erosion, soils, vegetation cover, etc.
 - an outline of baseline groundwater information, including, for example, depth to water table, flow direction and gradient, groundwater quality, reliance on groundwater by surrounding users and by the environment.
 - historic river flow data.
- State the Water Quality Objectives for the receiving waters relevant to the proposal. These refer to the community's agreed environmental values and human uses endorsed by the NSW Government as goals for ambient waters (<http://www.environment.nsw.gov.au/ieo/index.htm>). Where groundwater may be impacted the assessment should identify appropriate groundwater environmental values.
- State the indicators and associated trigger values or criteria for the identified environmental values. This information should be based on the ANZECC (2000) Guidelines for Fresh and Marine Water Quality as a minimum given the sensitive receiving environment of the Hunter River and any advice from Water NSW.
- State any locally specific objectives, criteria or targets which have been endorsed by the NSW Government.

Impact Assessment

- Describe the nature and degree of impact that any proposed discharges will have on the receiving environment, both surface water and groundwater.
- Detail contractual and other arrangements that will be put in place to prevent pollution from haul roads and unsealed roads per se, particularly rights of carriageways not owned by the proponent.

- Assess impacts against the relevant ambient water quality outcomes. Demonstrate how the proposal will be designed and operated to:
 - protect the Water Quality Objectives for receiving waters where they are currently being achieved; and
 - contribute towards achievement of the Water Quality Objectives over time where they are not currently being achieved.
- Where a discharge is proposed that includes a mixing zone, the proposal should demonstrate how wastewater discharged to waterways will ensure the ANZECC (2000) water quality criteria for relevant chemical and non-chemical parameters are met at the edge of the initial mixing zone of the discharge, and that any impacts in the initial mixing zone are demonstrated to be reversible.
- Propose water quality limits for any discharge(s) that adequately protects the receiving environment.
- Assess impacts on groundwater and groundwater dependent ecosystems.
- Describe how stormwater will be managed both during and after construction.

Monitoring

- Describe how predicted impacts will be monitored and assessed over time.

6.2 Soil

The EIS & REF should include:

- An assessment of potential impacts on soil and land resources should be undertaken, being guided by Soil and Landscape Issues in Environmental Impact Assessment (DLWC 2000). The nature and extent of any significant impacts should be identified. Particular attention should be given to:
 - Soil erosion and sediment transport - in accordance with Managing urban stormwater: soils and construction, vol. 1 (Landcom 2004) and vol. 2 (A. Installation of services; B Waste landfills; C. Unsealed roads; D. Main Roads; E. Mines and quarries) (DECC 2008).
 - Mass movement (landslides) - in accordance with Landslide risk management guidelines presented in Australian Geomechanics Society (2007).
 - Urban and regional salinity - guidance given in the Local Government Salinity Initiative booklets which includes Site Investigations for Urban Salinity (DLWC, 2002).
 - Assessment and management of any land contamination present (if applicable) with reference to Contaminated Land – EPA website, Guidelines for the NSW Site Auditor - Scheme – 3rd Edition (EPA, 2017), Guidelines for Consultants Reporting on Contaminated Sites (EPA, 2000), Sampling Design Guidelines (EPA, 1995), National Environment Protection (Assessment of Site Contamination) Measure
- A description of the mitigation and management options that will be used to prevent, control, abate or minimise identified soil and land resource impacts associated with the project. This should include an assessment of the effectiveness and reliability of the measures and any residual impacts after these measures are implemented.

7 Waste

The EIS & REF should:

- Include a detailed plan for in-situ classification of waste material, including the sampling locations and sampling regime that will be employed to classify the waste, particularly with regards to the identification of contamination hotspots.
- Identify, quantify, characterise and classify all waste that currently exists at the site.

Identify the intended end use, for example reuse or disposal, and the end use location(s) for the waste. Also, specify the mechanism under which waste will be reused or disposed, such as a Resource Recovery Exemption. Note: All waste must be classified in accordance with EPA's Classification Guidelines.

- Identify, characterise and classify all waste that will be generated onsite through excavation, demolition or construction activities, including proposed quantities of the waste. Note: All waste must be classified in accordance with EPA's Waste Classification Guidelines.
- Identify, characterise and classify all waste that is proposed to be disposed of to an offsite location, including proposed quantities of the waste and the disposal locations for the waste. This includes waste that is intended for re-use or recycling. Note: All waste must be classified in accordance with EPA's Classification Guidelines.
- Include a commitment to retaining all sampling and classification results for the life of the project to demonstrate compliance with EPA's Waste Classification Guidelines.
- Provide details of how waste will be handled and managed onsite to minimise pollution, including:
 - a) Stockpile location and management
 - Labelling of stockpiles for identification, ensuring that all waste is clearly identified and stockpiled separately from other types of material (especially the separation of any contaminated and non-contaminated waste).
 - Proposed height limits for all waste to reduce the potential for dust and odour. Procedures for minimising the movement of waste around the site and double handling.
 - Measures to minimise leaching from stockpiles into the surrounding environment, such as sediment fencing, geofabric liners etc.
 - b) Erosion, sediment and leachate control including measures to be implemented to minimise erosion, leachate and sediment mobilisation at the site during works. The EIS & REF should show the location of each measure to be implemented. The Proponent should consider measures such as:
 - Sediment traps
 - Diversion banks
 - Sediment fences
 - Bunds (earth, hay, mulch)
 - Geofabric liners
 - Other control measures as appropriateThe Proponent should also provide details of:
 - how leachate from stockpiled waste material will be kept separate from stormwater runoff;
 - treatment of leachate through a wastewater treatment plant (if applicable); and
 - any proposed transport and disposal of leachate off-site.
- Provide details of how the waste will be handled and managed during transport to a lawful facility. If the waste possesses hazardous characteristics, the Proponent must provide details of how the waste will be treated or immobilised to render it suitable for transport and disposal.
- Include details of all procedures and protocols to be implemented to ensure that any waste leaving the site is transported and disposed of lawfully and does not pose a risk to human health or the environment.
- Include a statement demonstrating that the Proponent is aware of EPA's requirements

with respect to notification and tracking of waste.

- Include a statement demonstrating that the Proponent is aware of the relevant legislative requirements for disposal of the waste, including any relevant Resource Recovery Exemptions, as gazetted by EPA from time to time.
- Outline contingency plans for any event that affects operations at the site that may result in environmental harm, including: excessive stockpiling of waste, volume of leachate generated exceeds the storage capacity available on-site etc.

8 Dangerous Goods, Chemical storage and Bunding

- The EIS and REF must outline all details regarding the transport, handling, storage and use of dangerous goods, chemicals and products, including fuel, both on site and with ancillary activities and describe the measures proposed to minimise the potential for leakage or the migration of pollutants into the soil/waters or from the site.
- The EIS & REF should identify any fuel or chemical storage areas proposed for the site.
- The EIS & REF should consider compliance with the following legislation, standards and guidelines where relevant:
 - Australian Standard AS1692:1989 Tanks for Flammable and combustible liquids; The DECC's "Bunding and Spill Management" Technical Guideline (November 1997) Australian Standard AS 1940:2004 The Storage and Handling of Flammable and Combustible Liquids
Australian Standard AS 4452-1997: The Storage and Handling of Toxic Substances; Australian/New Zealand Standard AS/NZS 4452:1997: The Storage and Handling of Mixed Classes of Dangerous Goods in Packages and Intermediate Bulk Containers; and
Road and Rail Transport (Dangerous Goods) Act 1997

9 Monitoring Programs

The EIS & REF should include a detailed assessment of any noise, air quality, weather, water or waste monitoring required during the construction and on-going operation of the site to ensure that the development achieves a satisfactory level of environmental performance. The evaluation should include a detailed description of the monitoring locations, sample analysis methods and the level of reporting proposed.

ATTACHMENT B – EPA’s Guidance Material (not exhaustive)

<u>Title</u>	<u>Web Address</u>
<i>Environmental Planning and Assessment Act 1979</i>	http://www.legislation.nsw.gov.au/maintop/view/inforce/act+203+1979+cd+0+N
<i>Protection of the Environment Operations Act 1997</i>	http://www.legislation.nsw.gov.au/maintop/view/inforce/act+156+1997+cd+0+N
<i>Protection of the Environment Operations (Noise Control) Regulation 2017</i>	https://legislation.nsw.gov.au/#/view/regulation/2017/449
<i>Protection of the Environment Operations (Clean Air) Regulation 2010</i>	https://legislation.nsw.gov.au/#/view/regulation/2010/428
<i>Protection of the Environment Operations (Waste) Regulation 2014</i>	https://legislation.nsw.gov.au/#/view/regulation/2014/666
<i>Waste Avoidance and Resource Recovery Act 2001</i>	https://legislation.nsw.gov.au/#/view/act/2001/58
<i>Contaminated Land Management Act 1997</i>	http://www.legislation.nsw.gov.au/#/view/act/1997/140
<i>Water Management Act 2000</i>	https://legislation.nsw.gov.au/view/html/inforce/current/act-2000-092
<i>Road and Rail Transport (Dangerous Goods) Act 1997</i>	https://www.legislation.nsw.gov.au/view/html/inforce/current/act-2008-095
<u>Licensing</u>	
Licensing Requirements	https://www.epa.nsw.gov.au/licensing-and-regulation/licensing
<u>Air/Odour</u>	
Approved methods for the Modelling and Assessment of Air Pollutants in NSW (2016)	http://www.epa.nsw.gov.au/resources/epa/approved-methods-for-modelling-and-assessment-of-air-pollutants-in-NSW-160666.pdf
Approved methods for the Sampling and Analysis of Air Pollutants in NSW (2007)	http://www.epa.nsw.gov.au/resources/air/07001amsaap.pdf
National Environment Protection (Ambient Air Quality) Measure	http://www.nepc.gov.au/nepms/ambient-air-quality
No EPA specific guidance material exists for the control of dust from construction sites. Consideration should be given to the POEO Act and the Local Government Air Quality Toolkit (DECC, 2007)	http://www.epa.nsw.gov.au/air/lgaqt.htm
Technical Framework - Assessment and Management of Odour from Stationary Sources in NSW (DEC, 2006) and Technical Notes - Assessment and Management of Odour from Stationary Sources in NSW (DEC, 2006)	http://www.epa.nsw.gov.au/air/odour.htm http://www.epa.nsw.gov.au/air/odour.htm
<u>Noise/Vibration</u>	
Interim Construction Noise Guideline (DECC, 2009)	https://www.epa.nsw.gov.au/your-environment/noise/industrial-noise/interim-construction-noise-guideline
Assessing Vibration: a technical guideline (DEC, 2006)	https://www.epa.nsw.gov.au/your-environment/noise/industrial-noise/assessing-vibration

NSW Industrial Noise Policy	https://www.epa.nsw.gov.au/publications/noise/17p0524-noise-policy-for-industry
NSW Road Noise Policy (DECCW, 2011)	http://www.epa.nsw.gov.au/resources/noise/2011236nswroadnoisepolicy.pdf
NSW Waste Avoidance and Resource Recovery Strategy 2014-2021	http://www.epa.nsw.gov.au/wastestrategy/warr.htm
Waste Classification Guidelines – 4 Parts (EPA, 2014)	http://www.epa.nsw.gov.au/wasteregulation/classify-waste.htm
Resource recovery exemption	https://www.epa.nsw.gov.au/your-environment/recycling-and-reuse/resource-recovery-framework/current-orders-and-exemption
<u>Water/Soils</u>	
ANZECC Guidelines for Fresh and Marine Water Quality (2018)	https://www.waterquality.gov.au/guidelines/anz-fresh-marine
NSW Water Quality and River Flow Objectives	http://www.environment.nsw.gov.au/ieo/index.htm
Applying Goals for Ambient Water Quality Guidance for Operations Officers – Mixing Zones	http://deccnet/water/resources/AWQGuidance7.pdf
Approved Methods for the Sampling and Analysis of Water Pollutant in NSW (2004)	https://www.epa.nsw.gov.au/-/media/epa/corporate-site/resources/water/approvedmethods-water.pdf
Soil and Landscape Issues in Environmental Impact Assessment (DLWC 2000)	https://www.shop.nsw.gov.au/publication/soil-and-landscape-issues-in-environmental-impact-assessment-technical-report-no-34-1324-6860-839
Managing urban stormwater: soils and construction, vol. 1 (Landcom, 2004) and Addendum Publications (Various)	http://www.environment.nsw.gov.au/stormwater/publications.htm
Landslide Risk Management (2007)	http://www.australiangeomechanics.org/resources/downloads/
Site Investigations for Urban Salinity (DLWC, 2002)	http://www.environment.nsw.gov.au/resources/salinity/booklet3siteinvestigationsforurbansalinity.pdf
Dryland Salinity Resources (Various)	http://www.environment.nsw.gov.au/salinity/solutions/urban.htm
<u>Waste</u>	
NSW Waste Avoidance and Resource Recovery Strategy 2014-2021	http://www.epa.nsw.gov.au/wastestrategy/warr.htm
Waste Classification Guidelines – 4 Parts (EPA, 2014)	http://www.epa.nsw.gov.au/wasteregulation/classify-waste.htm
Resource recovery exemption	https://www.epa.nsw.gov.au/your-environment/recycling-and-reuse/resource-recovery-framework/current-orders-and-exemption
<u>Contaminated Sites Assessment and Remediation</u>	
Contaminated Land – EPA website	https://www.epa.nsw.gov.au/your-environment/contaminated-land
Guidelines for the NSW Site Auditor Scheme – 3rd Edition (EPA, 2017)	https://www.epa.nsw.gov.au/publications/contaminatedland/17p0269-guidelines-for-the-nsw-site-auditor-scheme-third-edition
Guidelines for Consultants Reporting on Contaminated Sites (EPA, 2000)	http://www.epa.nsw.gov.au/resources/clm/20110650consultantsguidelines.pdf
Sampling Design Guidelines (EPA, 1995)	http://www.epa.nsw.gov.au/resources/clm/95059samppgdline.pdf

National Environment Protection (Assessment of Site Contamination) Measure	http://www.nepc.gov.au/nepms/assessment-site-contamination
<u>Chemical and Fuel Storage</u>	
Hazardous Chemicals	https://www.safework.nsw.gov.au/hazards-a-z/hazardous-chemical
List of Codes of Practice	https://www.safework.nsw.gov.au/resource-library/list-of-all-codes-of-practice
Environmental Guidelines: Storage and Handling of Liquids (EPA, 2007)	https://www.epa.nsw.gov.au/licensing-and-regulation/licensing/environment-protection-licences/compliance-audit-program/chemical-storage-handling-and-spill-management/storing-and-handling-liquids-trainers-manual



16 March 2021

Attention: Jayson McKellar
jayson.mckellar@rfs.nsw.gov.au
NSW Rural Fire Service
110 Mt Vincent Road
East Maitland, NSW 2323
Australia

Dear Jayson,

RE: Consultation regarding the proposed widening of the Pacific Highway/Maitland Road/New England Highway (A43) at Hexham

Transport for NSW (Transport) are currently preparing an Environmental Impact Statement (EIS) and a Review of Environmental Factors for the Hexham Straight Widening and Ironbark Creek Bridge replacement Project (the proposal). The EIS assessment relates only to those areas of the proposal which are mapped State Environmental Planning Policy (Coastal Management) 2018 (CM SEPP). A Review of Environmental Factors (REF) is being prepared for the remaining areas of the proposal (refer to figure 1.3a & 1.3b). The purpose of this letter is to consult with NSW Rural Fire Service on issues relevant to the preparation of both the EIS and REF.

The EIS and REF includes the assessment of potential impacts of the proposed development on the existing environment, during construction and operation (including cumulative impacts where necessary), and develop appropriate measures to avoid, minimise, mitigate offset, manage and/or monitor these potential impacts.

We are planning to finalise and display the EIS and REF for the proposal in late 2021.

Transport for NSW would like to offer the NSW Rural Fire Service the opportunity to meet to discuss the proposal and provide the current status of the environmental assessments.

It would be appreciated if you could provide any written comments regarding this proposal to the undersigned within 21 days from receipt of this letter. Any response from NSW Rural Fire Service within that period will be considered by Transport in its consideration of this proposal.

Should you have any queries about this matter, please do not hesitate to contact the Project Development Manager, Andrew Thompson on 02 4908 7630 or by email Andrew.J.THOMPSON@transport.nsw.gov.au

Yours sincerely,

Andrew Thompson
Project Development Manager
Technical & Project Services
Infrastructure & Place
Transport for NSW
T 02 4908 7630 | M 0427 535 448
Level 7 266 King Street Newcastle NSW 2300



15 March 2021

Attention: Angus Seberry
angus.seberry@hunterwater.com.au
Hunter Water
36 Honeysuckle Drive
PO Box 5171
HRMC, NSW 2310
Australia

Dear Angus,

RE: Consultation regarding the proposed widening of the Pacific Highway/Maitland Road/New England Highway (A43) at Hexham

Transport for NSW (Transport) are currently preparing an Environmental Impact Statement (EIS) and a Review of Environmental Factors for the Hexham Straight Widening and Ironbark Creek Bridge replacement Project (the proposal). The EIS assessment relates only to those areas of the proposal which are mapped State Environmental Planning Policy (Coastal Management) 2018 (CM SEPP). A Review of Environmental Factors (REF) is being prepared for the remaining areas of the proposal (refer to figure 1.3a & 1.3b). The purpose of this letter is to consult with Hunter Water on issues relevant to the preparation of both the EIS and REF.

The EIS and REF includes the assessment of potential impacts of the proposed development on the existing environment, during construction and operation (including cumulative impacts where necessary), and develop appropriate measures to avoid, minimise, mitigate offset, manage and/or monitor these potential impacts.

We are planning to finalise and display the EIS and REF for the proposal in late 2021.

Transport for NSW would like to offer Hunter Water the opportunity to meet to discuss the proposal and provide the current status of the environmental assessments.

It would be appreciated if you could provide any written comments regarding this proposal to the undersigned within 21 days from receipt of this letter. Any response from Hunter Water within that period will be considered by Transport in its consideration of this proposal.

Should you have any queries about this matter, please do not hesitate to contact the Project Development Manager, Andrew Thompson on 02 4908 7630 or by email Andrew.J.THOMPSON@transport.nsw.gov.au

Yours sincerely,

Andrew Thompson
Project Development Manager
Technical & Project Services
Infrastructure & Place
Transport for NSW
T 02 4908 7630 | M 0427 535 448
Level 7 266 King Street Newcastle NSW 2300



2 March 2021

State Emergency Service
Nicholas Kuster
Manager Planning and Engagement – Regional
Nicholas.kuster@ses.nsw.gov.au

Dear Nicholas,

RE: Consultation regarding the proposed widening of the Pacific Highway/Maitland Road/New England Highway (A43) at Hexham

Transport for NSW (Transport) proposes to widen about six kilometres of the Pacific Highway/Maitland Road/New England Highway (A43) from four lanes to six lanes, from the intersection with the Newcastle Inner City Bypass at Sandgate, through to Hexham Bridge, in Hexham, NSW (the overall proposal). The section of road known as the 'Hexham Straight' is located in the suburb of Hexham within the Newcastle local government area (LGA).

The proposal is subject to assessment under two planning pathways, a review of environmental factors (REF) under Part 5 of *Environmental Planning and Assessment Act 1979* (EP&A Act) and an environmental impact statement (EIS) under Part 4 of the EP&A Act. The majority of the proposal is subject to approval under Part 5 of the EP&A Act (known as the REF proposal) and a REF is currently being prepared by Jacobs on behalf of Transport for the REF proposal. However, a small part of the proposal (3.13 hectares) falls on land mapped as "coastal wetlands" under State Environmental Planning Policy (Coastal Management) 2018 (CM SEPP). As such, that part of the proposal (known as the EIS proposal) is subject to approval under Part 4 of the EP&A Act and will be assessed within an EIS. Further detail on the proposal is included in **Attachment A** and in Transport's December 2020 Community Update included as **Attachment B**.

The purpose of this letter is to formally consult with the State Emergency Service (SES) about the REF proposal (as described in **Attachment A**), under clause 12AA of the State Environmental Planning Policy (Infrastructure) 2007. The REF proposal will require upgrade of the Pacific Highway/Maitland Road (A43) starting about 290 metres to the south of the intersection with the Newcastle Inner City Bypass (A37) at Sandgate and extending to about 760 metres north of Hexham Bridge at Hexham on the New England Highway/Maitland Road. The highway would be widened from generally two lanes in each direction to three lanes in each direction. The REF proposal is also located next to the Hunter River and sections of the proposal are on flood liable land.

During construction Transport are proposing to keep two lanes open in both directions during the day and one lane in both directions during night works. However, it is noted that there are some areas along the proposal length where the road shoulders may not be wide enough for emergency vehicles to access if there is a road accident and the emergency vehicles may need to access the opposing carriageway.

To assist in our investigation and to ensure all potential issues are addressed, it would be appreciated if you could provide any comments regarding this proposal to the undersigned within 21 days from receipt of this letter. Any response from the SES within that period will be considered by Transport in its consideration of the REF proposal.

Transport would be pleased to provide further information if required. Should you have any queries about this matter, please do not hesitate to contact the Project Development Manager, Andrew Thompson on 02 4908 7630 or by email Andrew.J.THOMPSON@transport.nsw.gov.au

Yours sincerely,

A handwritten signature in blue ink, appearing to read 'A. Thompson', with a long horizontal flourish extending to the right.

Andrew Thompson

Project Development Manager
Technical & Project Services
Infrastructure & Place
Transport for NSW
T 02 4908 7630 | M 0427 535 448
Level 7 266 King Street Newcastle NSW 2300

25 February 2021

Nour Ette
Australian Rail Track Corporation
5/33 Newton St
Broadmeadow NSW 2292

Dear Nour,

RE: Consultation regarding the proposed widening of the Pacific Highway/Maitland Road/New England Highway (A43) at Hexham

Transport for NSW (Transport) proposes to widen about six kilometres of the Pacific Highway/Maitland Road/New England Highway (A43) from four lanes to six lanes, from the intersection with the Newcastle Inner City Bypass at Sandgate, through to Hexham Bridge, in Hexham, NSW (the overall proposal). The section of road known as the 'Hexham Straight' is located in the suburb of Hexham within the Newcastle local government area (LGA).

The proposal is subject to assessment under two planning pathways, a review of environmental factors (REF) under Part 5 of *Environmental Planning and Assessment Act 1979* (EP&A Act) and an environmental impact statement (EIS) under Part 4 of the EP&A Act. The majority of the proposal is subject to approval under Part 5 of the EP&A Act (known as the REF proposal) and a REF is currently being prepared by Jacobs on behalf of Transport for the REF proposal. However, a small part of the proposal (3.13 hectares) falls on land mapped as "coastal wetlands" under State Environmental Planning Policy (Coastal Management) 2018 (CM SEPP). As such, that part of the proposal (known as the EIS proposal) is subject to approval under Part 4 of the EP&A Act and will be assessed within an EIS. Further detail on the proposal is included in **Attachment A** and in Transport's December 2020 Community Update included as **Attachment B**.

The purpose of this letter is to formally consult with the Australian Rail Track Corporation (ARTC) about the REF proposal (as described in **Attachment A**), under section 16(1) of the State Environmental Planning Policy (Infrastructure) 2007. The Main North Rail Line lies to the west of the REF proposal and the rail corridor shares a common boundary on the western side of the Pacific Highway/Maitland Road (A43) about 100 metres to the north of Clark Street and extending north to the northbound entrance lane to the A1 northbound Pacific Highway and the crossing of the Hunter River over Hexham Bridge.

It is proposed to relocate the existing access to Hexham Station slightly to the north to the reconfigured signalised intersection at Maitland Road and Old Maitland Road, Hexham. Widening of the road formation at the signalised crossing opposite Hexham Bowling club is also required to provide adequate pedestrian facilities and a bus stop. These upgrades encroach into rail owned property.

To assist in our investigation and to ensure all potential issues are addressed, it would be appreciated if you could provide any comments regarding this proposal to the undersigned within 21 days from receipt of this letter. Any response from ARTC within that period will be considered by Transport in its consideration of the REF proposal.

Transport would be pleased to provide further information if required. Should you have any queries about this matter, please do not hesitate to contact the Project Development Manager, Andrew Thompson on 02 4908 7630 or by email Andrew.J.THOMPSON@transport.nsw.gov.au

Yours sincerely,

A handwritten signature in blue ink, appearing to read 'A. Thompson', with a stylized flourish extending to the right.

Andrew Thompson

Project Development Manager
Technical & Project Services
Infrastructure & Place
Transport for NSW
T 02 4908 7630 | M 0427 535 448
Level 7 266 King Street Newcastle NSW 2300