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Nowra Bridge Project

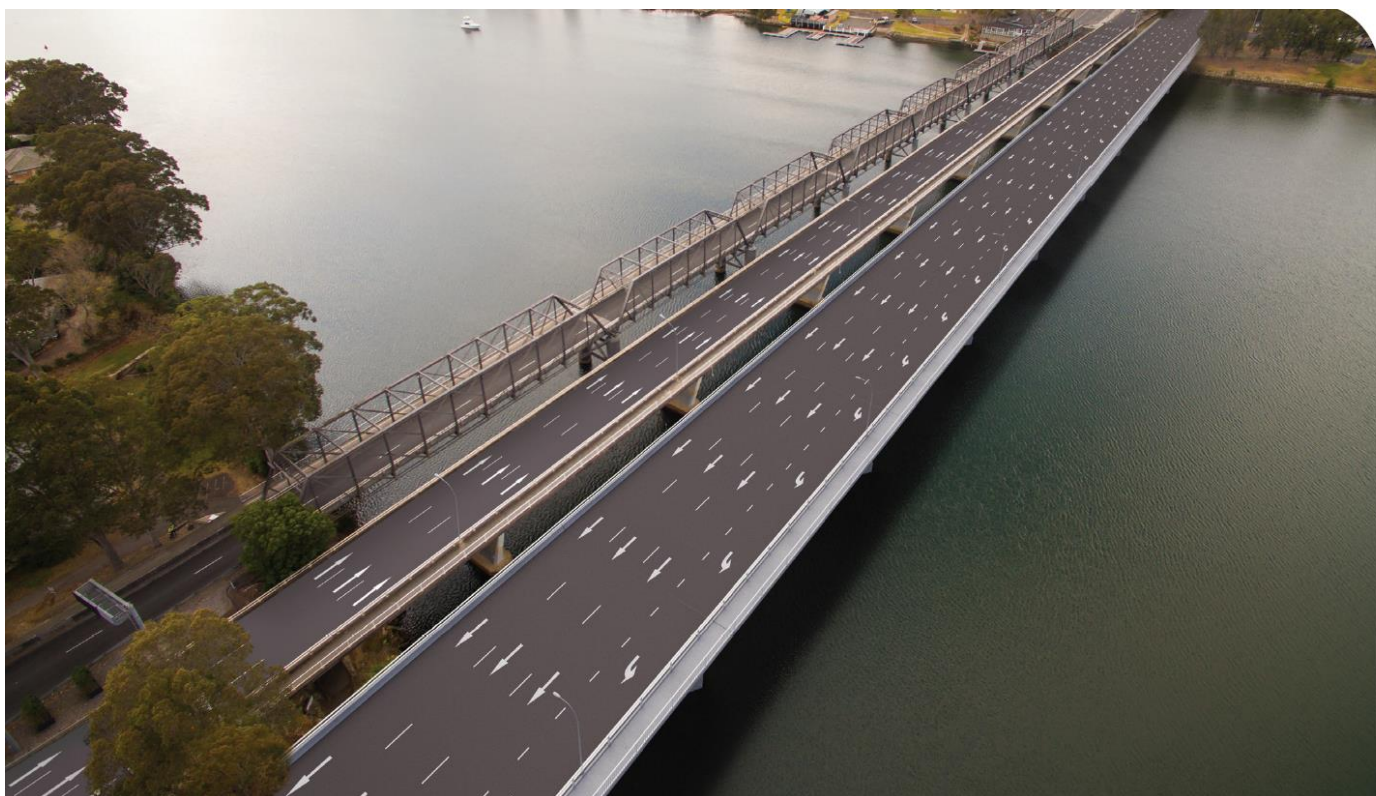
Community Consultation Report

Roads and Maritime Services | August 2018



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Artist's impression – Perspective of the new four-lane bridge over the Shoalhaven River – view from the north-west side of the river.

Executive summary

This Community Consultation Report provides a summary of the community consultation carried out by Roads and Maritime Services during the display of the preferred option for the Nowra Bridge project.

Based on multiple options considered and assessed, a preferred option for the project was determined and put on display for community comment from Monday 19 February until Friday 23 March 2018.

The preferred option included:

- a new four-lane northbound bridge immediately to the west of the existing bridge crossings
- reconfiguration of the existing northbound bridge to carry three southbound traffic lanes
- upgrade of the intersection at Illaroo Road including additional turning lanes
- upgrade of the Bridge Road intersection to a T-intersection with access to Pleasant Way removed
- a new intersection further to the south of the existing Pleasant Way intersection with all turning movements provided
- additional lanes on the Princes Highway between Bolong Road and Bridge Road
- keeping the existing southbound bridge for adaptive reuse such as a shared pedestrian and cyclist path.

During the consultation period Roads and Maritime received 84 written submissions and 250 online and written surveys. Of the written submissions received, one submission was received from Shoalhaven City Council and 83 submissions were received from community members, interest groups, transport providers, local businesses, and government agencies.

Key issues raised by the community during the consultation include:

- need for a bypass
- the proposal will not do much to ease congestion, especially during peak holiday seasons
- there are too many sets of traffic lights which affects efficient movement of traffic, and the proposal is adding a further set
- grade separated interchanges should be provided at Illaroo Road and Bridge Road instead of the proposed at grade arrangements
- provide a dedicated left-hand turn from Illaroo Road to the Princes Highway
- need for the railway line to be extended
- the old bridge should be removed and replaced with a new four-lane bridge
- construction impacts, especially to Illaroo Road.

The decision

In response to the feedback received, Roads and Maritime used the feedback from this consultation period to further refine the project design and inform the environmental assessment for the Nowra Bridge Project.

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1 Introduction

1.1 Background

Roads and Maritime Services (Roads and Maritime) is planning for a new Princes Highway bridge over the Shoalhaven River at Nowra. The project would also upgrade the intersections at Bolong Road, Illaroo Road, and Bridge Road.

The NSW Government has invested \$1.6 million, with the Australian Government committing \$10 million, toward planning a new bridge on the Princes Highway over the Shoalhaven River at Nowra. On 3 May 2018, the Australian Government announced it would contribute \$155 million toward the \$310 million estimated total project cost. On 18 June 2018, the NSW Government announced it would also contribute \$155 million to the project, with \$15.3 million allocated in the 2018-19 Budget to continue project planning.

During early investigations, the project team identified the following key issues with the current crossing of the Shoalhaven River at Nowra, particularly the existing southbound bridge (the historic 'Whipple truss' iron bridge):

- Some large and heavy trucks cannot use the existing southbound bridge due to height and load restrictions
- There is a high risk of damage and consequent closure of the existing southbound bridge from an impact by an over height vehicle
- There are increasing maintenance requirements and difficulties with the existing southbound bridge
- There are higher than State average annual crash rates on the Princes Highway on the approaches to the bridges
- There are increasing congestion issues in peak times from local traffic
- Increased demand and future traffic growth.

Around 50,000 vehicles cross the Shoalhaven River on an average day. Any extensive closures of the existing southbound bridge due to maintenance or emergency repairs would cause major delays in the network around Nowra. During early planning it became clear that the project should focus on a new bridge that could address these key issues.

1.2 The proposal

The preferred option is considered to be the most balanced proposal as it addresses the objectives of the project, meets the expectations of key stakeholders and the community, and ensures that long-term planning for the Princes Highway is not compromised while minimising impact on the surrounding community and environment as much as possible.

The preferred option for the Nowra Bridge Project includes:

- A new four-lane northbound bridge immediately to the west of the existing bridge crossings
- Reconfiguration of the existing northbound bridge to carry three southbound traffic lanes
- Upgrade of the intersection at Illaroo Road including additional turning lanes

- Upgrade of the Bridge Road intersection to a T-intersection with access to Pleasant Way removed
- A new intersection further to the south of the existing Pleasant Way intersection with all turning movements provided
- Additional lanes on the Princes Highway between Bolong Road and Bridge Road
- Keeping the existing southbound bridge for adaptive reuse such as a shared pedestrian and cyclist path.

A Preferred Option Report was prepared which detailed the analysis of options and the process used to select the preferred option. The report was made available to view or download from the Roads and Maritime website¹.

The locations of the preferred option and intersection upgrades as part of the Nowra Bridge Project are shown below.

¹ www.rms.nsw.gov.au/documents/projects/south-coast/nowra-bridges-shoalhaven-river/nowra-bridge-project-preferred-option-poster-february-2018.pdf

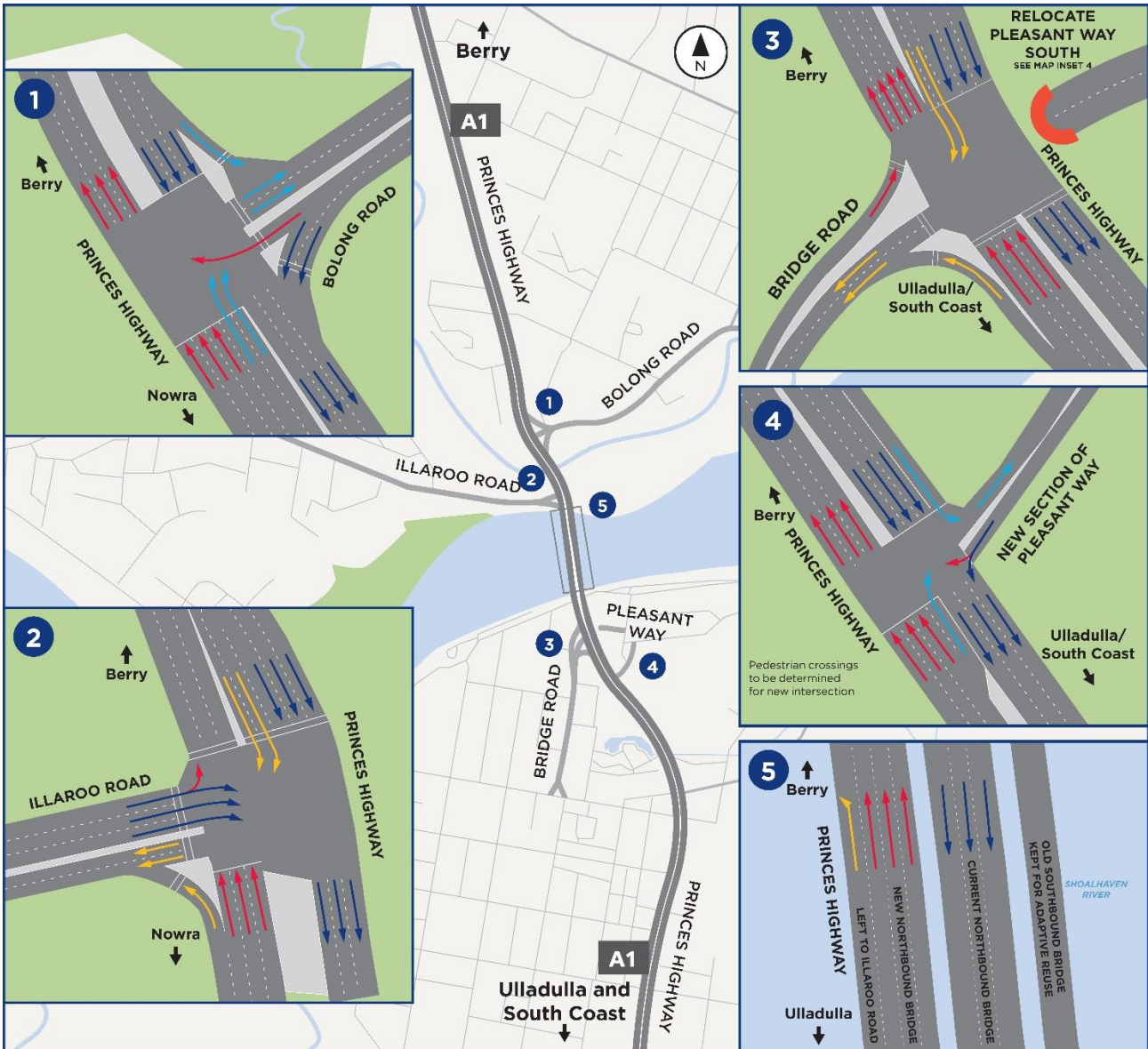


Figure 1-1: Concept design for the Nowra Bridge Project

Details of each insert are as follows:

1. Princes Highway/Bolong Road intersection
 - Minor adjustments to be confirmed during concept design
2. Princes Highway/Illaroo Road intersection:
 - An additional right turn lane to the Princes Highway from Illaroo Road
 - An addition right turn lane from the Princes Highway onto Illaroo Road
3. Princes Highway/Bridge Road intersection:
 - Relocated further south, see insert 4
 - Converted to a T-intersection
 - Access to Pleasant Way closed.

4. New Princes Highway/ local road intersection
 - New T-intersection located further to the south
 - All turning movements catered for

5. Bridges over the Shoalhaven River:

New northbound bridge:

- Located west of the existing bridges
- Four northbound traffic lanes
- Pedestrian/cyclist facilities

Existing northbound concrete bridge:

- Reconfigured to accommodate southbound traffic
- Southbound capacity would increase from two to three lanes

Existing southbound bridge:

- Kept for adaptive reuse such as a shared pedestrian and cyclist path
- Closed to vehicle traffic once construction of the new bridge is complete.

2 Consultation approach

2.1 Consultation objectives

The preferred option for the Nowra Bridge Project was placed on display for community comment from Monday 19 February to Friday 23 March 2018.

The purpose of the community consultation was to:

- Inform community members and stakeholders about the preferred option for the Nowra Bridge Project
- Seek comment, feedback, ideas and suggestions from the community to be considered to further refine the project design and develop the environmental assessment
- Provide an opportunity for the community to meet with the project team to ask questions and provide feedback on the preferred option
- Build a database of community members and stakeholders for Roads and Maritime to engage with through the development of the Nowra Bridge project
- Provide information about next steps in the project.

2.2 How consultation was done

A number of events and consultation activities were delivered so members of the community could learn more about the Nowra Bridge project upgrade, meet the project team, and have their say. A list of the key activities is provided in Table 2-1.

Community members were encouraged to provide feedback, leave comments, and make submissions at information sessions or via mail, email or phone contact with the project team. The community was able to contact Roads and Maritime and leave comments and submissions by:

Email: NowraBridgeProject@rms.nsw.gov.au

Phone: 1800 331 713

Facebook: M1 / Princes Highway upgrade – Roads and Maritime Services

Mail: Nowra Bridge Project, PO Box 477 Wollongong NSW 2500.

Table 2-1: Consultation activities

Activity type	Details of activity
Media release (Appendix A)	A media release (Appendix A) was distributed to all major Sydney and Shoalhaven publications on 19 February 2018 encouraging local community members and stakeholders to get involved in the consultation process.
Newspaper advertisements (Appendix B)	A total of 12 newspaper advertisements appeared in local papers between 21 February and 10 March 2018 to raise awareness of the consultation and information sessions. Publications included: South Coast Register (21, 23 and 28 February 2018, and 2, 7 and 9 March 2018). Illawarra Mercury (21, 24 and 28 February 2018, and

Activity type	Details of activity
	3,7 and 10 March 2018)
Community update flyer (Appendix C)	A community update flyer (A4 double sided) was distributed via a letterbox drop to 14,000 local properties and businesses to the Nowra, North Nowra, West Nowra, Bomaderry, Worrigeer, Cambewarra, Tapitallee and Meroo Meadow areas. Refer to Appendix D for a map of the distribution area.
Community update (Appendix E)	A community update (6 pages) was mailed out to all key stakeholders and copies provided at fixed displays at Shoalhaven City Council, Nowra Library, and TAFE NSW Nowra.
Static displays (Appendix F)	Posters were displayed at Shoalhaven City Council, Nowra Library, and TAFE NSW together with copies of the community update and Preferred Options Report.
Webpage	The project webpage ² was updated on 19 February 2018 with the latest project information including the community update, online survey, Preferred Options Report, display locations and project drop-in sessions, and how to submit feedback.
Online survey	Roads and Maritime received 250 online and written forms from the community during the display period between Monday 19 February and Friday 23 March 2018. The online survey results are summarised in Appendix G.
Media coverage	Six stories were published in local newspapers during the Preferred Option Report display period as follows: <ul style="list-style-type: none"> • Illawarra Mercury (19 February 2018) • South Coast Register (Three stories on 19 February, one story on 20 February, one story on 21 February 2018). There was also in coverage in the Sydney Morning Herald and on WIN Television.
Community information sessions	Five community information sessions were held at: <ul style="list-style-type: none"> • Stocklands Nowra, 60 East Street, Nowra, Thursday 1 and Saturday 3 March 2018, 11am – 3pm • North Nowra Shops, 1–13 McMahons Road, North Nowra, Thursday 8 and Saturday 10 March 2018, 11am – 3pm • Nowra School of Arts Annex, Berry Street, Nowra, Saturday 17 March 2018, 10am – 1pm. About 630 community members attended the five community information sessions.

² www.rms.nsw.gov.au/nowrabridge.

3 Consultation summary

3.1 Overview

During the consultation period Roads and Maritime received 84 written submissions and over 250 online and written surveys. Of the written submissions received, one submission was received from Shoalhaven City Council and 83 submissions were received from community members, interest groups, transport providers, local businesses and government agencies.

Key issues raised by the community during the consultation include:

- Need for a bypass
- The proposal will not do much to ease congestion, especially during peak holiday seasons
- There are too many sets of traffic lights which affects efficient movement of traffic, and the proposal is adding a further set
- Grade separated interchanges should be provided at Illaroo Road and Bridge Road instead of the proposed at grade arrangements
- Provide a dedicated left-hand turn from Illaroo Road to the Princes Highway
- Need for the railway line to be extended
- Need for the Princes Highway to be upgraded from Nowra to Wollongong
- Construction impact, especially to Illaroo Road.

These issues and concerns have been summarised and responded to by Roads and Maritime in Section 3.3.

3.2 Online and written survey

Roads and Maritime received 250 online and written from the community during the display period between Monday 19 February and Friday 23 March 2018.

The survey included questions about the project and the consultation process. A summary of the information provided through the feedback forms is detailed below.

246 people provided information about the distance and direction they live from the bridges, with 97 living north within 10 kilometres, 42 living south within 10 kilometres, 54 living north more than 10 kilometres, and 53 living south more than 10 kilometres.

246 people provided information about how often they travel over the current bridges during the week, with 98 respondents travelling over the bridges one to four times a week, 67 travelling 5–10 times a week, 57 travelling 10–20 times a week, and two travelling 21 or more times a week.

Respondents were asked if they agreed the project was a good outcome for the community (considering 90 per cent of traffic travelling on the Princes Highway over the Shoalhaven River has origins or destinations in the Nowra area). 246 people responded to the question, with 93 completely agreeing with the statement, 62 somewhat agreeing, 40 somewhat disagreeing, and 51 completely disagreeing with the statement.

Respondents were asked if the proposed new northbound bridge should be four lanes and include a turning lane to Illaroo Road. 243 people responded and 140 completely agreed, 39 somewhat agreed, 22 somewhat disagreed, and 42 completely disagreed. Additional comments provided included a need for cycle and walking paths, an overpass at the Illaroo Road intersection, and bypassing Nowra entirely.

Respondents were asked if the proposed improvements to the Princes Highway intersections at Bolong Road, Illaroo Road, Bridge Road and Pleasant Way would improve traffic flow after opening. 244 people responded, with 53 completely agreeing, 77 somewhat agreeing, 51 somewhat disagreeing, and 63 completely disagreeing. Additional comments included the need for a bypass of Nowra, more information about southbound traffic, the desire for a railway, and resistance to another set of traffic lights.

Respondents were asked if they had any general comments on the preferred option of the Nowra Bridge project. Thirteen people responded and their comments included:

- The need for a bypass of Nowra
- Suggestion that the project should use local labourers to bring employment to the area
- A question of whether a new bridge needs to be built so close to existing bridges and that it may function more effectively if it was located further down the river
- The need for an overpass or underpass instead of more traffic lights to ease southbound traffic
- The need to consider future traffic increases
- Improved access from Illaroo Road
- A community opening.

The community was also asked for feedback on the consultation process and the display material.

The community was asked if it found the display material informative and 241 people responded, with 64 very informative, 133 somewhat informative, 26 somewhat uninformative, and 18 very uninformative.

237 people responded to the question asking if they found the display material easy to access, with 203 responding yes and 34 responding no.

235 people responded to the question asking how they would like to be kept informed about the project, with 99 selecting email, 65 selecting social media, 22 selecting mail-outs, and 49 selecting website.

3.3 Issues and responses

The following table details the issues raised in submissions received during the consultation period and Roads and Maritime’s responses to each issue.

Ref	Issue category	Sub-issue	Total number of submissions	Issue(s) raised	Roads and Maritime response
1	Traffic and road safety	Number and/or phasing of traffic lights	13	An additional set of traffic lights at the new local road connection combined with the existing traffic lights would result in less efficient traffic movement along this section of the highway.	<p>Traffic light operations on the Princes Highway would be coordinated and optimised for the intersections within and to the south of the project area.</p> <p>The access between Pleasant Way and Princes Highway will be closed as part of the project. A new local road connecting Lyrebird Drive to the Princes Highway will be constructed about 300 meters south of Pleasant Way to provide access to the residential area to the south east of the bridge following the closure of Pleasant Way. The new local road connection would provide left in and left out access to the Princes Highway and will not require new traffic lights.</p> <p>Motorists travelling north on the highway wanting to access the town centre and riverfront east of the highway would so via Moss Street, Ferry Lane and Riverside Drive.</p>
2	Traffic and road safety	Number and/or phasing of traffic lights	3	Concerns over the number of traffic lights on the Princes Highway within Nowra.	<p>The access between Pleasant Way and Princes Highway will be closed as part of the project. A new local road connecting Lyrebird Drive to the Princes Highway will be constructed about 300 meters south of Pleasant Way to provide access to the residential area to the south east of the bridge following the closure of Pleasant Way. The new local road connection would provide left in and left out access to the Princes Highway and will not require</p>

Ref	Issue category	Sub-issue	Total number of submissions	Issue(s) raised	Roads and Maritime response
					new traffic lights. As noted above, traffic light operations would be coordinated and optimised for the intersections within and to the south of the project area.
3	Traffic and Road Safety	Number and/or phasing of traffic lights	1	Concerns over the effectiveness of extra highway lane going south and the additional traffic lights before the Moss Street traffic lights.	<p>An additional southbound lane has been provided to improve traffic flow through the area. The additional lanes would store more vehicles at the traffic lights, which would allow more vehicles through on the green phase.</p> <p>The new local road connection would provide left in and left out access to the Princes Highway and will not require new traffic lights to be installed.</p> <p>As previously noted, traffic lights along this stretch of the highway would be coordinated and optimised.</p>
4	Traffic and Road Safety	Number and/or phasing of traffic lights	1	Due to the number of traffic lights, improvements to traffic light phasing between Bolong Road and Moss Street will be required.	<p>The intersections are currently operating at close to capacity during peak times. The project will result in upgrades to the layouts of intersections including additional lanes and changes to turning movements which would increase the capacity of the intersections.</p> <p>As previously noted, traffic lights along this stretch of the highway will be coordinated and optimised.</p>
5	Traffic and Road Safety	Number and/or phasing of traffic lights	1	The number of intersections between Bridge Road and Kalandar Street.	The access between Pleasant Way and Princes Highway will be closed as part of the project. A new local road connecting Lyrebird Drive to the Princes Highway will be constructed about 300 meters south of Pleasant Way to provide access to the residential area to the south east of the bridge following the closure of Pleasant Way. The new local road connection would provide left in and left out access to the Princes Highway and will not require

Ref	Issue category	Sub-issue	Total number of submissions	Issue(s) raised	Roads and Maritime response
					<p>new traffic lights.</p> <p>The project would add one intersection between Bridge Road and Moss Street. The new local road connection would provide left in and left out access to the Princes Highway and will not require new traffic lights.</p> <p>The project would also involve changes to the layout of other intersections which would improve the efficient operation of these intersections.</p> <p>As previously noted, traffic lights along this stretch of the highway will be coordinated and optimised</p>
6	Traffic and Road Safety	Number and/or phasing of traffic lights	1	There is a need to reduce the number of traffic lights between Bolong Road and Kalandar Street.	<p>Traffic lights are required in order to manage the competing demands of local traffic on side streets as well as through movements along the Princes Highway. Without traffic lights in place it would become very difficult for vehicles to access the Princes Highway during peak periods from local roads such as Bolong Road, Illaroo Road and Bridge Road.</p> <p>There are no additional traffic lights proposed as part of the project.</p> <p>Flyovers, or grade separation (of traffic), were investigated earlier in the project's development. It was identified that these would provide a small benefit to intersection performance but these would have much greater property and environmental impacts.</p>
7	Traffic and Road Safety	Number and/or phasing of traffic lights	1	The existing traffic congestion is caused by the number of intersections in Nowra, especially Bolong Road, Illaroo	Bolong Road, Illaroo Road and Bridge Road are key intersections in the Nowra road network, providing access to and from local areas via the Princes Highway.

Ref	Issue category	Sub-issue	Total number of submissions	Issue(s) raised	Roads and Maritime response
				Road and Bridge Road.	<p>Movement of vehicles through these intersections is managed using traffic lights to provide equitable access for local traffic and through traffic.</p> <p>The traffic modelling carried out for the project identified there would be improvements in the performance of these key intersections once the project had been constructed.</p> <p>Flyovers, or grade separation (of traffic), were investigated earlier in the project's development. It was identified that these would provide a small benefit to intersection performance but these would have much greater property and environmental impacts.</p>
8	Traffic and Road Safety	Number and/or phasing of traffic lights	1	Replace traffic lights with roundabouts.	Removing traffic lights and replacing these with roundabouts would only add further traffic delays and queueing, as roundabouts provide less control to manage traffic movements at intersections. Without traffic lights in place it would become very difficult for vehicles to access the Princes Highway during peak periods from side streets such as Bridge Road and Bolong Road.
9	Traffic and Road Safety	Number and/or phasing of traffic lights	1	Retain the existing southbound bridge and remove the traffic lights at Illaroo Road.	The existing southbound bridge has only two lanes results in traffic congestion as the road is reaching its capacity; there are over height constraints for larger vehicles and the cost to maintain it is increasing. These issues would be addressed as the project proposes to use the existing northbound bridge for southbound traffic and it will be reconfigured to have an additional lane and no height restrictions. It is an existing concrete bridge

Ref	Issue category	Sub-issue	Total number of submissions	Issue(s) raised	Roads and Maritime response
					that requires less maintenance than the old steel bridge. The traffic lights at Illaroo Road are essential to allow safe traffic movements to and from North Nowra, and there are no plans to remove them. There is currently no other viable alternative access to the North Nowra area for day to day traffic.
10	Traffic and Road Safety	Number and/or phasing of traffic lights	1	Provide left turn slip lanes in and out of Illaroo Road and no right turn out of Illaroo Road.	<p>The upgraded intersection of the Princes Highway and Illaroo Road will include:</p> <ul style="list-style-type: none"> • Two southbound right turn lanes from the Princes Highway into Illaroo Road • Three dedicated right turn lanes and one dedicated left turn lane from Illaroo Road to Princes Highway • An acceleration and merge lane for northbound traffic turning into Illaroo Road from the Princes Highway <p>Retention of the right turn movement is required to provide access for vehicles travelling southbound from North Nowra.</p> <p>Three lanes will be provided for traffic to turn right from Illaroo Road. This, together with improved coordination of the traffic lights network would improve the operation of this intersection.</p>
11	Traffic and Road Safety	Other	1	Lanes provided both southbound and northbound on the Princes Highway and the three lanes onto the bridge from Illaroo Road will have a positive impact on traffic flows.	Positive response is noted.

Ref	Issue category	Sub-issue	Total number of submissions	Issue(s) raised	Roads and Maritime response
12	Traffic and Road Safety	Other	1	A second bridge would provide alternative route in the case of an accident.	The safety benefit is noted.
13	Traffic and Road Safety	Traffic modelling	19	<p>Traffic modelling assumptions:</p> <ul style="list-style-type: none"> • Do not address future population growth in the area • Assumptions on local traffic volumes are incorrect • Traffic modelling based on growth rates of 1.7%, not the current growth rate of 2.7% • Does not address tourism • Does not address holiday and long weekend peaks • Should consider fluctuations in future traffic. 	<p>Historical traffic data and Shoalhaven City Council's strategic traffic model were reviewed to determine the appropriate growth rates for the future traffic demands on the road network in the vicinity of the project. The model considers future land use changes in the surrounding area that would influence the volume of traffic on the road network, and takes into account future development sites and general population growth.</p> <p>While the most recent traffic data indicates growth on the Princes Highway to be in the order of 1–1.5% per annum, a greater growth rate of 1.7% per annum has been adopted for the analysis. This considers traffic growth that would arise from future development in the area (as forecast in the Council traffic model) and aligns with the rate identified in the Princes Highway Corridor Strategy document for the Princes Highway between Gerringong and Falls Creek.</p> <p>It is accepted practice to not base the ultimate design on the 'worst case' scenario of traffic during holiday periods. Instead the proposed design takes into account typical conditions on a weekday during peak hours. Designing for the worst case would not provide best value for money and result in a poor urban outcome due to the size of intersections required to accommodate traffic flows.</p> <p>The traffic modelling has considered traffic growth up to the year 2046, which is about 20-25 years after the opening of the new bridge. It should also be noted that</p>

Ref	Issue category	Sub-issue	Total number of submissions	Issue(s) raised	Roads and Maritime response
					the accuracy and reliability of traffic modelling forecasts reduce with the length of the forecast period.
14	Traffic and Road Safety	Traffic modelling	2	The proposal does not take into account strategic planning or planned growth within the area, including major residential development north of Nowra and major employment in south of Nowra including Defence.	Historical traffic data and Shoalhaven City Council's strategic traffic model were reviewed to determine the appropriate growth rates for the future traffic demands on the road network in the vicinity of the project. The model considers future land use changes in the surrounding area that would influence the volume of traffic on the road network, and takes into account future development sites, general population growth, and other relevant matters including employment generation. In this regard, a slightly higher growth rate of 2.3% has been adopted for Illaroo Road and this is in line with Council's strategic traffic model.
15	Traffic and Road Safety	Number and/or phasing of traffic lights	1	Traffic congestion at the 8:30 am to 9:30 am peak and during holidays are not going to be reduced if traffic lights remain at Bolong Road, Illaroo Road and Pleasant Way.	<p>Without traffic lights at these intersections, it would be very difficult for vehicles to access the Princes Highway during peak periods from side roads such as Bridge Road and Bolong Road.</p> <p>Traffic light operations would be coordinated and optimised for the intersections within and to the south of the project area.</p> <p>Flyovers, or grade separation (of traffic) were investigated earlier in the project's development. It was identified that these would provide a small benefit to intersection performance but these would have much greater property and environmental impacts.</p>

Ref	Issue category	Sub-issue	Total number of submissions	Issue(s) raised	Roads and Maritime response
16	Traffic and Road Safety	Traffic modelling	2	Disagree with the proposal or the need for a new bridge.	<p>The investigations carried out for the project are considered to have demonstrated that the collective capacity of the two existing bridges, together with a range of issues associated with the southbound bridge, are having a major effect on the efficiency of traffic movements along this section of the Princes Highway.</p> <p>Investigations have shown that the majority of journeys occur within the Nowra Bomaderry area, and that a new bridge in the vicinity of the existing bridges would provide additional capacity and ease congestion.</p>
17	Traffic and Road Safety	Traffic modelling	1	Access from Illaroo Road in peak times and holiday periods needs to improve.	<p>The problems with existing congestion at this intersection are acknowledged. The upgraded intersection will include three dedicated right turn lanes and one dedicated left turn lanes from Illaroo Road to the Princes Highway.</p> <p>The project would provide additional capacity at this intersection and would allow more vehicles to exit Illaroo Road on each green light phase.</p> <p>It is likely that congestion would continue to be experienced during holiday periods but with the extra turning lanes this would be less than currently experienced.</p>
18	Traffic and Road Safety	Traffic modelling	1	Berry Bypass and upgrades north will increase traffic flows.	<p>The traffic modelling has taken into account the expected increase in traffic on the Princes Highway as a result of the Berry Bypass project.</p>

Ref	Issue category	Sub-issue	Total number of submissions	Issue(s) raised	Roads and Maritime response
19	Traffic and Road Safety	Traffic modelling	1	The current proposal would not address the traffic issues at Illaroo Road, Bridge Road and Bolong Road.	The project would include changes to all three intersections, including additional lanes to improve capacity. Traffic modelling carried out for the project has identified that the efficiency of operation of these intersections will be improved.
20	Traffic and Road Safety	Traffic modelling	1	The new bridge will result in traffic congestion at the next traffic lights.	The traffic modelling carried out for the project has identified that the additional traffic lane on the new bridge and the changes to key intersections would improve traffic flow through this part of the Nowra Bomaderry road network.
21	Traffic and Road Safety	Traffic modelling	1	The project does not increase the capacity of the highway on the approaches to the bridge.	The project would add an additional northbound and southbound traffic lane on the Princes Highway from north of Bolong Road through to Moss Street. An additional second right turn lane into Illaroo Road and extended right turn lanes at Bridge Road will also be provided.
22	Traffic and Road Safety	Traffic modelling	1	Congestion in Nowra is a substantial problem, particularly on weekends and holidays, due to insufficient lanes for traffic across the river.	The project would provide an additional northbound and southbound traffic lane on the Princes Highway from north of Bolong Road through to Moss Street. This additional capacity, together with the intersection improvements would contribute to less congestion. It is likely that congestion would continue to be experienced during holiday periods but with the additional capacity this would be less than currently experienced.

Ref	Issue category	Sub-issue	Total number of submissions	Issue(s) raised	Roads and Maritime response
23	Traffic and Road Safety	Traffic modelling	1	Does the modelling take into the account peak growth development approved along Moss Vale Road?	<p>Historical traffic data and Shoalhaven City Council's strategic traffic model were reviewed to determine the appropriate growth rates for the future traffic demands on the road network in the vicinity of the project.</p> <p>The model considers future land use changes in the surrounding area, including Moss Vale Road, which would influence the volume of traffic on the road network, and takes into account future development sites and general population growth. In this regard, a slightly higher growth rate of 2.3% has been adopted for Illaroo Road in line with Council's strategic traffic model while a growth rate of 1.7% has been adopted for the Princes Highway.</p>
24	Traffic and Road Safety	Traffic modelling	2	Three northbound lanes merging into two lanes will cause traffic congestion.	<p>The location where the three lanes will merge to two lanes is north of Bolong Road and away from the intersection. This is unlikely to have an impact on congestion. About one third of northbound traffic has left the highway at this location, turning off to Illaroo Road or Bolong Road.</p> <p>The traffic modelling has identified that the upgrades to the Illaroo Road and Bolong Road intersections would provide an improvement in the efficiency of vehicle movements through these intersections.</p>
25	Traffic and Road Safety	Traffic modelling	2	The preferred option should address traffic projections for the next 30 years.	<p>The proposal would improve traffic capacity within the study area, most notably at;</p> <ul style="list-style-type: none"> The southbound crossing over the Shoalhaven River increasing from two lanes to three lanes

Ref	Issue category	Sub-issue	Total number of submissions	Issue(s) raised	Roads and Maritime response
					<ul style="list-style-type: none"> • The new northbound crossing over the Shoalhaven River increasing from three lanes to four lanes • Widening of Illaroo Road eastbound from two lanes to four lanes at the Princes Highway intersection • Extended right turning lanes on to Bridge Road • A new unsignalised intersection south of Pleasant Way <p>Traffic modelling has identified the above works would significantly improve the performance of the road network and provide an improvement on the current situation up until the year 2046 if there are no further improvements to the Nowra Bomaderry road network.</p> <p>However, the investigations that are carried out as part of the Princes Highway Corridor Strategy are likely to identify other projects to further improve the Princes Highway and local road network.</p>
26	Traffic and Road Safety	Traffic modelling	2	Proposal is inadequate to manage the future growth of the region.	The traffic modelling carried out for the project considered several future years up to 2046. The modelling identified that the project would significantly improve the performance of the road network and provide an improvement on the current situation.
27	Traffic and Road Safety	Traffic modelling	1	Does not agree the proposal will provide the medium to long term relief claimed, and will only marginally increase local capacity.	The traffic modelling carried out for the project considered several future years up to 2046. This identified that the project would improve the performance of the road network and provides an improvement on the current situation.
28	Traffic and Road	Traffic	2	How has the forecast growth in	Historical traffic data and Shoalhaven City Council's

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	Safety	modelling		Bangalee, North Nowra and Bomaderry been taken into assessment of the design?	strategic traffic model were reviewed to determine the appropriate growth rates for the future traffic demands on the road network in the vicinity of the project. The model considers future land use changes in the surrounding area, including those noted, that would influence the volume of traffic on the road network, and takes into account future development sites, general population growth, and other relevant matters.
29	Traffic and Road Safety	Traffic modelling	2	Figure 4.1 of the PAR suggests VHT for the preferred option will be back at 2018 levels in 2026/27, only four years after opening. How will the proposal address the congestion issues at the Illaroo Road intersection given the current problems?	VHT, or vehicle hours travelled, is not a representation of individual vehicle travel time. It is a measure of the total travel time for all vehicles in the network being modelled/investigated. As the number of vehicles in the network increases (due to forecast traffic growth in the area) so will VHT. The traffic modelling indicates that, as a result of the capacity improvements associated with the project, the average time spent by vehicles travelling on the road network would be lower up until the year 2046 compared to that currently experienced.
30	Traffic and Road Safety	Traffic modelling	2	Given there will be turn restrictions on Bridge Road at the intersection with Scenic Drive, there is a need for Roads and Maritime to do a thorough assessment of the resulting traffic impact at the intersection of Bridge Road and Hyam Street. Council believes traffic lights will be required in the short term.	Roads and Maritime will continue to consult further with Council on the section of the project surrounding Bridge Road, Hyam Street and Scenic Drive.
31	Traffic and Road	Traffic	2	The new infrastructure needs to be	The great majority of traffic using this section of the

Ref	Issue category	Sub-issue	Total number of submissions	Issue(s) raised	Roads and Maritime response
	Safety	modelling		comparable to the new work to the north of the Shoalhaven River and meet the future social and economic requirements of the Nowra area.	highway is associated with local journeys in the Nowra Bomaderry area. A bypass of Nowra or the provision of a dual carriageway at motorway standard is not warranted at this time.
32	Traffic and Road Safety	Traffic modelling	1	<p>The preferred option report:</p> <ul style="list-style-type: none"> • Does not provide any information on how RMS arrived at a preferred river crossing • Limited in detail compared to 2014 report • Traffic flow and urban growth have not been adequately addressed • Is not a quality technical document with adequate technical, scientific and social data. 	<p>The Preferred Option report is intended as a short summary to provide context around the preferred option display, including basic intersection layouts and numbers of lanes. This allows for community and stakeholder feedback to be sought and considered before the more detailed and final design work takes place.</p> <p>Further information around options and the other matters noted will be available in the environmental assessment.</p>
33	Traffic and Road Safety	Construction traffic impact	1	Concern about disruption to traffic flows, disturbance and costs during construction including access to North Nowra and Illaroo Road access.	<p>The project aims to keep the current number of lanes open for all approaches during construction. However at times it may be necessary for temporary lane closures and as far as practicable, these would occur outside of peak traffic periods to minimise impacts on road users.</p> <p>Some traffic delays and disruption is an unavoidable aspect of any road upgrade project. Construction staging would be planned to manage this to the greatest extent practicable. Through traffic movements on the highway and access to private properties would be maintained during construction.</p>

Ref	Issue category	Sub-issue	Total number of submissions	Issue(s) raised	Roads and Maritime response
34	Traffic and Road Safety	Freight	1	The safe movement of longer trucks through the eight roundabouts on either side of the bridge and through Nowra town centre.	While these roundabouts are outside the project area, the intersection designs provide for articulated vehicle movements in accordance with Austroads design guidelines.
35	Bypass	Without bridge	28	A bypass would be a better option, providing a long-term solution, addressing freight, tourists and economic growth. Consider bypass to include Milton and Ulladulla.	<p>The need for a bypass has been examined and the traffic modelling has shown this would only take about 15 percent of current traffic out of the Nowra Bomaderry network.</p> <p>A bypass of Nowra would be a significant investment for the community, and any proposal would likely need to provide evidence of major benefits for the region.</p> <p>Building a new bridge now would not preclude the future planning of a bypass if/when traffic demand is reached. However, at this point in time, the Princes Highway Corridor Strategy, prepared by Roads and Maritime in 2016, does not identify a full bypass of Nowra as a priority.</p> <p>Bypass projects in other locations are likely to make major improvements to safety and reduce traffic congestion. For example, the Berry bypass was projected to take about 75-80 percent of traffic out of Berry, and the Albion Park Rail bypass, is expected to take up to 70-75 percent of vehicles out of Albion Park Rail when built.</p>
36	Bypass	Without bridge	2	Build a bypass with a bridge over Pig Island.	This option was considered together with other bypass options to the east of Nowra, as documented in the Nowra Bridge Project Site Options Development Report, prepared in 2016, and available from the project website. A key constraint for eastern bypass options is the

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					<p>Shoalhaven River floodplain which would likely require a bridge about 1500 metres in length and an elevated viaduct or roadway for about three kilometres to provide the necessary level of flood immunity.</p> <p>As noted previously, the need for a bypass has been investigated, and it was identified that this would not address the local traffic demands in the area. Future consideration of a bypass would consider all feasible routes.</p>
37	Bypass	Without bridge	1	Concerned the bridge would be built by 2030 when the bypass is expected to be needed.	<p>The traffic modelling carried out for the project considered several future years up to 2046. The modelling identified that the project would significantly improve the performance of the road network and provide an improvement on the current situation.</p> <p>The Australian and NSW governments have committed joint funding for the project. RMS anticipates construction could start around 2020, subject to approvals.</p>
38	Bypass	With bridge	2	Bypass Meroo Meadow to south of Nowra and retain the existing southbound bridge for vehicles only.	<p>As noted, the need for a bypass has been extensively investigated and it was identified that this would not address the local traffic demands in the area, which is about 85 percent of traffic volume in Nowra-Bomaderry.</p> <p>Investigation into ongoing use of the existing southbound bridge to carry traffic has identified a number of constraints, particularly for heavy vehicles, and the increase in maintenance costs.</p> <p>Removal of traffic from this bridge and construction of a new bridge with additional capacity is considered to provide a better outcome.</p>

Ref	Issue category	Sub-issue	Total number of submissions	Issue(s) raised	Roads and Maritime response
39	Bypass	With bridge	1	Provide a bypass for heavy vehicles and leave the current bridges for local traffic.	<p>As noted, Roads and Maritime has identified significant constraints associated with retention of the existing southbound bridge for traffic, and also noting that having only two lanes, it also places significant constraints on movement of traffic through the local network.</p> <p>Traffic counts carried out in December 2017 identified that heavy vehicles comprised about up to 14 per cent of total traffic crossing the Shoalhaven River; removal of these vehicles would not contribute to a material reduction in congestion along this section of the highway.</p>
40	Bypass	With bridge	1	Confirmation requested the project will not preclude an eventual Nowra bypass to the west of the town	The provision of a new bridge is considered to be the best solution in improving the existing traffic congestion issues. Future consideration of a bypass would consider all feasible routes.
41	Bypass	With bridge	1	Would prefer to see the Nowra Bypass built first, but accepts that given the age of the existing southbound bridge, this project is a fait accompli.	With regard to a bypass, please refer to responses 35-41.
42	Bypass	With bridge	1	A second free flowing corridor across the river through Bomaderry / Nowra is needed, particularly when there is a traffic incident or bushfire emergency.	<p>Please refer to previous responses to the issue of a bypass.</p> <p>With regard to emergencies, the project would provide more capacity and improved efficiency on the local road network, which would contribute to improved response times.</p>
43	Bypass	With bridge	1	No bypass bridge or traffic corridor for Nowra has been considered at this	The Nowra Bridge Project Site Options Development Report, prepared in 2016, and available from the project

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				stage, which is hard to believe when other townships are being bypassed as part of the existing improvements.	<p>website, provides a comprehensive and detailed review of an extensive range of options considered, including bypasses to the east and west of Nowra.</p> <p>Discussion of a bypass is also provided in the Princes Highway Corridor Strategy, also prepared in 2016.</p> <p>The need for a bypass has been extensively examined and the traffic modelling has shown this would only take about 15 percent of current traffic out of the Nowra-Bomaderry network.</p> <p>Bypass projects in other locations are likely to make major improvements to safety and reduce traffic congestion. For example, the Berry bypass was projected to take about 75-80 percent of traffic out of Berry, and the Albion Park Rail bypass, is expected to take up to 70-75% of vehicles out of Albion Park Rail when built.</p>
44	Bypass	With bridge	1	Both a new bridge and bypass are needed.	The investigations carried out for the project identified that a new bridge, and upgrading of key intersections would be the best solution to address the local traffic demands in the area, which account for about 85% of traffic movements. The project would not preclude any future bypass.
45	Design	Grade separation - general	7	<p>An overpass/grade separation is needed to address traffic congestion and reduce delays.</p> <p>Need a bypass or flyover.</p>	<p>Please refer to previous responses to the issue of a bypass.</p> <p>Flyovers, or grade separation (of traffic), have been investigated as part of the options development and assessment process. It was identified that these would provide a small benefit to intersection performance but these would have much greater property and</p>

Ref	Issue category	Sub-issue	Total number of submissions	Issue(s) raised	Roads and Maritime response
					environmental impacts.
46	Design	Grade separation - general	1	Clarification on Option A & B and which is upstream and downstream.	<p>Clarification was provided by return email. It is noted that in the February 2018 Preferred Option Report, the options included:</p> <p>Option 1: a new four lane bridge with no intersection upgrades</p> <p>Option 2: a new three lane bridge with at grade intersection upgrades</p> <p>Option 3: a new four lane bridge with at grade intersection upgrades</p> <p>Option 4: a new four lane bridge with grade separation on the southern approach</p> <p>Option 5: Nowra Bomaderry Structure Plan which includes a new four lane bridge and mix of at grade and grade separated upgrades</p> <p>Option 6: a new four lane bridge with grade separation on both approaches.</p> <p>The options were presented to an Options Assessment Workshop where the participants recommended Option 3 as the preferred option.</p>
47	Design	Grade separation - general	9	Grade separated intersections at Illaroo, Bolong Road and Bridge Roads.	A grade separated intersection would provide a small benefit to intersection performance but this would be disproportionate to the additional cost and would also

Ref	Issue category	Sub-issue	Total number of submissions	Issue(s) raised	Roads and Maritime response
					have greater property and environmental impact.
48	Design	Grade separation - general	1	Council is opposed to the "at grade" option proposed as it is a short-term solution only and the State Government needs to bring forward the planning and building of a Nowra-Bomaderry Bypass in the medium term.	The Princes Highway Corridor Strategy prepared by Roads and Maritime in 2016 is informing the program of works and investigations over a 20-year period, including in the Nowra Bomaderry area. Roads and Maritime would continue to work with Council on its arterial road plans.
49	Design	Grade separation - general	1	Option report is very short on fact based argument on why a flyover does not stack up.	The Preferred Option report is intended as a short summary to provide context around the preferred option display, including basic intersection layouts and number of lanes. This allows for community and stakeholder feedback to be sought and considered before the more detailed and final design work takes place. Further information on this issue will be available in the environmental assessment.
50	Design	Grade separation - general	1	Overpasses like options 5 and 6 would reduce traffic jams.	Flyovers, or grade separation (of traffic), have been investigated as part of the options development and assessment process. It was identified that these would provide a small benefit to intersection performance but these would have much greater property and environmental impacts.
51	Design	Grade separation - general	3	Northbound and southbound approaches to the bridge should be grade separated, and the elimination of traffic lights at the intersections of Bolong, Illaroo and Bridge roads.	Please refer to the previous responses to grade separation at intersections. The North Nowra Link Road is a council project. The project would not preclude and does not require the

Ref	Issue category	Sub-issue	Total number of submissions	Issue(s) raised	Roads and Maritime response
				(unless North Nowra Link Road is constructed before bridge construction)	development of the North Nowra Link Road.
52	Design	Grade separation – Illaroo Road	8	An overpass and removal of traffic lights at Illaroo Road is needed, also addressing access problems at Fairway Drive.	Grade separation of the Illaroo Road intersection is not favoured in view of the much greater property and environmental impacts compared to the proposed at grade design. It is likely there would also be significant design challenges in bringing traffic from Illaroo Road down to the level of the highway. Roads and Maritime would investigate opportunities to address access issues at Fairway Drive as part of future design development for the project, and would consult with Council regarding its requirements.
53	Design	Grade separation – Illaroo Road	1	Raise the bridge to allow traffic to flow directly from Illaroo Road into the southbound line of traffic on the Princes Highway.	The alternative option is noted. However, this is not considered a feasible option given the constraints of the site. This would also require the level of the Bomaderry Creek bridge to be raised which would cause considerable design challenges and construction impacts.
54	Design	Grade separation – Bolong Road	1	Propose grade separation to take northbound traffic onto Bolong Road and an exclusive lane for traffic turning left from Bolong Road onto the Princes Highway.	Please refer to the previous responses to grade separation at intersections. An unsignalised left turn slip lane would introduce a dangerous weave for vehicles exiting Bolong Road and wanting to turn right at Illaroo Road or Bridge Road. The traffic lights would allow these vehicles to cross safely to the right hand lanes unimpeded.
55	Design	Grade separation –	6	Construction of a flyover or grade separated intersection from Illaroo Road	Please refer to the previous responses to grade

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		Bolong Road and Illaroo Road		to Bolong Road and also a second lane to the highway.	separation at intersections. Roads and Maritime has examined this option however there are considered to be too many significant constraints to make this a feasible option. These include requiring a much larger area to accommodate ramps and other works which would have greater impacts on property and require much more vegetation to be removed.
56	Design	Grade separation – Bridge Road	2	Grade separation at Bridge Road, removing the traffic lights at Bridge Road and Pleasant Way.	A flyover or grade separated intersection would provide a small benefit in intersection performance but this would have a greater property and environmental impact. While not specifically identified in the Princes Highway Corridor Strategy, the project would not preclude grade separation being constructed in the future.
57	Design	Grade separation – Bridge Road	2	Access to Pleasant Way should be altered to provide slipways/underpasses at the southern end of the bridge, eliminating another set of traffic lights.	The access between Pleasant Way and Princes Highway will be closed as part of the project. A new local road connecting Lyrebird Drive to the Princes Highway will be constructed about 300m south of Pleasant Way to offset the closure of Pleasant Way. The new local road connection would provide left in and left out access to the Princes Highway and will not require new traffic lights to be installed. Grade separated intersections have higher costs and greater property and environmental impacts. Grade separation using an underpass is not considered to be a feasible option at this location due to the associated flood risks.
58	Design	Grade	2	Construction of an underpass at Moss	The comment is noted. There is no proposal to change

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		separation – Other		Street, utilising existing roads and reduce the traffic delay times for accessing the highway. The use of slipways and/or underpasses will eliminate the need for traffic lights.	the existing intersection arrangements. A grade separated solution would likely have major property and environmental impacts.
59	Design	Other	1	With the total of six proposed lanes going south, there will have to be widening of the highway to incorporate another lane each way up to Moss Street.	The project would provide three lanes in both directions from north of Bolong Road to Moss Street. The project would tie in to the existing three lanes at Moss Street.
60	Design	Other	1	Traffic lights are required at the intersection of Illaroo Road and Fairway Drive.	Roads and Maritime would continue to consult further with Council on the Illaroo Road and Fairway Drive intersection. The intersection is currently outside the project area.
61	Design	Other	2	Support for project and proposed new four lane north bound bridge.	Positive response is noted.
62	Design	Other	1	How will Scenic Drive connect to Bridge Road?	Scenic Drive/Bridge Road would become left in/left out, and the right turn movement to and from this intersection would be removed. Alternative access to the southern foreshore would be available via Hyam Street/Mandalay Avenue/Shoalhaven Street.
63	Design	Other	2	The new bridge, plus extra lanes planned for the Bridge Road / Princes Highway intersection would effectively close Scenic Drive denying access to	There are no current plans to close Scenic Drive or Bridge Street. While the right turn into Scenic Drive from Bridge Road would be removed, alternative access to the locations identified would still be available via Hyam

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				the parking area of the Thai restaurant; the only feasible access to the Nowra Pool, Waterway Swim School, the Dish and Spoon Cafe and the Riverhaven Motel, as well as the alternative access for some riverside residences, the Shoalhaven District Hospital and the Nowra Showground. It may be possible to reconfigure the road system between the highway and Scenic Drive to avoid this from happening.	Street / Mandalay Avenue / Shoalhaven Street to access Scenic Drive.
64	Design	Other	1	Need to rebuild the highway crossing of Bomaderry Creek to accommodate the extra two lanes on the northern side of the river	The design provides for the widening of Bomaderry Creek bridge to accommodate the additional traffic lanes.
65	Design	Other	1	There should be a southbound dedicated bus lane along the highway between Nowra Anglican College and Moss St in the morning and afternoon peaks as both route and school bus services get badly disrupted by traffic jams	The project would provide for more efficient traffic movement along this section of the Princes Highway, including for buses. This would provide improved journey reliability for bus operators. Provision of a dedicated bus lane would greatly reduce efficient traffic flow for other vehicles, with any benefits being far outweighed by the impacts.
66	Design	Other	1	Propose the two lanes of traffic travelling south should have no restrictions or traffic lights. Traffic heading north on the new bridge should have dedicated two lanes with no traffic lights	The project would provide an additional lane in both directions from north of Bolong Road to Moss Street. This would increase capacity and improve traffic flow. While grade separation of intersections would remove the need for traffic lights, this is not considered a practicable option for the project considering the substantial additional cost for the minimal benefit. There

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					are greater property and environmental impacts associated with the construction of grade separated interchanges.
67	Design	Other	1	Create a roundabout on the Princes Highway (southbound) and provide a slip road onto the roundabout from Bolong Road.	A roundabout is not a feasible option given the constraints of the site. A roundabout would greatly reduce opportunities for traffic on Bolong Road to access the Princes Highway during peak periods when there would be substantial volumes of through traffic on the highway. Traffic lights are the best way to provide access to the Princes Highway from local arterial roads.
68	Design	Other	2	Close the intersection of Bolong Road and the highway. Redirect traffic from Bolong Road using Beinda Street.	Belinda Street is a local road and not suitable for heavy vehicles or an authorised B-double route.
69	Design	Other	1	Create a new roundabout at the intersection of the Princes Highway and Moss Street giving all traffic a fair share of movement.	Removing traffic lights and replacing these with roundabouts would only add further traffic delays and queueing, as roundabouts provide less control to manage traffic movements at intersections. Without traffic lights in place it would become very difficult for vehicles to access the Princes Highway during peak periods from side streets.
70	Design	Other	1	Access available from Illaroo Road to Bolong Road.	The design has allowed for traffic movements between Bolong Road and Illaroo Road.
71	Design	Property access	1	Concerns over property access north of Illaroo Road near Bomaderry Creek.	Roads and Maritime is aware of this issue and the access issues for this property, and is currently

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					developing an alternative access option. The relevant parties would be consulted.
72	Design	Intersection – Illaroo Road	1	Increase the turning lane for south bound traffic turning into Illaroo Road	The project provides additional space for turning vehicles through adding an additional right turn lane into Illaroo Road.
73	Design	Intersection – Illaroo Road	1	What is the length of the third lane on Illaroo Road? Will it provide sufficient capacity for traffic turning north?	<p>The upgraded intersection of the Princes Highway and Illaroo Road will include:</p> <ul style="list-style-type: none"> • Two southbound right turn lanes from the Princes Highway into Illaroo Road • Three dedicated right turn lanes and one dedicated left turn lane from Illaroo Road to Princes Highway • An acceleration and merge lane for northbound traffic turning into Illaroo Road from Princes Highway <p>The additional lanes proposed in the preferred option would increase the capacity of the intersection and number of vehicles that can enter the Princes Highway on each green phase.</p>
74	Design	Intersection – Illaroo Road	13	A dedicated left turn lane for Illaroo Road onto the Highway. Acquisition of property may be required.	Please refer to previous response.
75	Design	Intersection – Illaroo Road	1	Provision of left turn lane only lane to Princes Highway from Illaroo Road, ban right turn movements, and then there would be safer through flow on the	<p>The upgraded intersection of the Princes Highway and Illaroo Road will include:</p> <ul style="list-style-type: none"> • Two southbound right turn lanes from the

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				highway with virtually no stopping at intersections.	<p>Princes Highway into Illaroo Road</p> <ul style="list-style-type: none"> • Three dedicated right turn lanes and one dedicated left turn lane from Illaroo Road to Princes Highway • An acceleration and merge lane for northbound traffic turning into Illaroo Road from Princes Highway <p>It is not practicable to remove right turn movements from Illaroo Road onto the highway as there no other alternative access options.</p> <p>Banning the right turn movement from the Princes Highway into Illaroo Road is not possible as there is currently no viable alternative access to North Nowra from the north.</p>
76	Design	Intersection – Illaroo Road	2	Northbound traffic turning onto Illaroo Road will be required to give way to southbound traffic turning from the Princes Highway to Illaroo Road, which has the potential to back up traffic into Illaroo Road and could bank up across the bridge.	A short merging lane westbound on Illaroo Road will be provided to allow for safe merging with southbound traffic turning right from the highway.
77	Design	Intersection – Illaroo Road	4	<p>Separate lane on Illaroo Road for traffic turning left onto Princes Highway.</p> <p>How is the extra lane between Illaroo Road and Bolong Road being accommodated?</p>	<p>The upgraded intersection of the Princes Highway and Illaroo Road will include:</p> <ul style="list-style-type: none"> • Two southbound right turn lanes from the Princes Highway into Illaroo Road • Three dedicated right turn lanes and one dedicated left turn lane from Illaroo Road to

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					<p>Princes Highway</p> <ul style="list-style-type: none"> An acceleration and merge lane for northbound traffic turning into Illaroo Road from Princes Highway <p>The Bomaderry Creek bridge would be widened to accommodate the extra lanes.</p>
78	Design	Intersection – Illaroo Road	1	Illaroo Road needs three lanes to Golf club turn	Illaroo Road currently has two westbound lanes at the intersection of Fairway Drive. There is not a sufficient volume of traffic wanting to turn into Fairway Drive for provision of a third lane.
79	Design	Intersection – Illaroo Road	1	The proposal does not include a dedicated northbound lane for traffic entering Illaroo Road. There should at least be a slip lane into Illaroo Road.	<p>The project does provide a dedicated left turn lane from Bridge Road to Illaroo Road.</p> <p>A short merging lane westbound on Illaroo Road to allow for safe merging with southbound traffic turning right from the highway is being provided.</p>
80	Design	Intersection – Bolong Road	1	A longer right-turn from Bolong Road onto the highway is needed, particularly for tankers.	This has been taken into consideration in the design of the intersection upgrade. The design accommodates up to a 26 metre B-double vehicle. This is longer than a tanker.
81	Design	Intersection – Bolong Road	1	Provide an extra right-hand lane from Bolong Road to Princes Highway.	<p>Traffic modelling carried out for the project identified that two left turn lanes, in conjunction with the widening of the Princes Highway to three lanes in each direction, would be sufficient to accommodate future traffic movements at the Bolong Road/Princes Highway intersection.</p> <p>Currently, less than 10 vehicles per hour turn right from Bolong Road onto the Princes Highway. Given this level</p>

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					<p>of usage, an additional right turn lane is not warranted.</p> <p>The design of the intersection upgrade would allow up to a 26 metre B-double vehicle to stop in the right turn lane without impeding the two left turn lanes on Bolong Road.</p>
82	Design	Intersection – Bolong Road	1	On Bolong Road provide a slip lane left on the Princes Highway and lights for a right turn.	<p>Traffic lights for vehicles turning left from Bolong Road to the Princes Highway need to be retained to ensure efficient movement of traffic for southbound vehicles on the Princes Highway.</p> <p>Provision of a slip lane would impact on the adjacent property and could introduce a merge safety issue for traffic travelling from Bolong Road to the right turn lanes at Illaroo Road or Bridge Road.</p>
83	Design	Intersection – Bolong Road	1	Separate slip merge lane on Princes Highway for right turn traffic from Bolong Road.	<p>Northbound traffic on the Princes Highway would be stopped at Bolong Road when the right turn movement from Bolong Road to the Princes Highway is being made. This would allow heavy vehicles making this movement to safely move to the left lane to accelerate up the hill at this location, and would therefore not require a merge lane.</p> <p>The traffic lights at the Illaroo Road and Bolong Road intersections would be phased together so that the right turn from Bolong Road to the highway would run at the same time as the right turn from Illaroo Road to the highway. Opposing movements would have the highway traffic stopped on the bridge.</p> <p>The need for appropriate signage to direct slower heavy vehicles to use the left-most lane to minimise delays for other road users would be investigated during future stages of design.</p>

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84	Design	Intersection – Bolong Road	3	The Bolong Road intersection needs to be addressed. The right turn movements are impeding southbound highway traffic and the Princes Highway northbound traffic having to stop for Bolong Road turning.	<p>Currently, vehicles turning right onto the Princes Highway from Bolong Road do not impede northbound vehicles travelling on the highway. The right turn from Bolong Road is required to allow large vehicles (particularly B-doubles) to access the Princes Highway.</p> <p>It is expected right turn movements from Bolong Road would be synchronised with right turn movements into and out of Illaroo Road while southbound traffic on the Princes Highway is stopped.</p> <p>The design of the intersection upgrade would allow up to a 26 metre B-double vehicle to stop in the right turn lane without impeding traffic in the two left turn lanes on Bolong Road.</p>
85	Design	Intersection – Bolong Road	1	Relocation of the intersection of Bolong Road and Princes Highway southwards on land with a lower gradient to address the steep hill and dangerous intersection.	It is not practical to relocate this intersection, particularly given its proximity to the Bomaderry Creek bridge. Any relocation would likely require property acquisition and potentially affect Bomaderry Creek.
86	Design	Intersection – Bolong Road	1	The traffic lights at the Princes Highway and Bolong Road need to stay.	Noted. The design does not propose to remove these lights.
87	Design	Intersection – Pleasant Way		Pleasant Way access to remain in current position to remove need for separate intersection.	<p>Pleasant Way currently provides access for southbound and northbound vehicles on the Princes Highway, and left out only for vehicles travelling south.</p> <p>Allowing these movements greatly reduces the efficiency of the intersection, in particular for southbound traffic on the Princes Highway.</p> <p>A new local road connecting Lyrebird Drive to the</p>

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					<p>Princes Highway will be constructed about 300m south of Pleasant Way to offset the closure of Pleasant Way. The new local road connection would provide left in and left out access to the Princes Highway and will not require new traffic lights to be installed.</p> <p>Northbound traffic wishing to access the urban area and riverfront east of the highway would so via Moss Street, Ferry Lane and Riverside Drive.</p>
88	Design	Intersection – Pleasant Way	3	On the new Pleasant Way intersection, provide a continuous dedicated left turn lane out of Pleasant Way onto the Princes Highway southbound.	The new local road connection would provide left in and left out access to the Princes Highway and will not require a new set of traffic lights. Traffic turning left from the new local road will have uncontrolled access to the Princes Highway.
89	Design	Intersection – Pleasant Way	1	<p>On the existing Pleasant Way, provide a southbound slip lane or dedicated left turn lane.</p> <p>Northbound access to the highway is insufficient to warrant either a set of lights or access to the northbound lane of the highway.</p>	<p>It is proposed that Pleasant Way would be closed to traffic, with alternative access to the area to the east of the highway provided via a new local road connection.</p> <p>Northbound access to the Princes Highway from the urban and riverfront east of the highway would be maintained via Riverside Drive, Ferry Lane and Moss Street.</p>
90	Design	Intersection – Pleasant Way	1	The right turn across the southbound lanes for northbound traffic to access Pleasant Way should be abolished, and access provided via the Moss Street intersection.	Northbound traffic wishing to access the residential area and riverfront east of the highway would so via Moss Street, Ferry Lane and Riverside Drive.
91	Design	Intersection –	4	Direct traffic on Pleasant Way to use Moss Street to remove need for	The new local road connection would provide left in and left out access to the Princes Highway and will not

Ref	Issue category	Sub-issue	Total number of submissions	Issue(s) raised	Roads and Maritime response
		Pleasant Way		<p>additional intersection and traffic lights on the Princes Highway.</p> <p>Consider no traffic lights and slip on and off roads at the Pleasant Way intersection.</p>	<p>require a new set of traffic lights</p> <p>Motorists travelling north wanting to access the urban area and riverfront east of the highway would so via Moss Street, Ferry Lane and Riverside Drive.</p>
92	Design	Intersection – Pleasant Way	3	<p>The design should also incorporate access to the river foreshore, providing a slip road both in to the left and out of Pleasant Way (as per current arrangements). This area is important for tourism in the area.</p>	<p>The new local road connection would provide left in and left out access to the Princes Highway and provide access to the urban area and riverfront east of the highway.</p> <p>Northbound traffic wishing to access the urban area and riverfront east of the highway would so via Moss Street, Ferry Lane and Riverside Drive.</p> <p>During development of the project, it has been recognised that there would likely also be opportunities to improve and/or enhance pedestrian access to the river foreshore, and further details would be provided in the forthcoming environmental assessment.</p> <p>Roads and Maritime would continue to work with Council and other stakeholders to support local access and to integrate the Nowra Bridge Project with the River Foreshore Master Plan.</p>
93	Design	Intersection – Pleasant Way	1	<p>Remove traffic lights at the new Pleasant Way. A pedestrian crossing could be located a short distance further south to reduce impact on the bridge flow and coordinated with the Bridge Road intersection to maximise efficiency.</p>	<p>The new local road connection would provide left in and left out access to the Princes Highway and provide access to the urban area and riverfront east of the highway.</p> <p>Controlled east-west pedestrian crossings would be provided at Moss Street, Bridge Road, Illaroo Road and Bolong Road. Pedestrian underpasses would also be</p>

Ref	Issue category	Sub-issue	Total number of submissions	Issue(s) raised	Roads and Maritime response
					available at the northern and southern foreshore.
94	Design	Intersection – Pleasant Way	1	Closure of Pleasant Way would not be popular and will impact on the proposed city gardens.	Alternative and more direct access would be available through the new local road connection, and would also reduce the volume of traffic passing residences.
95	Design	Intersection – Pleasant Way	1	Eliminate Pleasant Way exits and improve road condition for residents.	The project provides for the closure of the intersection of Pleasant Way and the Princes Highway, and construction of a new local road connection.
96	Design	Intersection – General	2	There should not be traffic lights at Illaroo Road/Princes Highway and Illaroo Road and Fairway Drive.	Traffic lights are required at the Illaroo Road and Princes Highway intersection to ensure safe turning movements for traffic to and from North Nowra. It is not proposed to provide traffic lights at the Fairway Drive intersection.
97	Design	Intersection – General	1	Has consideration been given to merging the Bolong Road and Illaroo Road intersection into one intersection? The two sets of traffic lights close together is a strong cause of traffic congestion.	This was investigated as part of the options assessment and discounted as a feasible option given the constraints of the site. It was also found to perform poorly, reaching capacity in about 10-15 years. Further detail on the range of options considered in the development of the preferred option is provided in the environmental assessment. Chapter 2.4 of the Review of Environmental Factors (REF) provides further information on the options considered.
98	Design	Intersection – General	1	Make the two western lanes dedicated to Illaroo Road traffic (both directions). Remaining two lanes cater for	The traffic modelling carried out for the project identified that one left turn lane for northbound traffic would provide sufficient capacity for this turning movement.

Ref	Issue category	Sub-issue	Total number of submissions	Issue(s) raised	Roads and Maritime response
				northwards flowing traffic.	
99	Design	Intersection – General	1	Eliminate traffic lights at each ends of the bridge.	<p>Traffic lights are required at the Illaroo Road and Bridge Road intersections (and others) to manage the competing demands of local traffic movements (those from side streets) as well as through movements along the Princes Highway.</p> <p>Without traffic lights in place it would become very difficult for vehicles to access the Princes Highway during peak periods from side streets such as Bridge Road and Bolong Road.</p> <p>Please refer to response provided for Issue 6 with regard to flyovers / grade separation.</p>
100	Design	Intersection – Bridge Road	3	Bridge Road needs left turn access, such as a slip lane, onto the southern approach to the bridge.	The project provides left turn access to the Princes Highway from Bridge Road.
101	Design	Intersection – Bridge Road	1	Eliminate the right turn into Bridge Road completely.	<p>Removal of the right-hand turn would require southbound traffic to travel further south (about 680 metres) to access the main CBD area and key infrastructure such as the Shoalhaven District Memorial Hospital via the Moss Street intersection. This would increase travel times for vehicles travelling to the Nowra CBD, which could be a particular issue for emergency vehicles travelling to the hospital.</p> <p>It would also increase traffic volumes at the Moss Street intersection. At a minimum, the Moss Street intersection would need to be upgraded to provide additional capacity.</p> <p>Opportunities to improve the efficiency of this</p>

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					intersection would be considered as part of the broader suite of investigations and works carried out under the Princes Highway Corridor Strategy.
102	Design	Intersection – Bridge Road	1	Keep proposed design but change intersections and have only exit/entry ramps accessing the highway from Bridge Road and Pleasant Way, with the main flow going through the Moss Street intersection.	Removal of access to Bridge Road from the Princes Highway would require southbound traffic to travel further south to Moss Street to access the main CBD. It would also increase traffic volumes at the Moss Street intersection. At a minimum, the Moss Street intersection would need to be upgraded to provide additional capacity.
103	Design	Alternative option	1	Replace the existing southbound bridge with a new 4 lane bridge.	This option would have a significant impact on traffic with the loss of two of the existing five traffic lanes across the river during demolition and construction. Through previous consultation for the project, the community has expressed a strong desire to retain the existing southbound bridge. In view of this, Roads and Maritime has committed to the ongoing maintenance of the bridge, and to carry out further consultation into options for adaptive reuse which will be progressed separately to the project.
104	Design	Alternative option	2	Relocation of bridge to the east of current highway. Maintain two lanes of traffic on the Princes Highway in both directions, providing two turn lanes into Bridge Road extended on a new four lane bridge. Current northbound configuration remains.	This option was examined but was not taken further due to significant traffic and safety reasons. Further details on the options selection process are available in Chapter 2.4 of the Review of Environmental Factors (REF). Traffic modelling carried out for the project identified three lanes in each direction were required to provide improved traffic efficiency along this section of the

Ref	Issue category	Sub-issue	Total number of submissions	Issue(s) raised	Roads and Maritime response
					highway.
105	Design	Bridge	2	<p>Construction of a two-level bridge:</p> <ul style="list-style-type: none"> • North approach commencing north of Bolong Road • Remove traffic lights at Illaroo Road with top level of the bridge feeding Illaroo Road. 	This is not considered a feasible option given the constraints of the area, particularly impacts on property. Chapter 2.4 of the Review of Environmental Factors (REF) provides further information on the options considered.
106	Design	Bridge	3	Consideration for the bridge to be constructed using steel.	A whole-of-life approach is being considered for the design of the new bridge and this includes considering its operation and maintenance costs, as well as construction costs. The final selection of construction materials will represent a balanced consideration of all relevant matters.
107	Design	Bridge	1	Build new bridge on the present alignment of the original metal bridge.	Please refer to the response provided for Issue 103.
108	Design	Bridge	1	New bridge is being built too close to existing bridge.	The bridge would be built at a safe distance from the current northbound bridge with sufficient separation to prevent people from jumping between the bridges. In any case, the likelihood of this is considered low as the pedestrian path on the existing northbound bridge will be removed.
109	Design	Bridge	3	Requested additional information on the bridge options.	<p>Email to all stakeholders was sent on 20 February 2018 and included links to the project webpage where more information is available.</p> <p>Chapter 2.4 of the Review of Environmental Factors</p>

Ref	Issue category	Sub-issue	Total number of submissions	Issue(s) raised	Roads and Maritime response
					(REF) provides further information on the options considered.
110	Design	Pedestrian and cyclists	1	Construct a footpath on the western side of the new northbound bridge for pedestrian access and a view to the west.	The proposal includes a shared path on the western side of the new northbound bridge.
111	Design	Pedestrian and cyclists	1	The proposal should be moved away from the existing bridge to allow pedestrian access to be provided on either side and making construction easier.	Allowance for construction and pedestrian access has been made in the design. Pedestrian access will be provided on both the new bridge and the old southbound bridge.
112	Design	Pedestrian and cyclists	1	The present path on the western side of the existing concrete bridge is inadequate in width for this amount of shared traffic. Provision for separated cycle path and pedestrian path should be made for the western side of the new bridge. There is no mention in the plans for this.	The project includes a new shared path on the new northbound bridge, which would be wider than the path on the existing northbound bridge.
113	Design	Pedestrian and cyclists	3	In favour of pedestrian and cycling activities for old bridge. Retention, extension and improvement of the current pedestrian and cycle underpass at either end and connections.	The project improves east-west connections across the Princes Highway for pedestrians and cyclists by providing an additional controlled crossing at the Bolong Road intersection and by upgrading the shared path underpasses at the northern and southern foreshore. The existing southbound bridge would be retained for adaptive reuse such as a shared pedestrian and cyclist path.

Ref	Issue category	Sub-issue	Total number of submissions	Issue(s) raised	Roads and Maritime response
114	Design	Pedestrian and cyclists	1	The changes at the intersection of Bridge Road and Scenic Drive will present an opportunity to incorporate a pedestrian refuge on Bridge Road, immediately south of Scenic Drive without impacting 2 lanes of southbound traffic.	Roads and Maritime would consult further with Council on this issue. A controlled pedestrian crossing is provided at the Bridge Road and Princes Highway intersection.
115	Design	Pedestrian and cyclists	1	Proposed works around Bridge Street provides the potential for a pedestrian bridge over the Princes Highway.	The current pedestrian underpass will be maintained, with pedestrian facilities provided at the intersections.
116	Design	Pedestrian and cyclists	1	There is a need for good cycle and walking paths.	There will be improved pedestrian and cycle access at all intersections and along the length of the 1.6 kilometre highway upgrade. There will also be improvements to landscaping, pedestrian access and community spaces on the foreshore.
117	Design	Property access	1	Access concerns for the house on the western side next to the south bank of Bomaderry Creek.	Roads and Maritime is investigating a safer alternative access in consultation with the landowner.
118	Old Bridge	Retain	7	Supportive of old bridge being retained for adaptive reuse such as light use, pedestrian or cycle path. Retention of the existing southbound bridge will be popular with the community.	Positive response is noted.

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119	Old Bridge	Retain	1	In favour of retaining old bridge for community use but suggest it is painted a bright colour other than grey	Support noted. Paint colour would be taken into consideration in the future adaptive reuse investigation and environmental assessment for the bridge, and noting that this would be considered when deciding to maintain the heritage values of the bridge.
120	Old Bridge	Retain	3	Ownership and ongoing maintenance of the existing southbound bridge to be retained by Roads and Maritime / State government. The use and integration of the bridge will need further investigation.	Roads and Maritime has committed to providing funding for the ongoing repair and maintenance of the existing southbound bridge. Roads and Maritime would carry out a separate investigation into opportunities for adaptive reuse of the bridge.
121	Old Bridge	Retain	1	Repurposing of the existing southbound bridge, installation of noise barriers on the existing concrete bridge to minimise noise transfer and act a wind break to activities on the existing southbound bridge.	The future use of the existing southbound bridge would be subject to a further separate assessment, and this would include consideration of all relevant matters including the need for any particular noise management measures. Roads and Maritime will continue to consult with key stakeholders.
122	Old Bridge	Other	1	The existing southbound bridge is an environmental hazard for the river and will be an ongoing drain on taxpayers' money.	Roads and Maritime has consulted with the community who have indicated a strong preference for the existing southbound bridge to be retained. The bridge will continue to be maintained in a safe condition both for the environment and for the general public.
123	Old Bridge	Other	1	The National Trust of Australia (NTISA) requests they are involved in any future community engagement or consultation,	Roads and Maritime would continue to work with the community and key stakeholders in the development of this project.

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				particularly in regard to the future uses and conservation of the old southbound Nowra bridge	
124	Old Bridge	Other	1	Blasting, painting and general maintenance in the future of the old bridge will be a major problem if the bridges are so close.	Current maintenance activities on the existing southbound bridge are carried out without affecting operation of the adjacent northbound bridge. It is not anticipated this would be changed by the project.
125	Old Bridge	Other	1	Report is not clear on the long-term maintenance of the existing southbound bridge. It is important such a commitment be made and publicly announced.	Roads and Maritime has committed to providing funding for the ongoing maintenance of the existing southbound bridge. Roads and Maritime will carry out a separate investigation into opportunities for adaptive reuse of the bridge. This would include consideration of all relevant matters including ongoing maintenance of the bridge.
126	Old Bridge	Remove	2	Demolish the existing southbound bridge once the new bridge is built. There is an ongoing cost for the state and provide a location for anti-social behaviours and location to jump off the bridge into the river.	Roads and Maritime has consulted with the community who have indicated a strong preference for the existing southbound bridge to be retained. Roads and Maritime has made a commitment to the public about the existing southbound bridge not being demolished. A separate plan for maintenance of the existing southbound bridge, including potential safety upgrades will be developed by Roads and Maritime in consultation with the community, Council and heritage groups.
127	Urban design	Bridge	1	Make the new bridge an icon for NSW like the Opera House.	The project has been developed in accordance with best practice urban design principles meeting the functional and spatial requirements of the site. Roads and Maritime

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					would continue to work with the urban designers, Council and the community to develop the project in subsequent phases.
128	Urban design	Bridge	2	Happy with the proposal to repurpose the bridge for pedestrian and cyclists.	Positive response is noted.
129	Urban design	Foreshore	1	Proposal does not incorporate any planning for future development of the river foreshore, does not address accessibility and beautification of the river.	An assessment of the landscape character and visual impact of the Nowra Bridge Project was undertaken. This included consideration of access to the river foreshore areas on both sides of the river, Roads and Maritime is consulting with Council with regard to landscape treatments and opportunities to align with Council's development plans for the area.
130	Urban design	Foreshore	1	Proposal doesn't address the dangerous access to the Bridge Fish Shop, or the visual impact of the "run down" caravan park.	The design for access to the fish and chip shop property would meet all relevant safety and design requirements. The visual appearance of the caravan park is a local council matter.
131	Urban design	Foreshore	1	Closing Pleasant Way will impact tourist development potential east of Princes Highway.	The project provides for alternative access to this area.
132	Urban design	Foreshore	1	Council requests the RMS urban design consultants work closely with Council, and Council's urban design consultants to ensure integration between the Nowra bridge project and the river foreshore masterplan.	Roads and Maritime is committed to working closely with Council to achieve the best possible outcome balancing the needs of the varied stakeholders involved with the project.

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133	Urban design	Foreshore	2	Better access to riverfront is needed. This could be done by acquiring the caravan park to the east of the bridge and construction new slipway and underpass to access the river, improve the amenity of the area, and attract drivers to stay in the area rather than drive straight through.	The proposal does include provision of an access to the river foreshore at this location and does not require acquisition of any land associated with the caravan park. Roads and Maritime would continue to consult with Council to maximise the value of the shared paths with regard to access to the riverfront.
134	Urban design	Other	1	The National Trust Illawarra Shoalhaven Branch recommends interpretative signage be developed for the bridge and its surrounds and that the Conservation Management Plan be updated to reflect the proposed new uses of the bridge.	This would be considered as part of the separate investigation into adaptive reuse options for the existing southbound bridge.
135	Urban design	Bridge		The new bridge and intersections need to be comparable to the new work to the north of the Shoalhaven River and meet the future social and economic requirements of the Nowra area.	An urban design strategy is being developed for the Nowra Bridge Project, which would be aligned with the urban design principles of the other Princes Highway upgrade projects.
136	Urban design	Environment	1	Concern there will be an increase in noise affecting residents in North Nowra. Is there any provision for abatement (plastic or clear barrier) on the western side of the new bridge?	A noise and vibration impact assessment has been completed as part of the environmental assessment to determine the potential noise impacts during construction and when the project is completed. Roads and Maritime will work with impacted properties eligible for noise mitigation treatments during the projects ongoing development Chapter 6.2 of the Review of Environmental Factors

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					(REF) provides further information on the noise and vibration assessment.
137	Urban design	Bridge	1	Any construction needs to be aesthetically pleasant and congruous to the area, with green walls and other recent developments in building.	An urban design strategy is being developed for the Nowra Bridge Project, which will be aligned with the urban design principles of the other Princes Highway upgrade projects.
138	Other	Rail	9	Extension of railway service to Nowra and beyond and future rail crossing to be considered.	At this stage, TfNSW has no plans to extend the South Coast Line to the south.
139	Other	Rail	1	The Preferred Option fails to incorporate rail as the alternative mode of transport to roads clogged with individual vehicles carrying freight and passengers.	TfNSW has no plans to extend the South Coast Line to the south. Separate to the project, the NSW Government has announced an upgrade of the line between Berry and Bomaderry for use by heavier trains which is expected to shift a portion of the existing road-based freight to rail. The project would support this through improved efficiency for northbound freight vehicles through the local road network.
140	Other	Rail	1	If the railway cannot be extended in the foreseeable future, then bus services should be upgraded, including a regular, direct service from Bomaderry Station through Nowra to South Nowra industrial area.	This is regarded as a separate issue to the project, noting that improved efficiency of traffic movement through the Nowra Bomaderry network would also be of benefit to public transport.
141	Other	Rail	1	A heavy gauge railway added to any	TfNSW has no plans to extend the South Coast Line to the south, the design does not provide for any future

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				new bridge design is essential.	addition of a railway.
142	Other	Consultation	1	No input from affected residents to identify preferred option.	<p>The preferred option display is part of the consultation strategy to ensure the ideas and opinions of the community and stakeholders are heard and where feasible, incorporated into the development of the final project. The REF for the proposal is now on display and is another opportunity for the public to provide feedback on the project.</p> <p>Roads and Maritime engaged with directly affected residents during the preferred option display, and further consultation has taken place after the display. Roads and Maritime will continue to engage with the wider community through stages of the project.</p> <p>Consultation has also previously been carried out in 2014 as part of the site options development process.</p>
143	Other	Consultation	1	Concerned opinions/options and the 'consultation sessions' were not being written down and the decision has already been made and the sessions are merely cosmetic.	The community information kiosks were intended to provide an opportunity for members of the community to ask questions and obtain more information about the project. All attendees were encouraged to make formal submissions, and there were a number of different ways for submissions to be provided.
144	Other	Consultation	1	Questions about project design, which were answered by project team by sending Community Update information.	N/A
145	Other	Consultation	1	Consider a community opening.	Roads and Maritime would take this feedback into

Ref	Issue category	Sub-issue	Total number of submissions	Issue(s) raised	Roads and Maritime response
					consideration for the future.
146	Other	Consultation	1	Provide a full map to provide realistic view of the proposal.	<p>The purpose of the Preferred Option report is to provide overview of the preferred option, including basic intersection layouts and numbers of lanes. This allows for community and stakeholder feedback to be sought and considered before the more detailed and final design work takes place.</p> <p>Further detail will be available when the environmental assessment is placed on public display later in 2018.</p>
147	Other	Consultation	1	Concern for miscommunication to the community of the limitations to the benefits of the preferred solution, project will not eliminate the bottleneck.	Extensive investigations have been carried out for the project, including substantial assessment and evaluation of numerous options. Further details will be provided in the environmental assessment that will be placed on public display later this year.
148	Other	Consultation	1	We need more detail ie continued travel through south.	The project addresses upgrade needs on the Princes Highway between Bolong Road and Moss Street. Roads and Maritime would continue to monitor the need to upgrade other sections of the highway.
149	Other	Property impact	1	What are the property impacts of the proposed work at Illaroo Road? These were not detailed in the preferred option documentation.	<p>The project would have property impacts outside the existing road reservation, and Roads and Maritime has commenced consultation with affected property owners and occupants.</p> <p>The purpose of the Preferred Option report is to provide overview of the preferred option, including basic intersection layouts and numbers of lanes. This allows for community and stakeholder feedback to be sought and considered before the more detailed and final design</p>

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					work takes place. Further details are available in the environmental assessment.
150	Other	Alternative option	1	Another road needed from Shoalhaven Heads joining all the coastal suburbs to Sussex Inlet.	This is a separate issue and outside the scope of this project. Roads and Maritime would continue to work with Council to develop and support road upgrade and expansion projects in the region.
151	Other	Alternative option	2	Move proposed bridge to east of existing bridge can offer more alternatives in the future (eg. feeder road to Greenwell Point Road) and save valuable riverside parkland.	Bypass options to the east of Nowra are documented in the Nowra Bridge Project Site Options Development Report, prepared in 2016. A key constraint for eastern bypass options is the Shoalhaven River floodplain which would likely require a bridge about 1500 metres in length and an elevated viaduct or roadway for about three kilometres to provide the necessary level of flood immunity.
152	Other	Alternative option	1	An alternative to the big overpass loop on the southern side of the river (Option 6) would be an underpass at Moss Street.	This is not a feasible option given the constraints of the site. Further detail on the range of options considered in the development of the preferred option is included in the environmental assessment.
153	Other	Alternative option	1	Proposes a new bridge to the east of the existing southbound bridge, which should be demolished. New bridge should have no more than three lanes and a pathway for pedestrians and cyclists.	Through previous consultation for the project, the community has expressed a strong desire to retain the existing southbound bridge. In view of this, Roads and Maritime has committed to the ongoing maintenance of the bridge, and to carry out further consultation into options for adaptive reuse which will be progressed

Ref	Issue category	Sub-issue	Total number of submissions	Issue(s) raised	Roads and Maritime response
					<p>separately to the project.</p> <p>Traffic modelling carried out for the project identified three through lanes and the left turn lane to Illaroo Road were required for the efficient movement of traffic through this section of the highway. The new bridge would include a shared pathway for pedestrians and cyclists.</p>
154	Other	Alternative option	1	Go around the back of Stockland to old Southern Road to Warra Warra roundabout.	Council is progressing other road upgrade projects in the area. The Nowra Bridge Project is intended to provide a new bridge across the Shoalhaven River to replace the existing southbound bridge.
155	Other	Council	2	Propose new eastern arterial road parallel to existing Princes Hwy from South Nowra to Nowra. This will allow traffic to access growth areas for housing at Worrigeer, east of Nowra.	<p>The East Nowra Sub Arterial (ENSA) Greenwell Point Road Bypass is a council project and Roads and Maritime has been consulting with Council about potential interactions between the two projects.</p> <p>Construction of the Nowra Bridge project would not preclude the development of the ENSA.</p>
156	Other	Council	1	North Nowra Link Road is needed with a major intersection to North Nowra, including for heavy vehicles.	The North Nowra Link Road is a council project and Roads and Maritime has been consulting with council about the potential interactions between the two projects. Construction of the Nowra Bridge project does not preclude the development of the North Nowra Link Road.
157	Other	Council	7	Greenwell Point Rd bypass should be constructed at the same time (East Nowra Sub Arterial - ENSA).	The East Nowra Sub Arterial (ENSA) is a council project and Roads and Maritime has been consulting with Council about the potential interactions between the two projects.

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					The Nowra Bridge project does not require the construction of the ENSA.
158	Other	Council	2	The proposal should incorporate the longer term planning of the region and it should include the East Nowra Sub Arterial.	<p>The traffic modelling has only considered committed projects rather than projects currently in planning by Council.</p> <p>The traffic growth rates adopted consider long term planning for the region based on strategic land use transport modelling which has included consideration of the East Nowra Sub Arterial.</p>
159	Other	Council	2	Roads and Maritime to help fund council to build a four-lane north Nowra / Bomaderry Link road with grade separation in the Berry to Bomaderry project to provide main access to North Nowra. This would halve the current volume of traffic on Illaroo Road.	Roads and Maritime would continue to consult with Council regarding local road improvements and connections to the Princes Highway. It is noted that the North Nowra Link Road is a council project and Council would be responsible for securing the project's funding.
160	Other	Council	1	Extension of Moreton Street to the rear of Stocklands to an extension of Junction Street.	This is a matter for Council. Roads and Maritime will pass on this comment to Council for its consideration.
161	Other	Council	1	The combined result of the above two proposals may require some upgrading of Beinda Street and possibly a roundabout at the Beinda/Bolong intersection.	Noted. This would be considered should the proposals proceed.
162	Other	Council	1	If the North Nowra bypass is constructed, the demand on the	The traffic modelling has only considered committed projects rather than projects currently in planning by

Ref	Issue category	Sub-issue	Total number of submissions	Issue(s) raised	Roads and Maritime response
				highway and the need for traffic lights would be decreased.	Council. The traffic growth rates adopted consider long term planning for the region based on strategic land use transport modelling.
163	Other	Council	1	Council requests that Roads and Maritime provides more detail and increase its level of communication with Council so detailed feedback can be provided in a timely manner and more certainty can be provided to allow Council to finalise its other plans.	Roads and Maritime is committed to working closely with Council to achieve the best possible outcome balancing the needs of the varied stakeholders involved with the project.
164	Other	Princes Highway	1	Grade separation at Kalandar Street needed to address cascading traffic congestion issues back to the bridge.	This intersection is outside the scope of this project. Roads and Maritime and Transport for NSW are progressing a traffic study identified in the Princes Highway Corridor Strategy to address localised traffic issues particularly around peak times. This would include consultation with Council with regard to any local road network projects or proposals, and/or where any works would tie in to the local road network.
165	Other	Princes Highway	1	Grade separation at Plunkett Street and appropriate left only/right only turns at other intersections.	Please refer to response provided to Issue 164.
166	Other	Princes Highway	1	Issues with Cambewarra Road and Princes Highway roundabout due to Gerringong to Bomaderry upgrades.	Please refer to response provided to Issue 164.

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167	Other	Other	1	The preferred option will relieve the traffic problem in crossing the Shoalhaven River at Nowra.	Traffic modelling carried out for the project identifies there would be an improvement in the efficiency of traffic flow along this section of the Princes Highway.
168	Other	Other	2	Clarification needed about the Aboriginal Land Claim on Crown Land near Bridge Rd identified in 2014 Bridge Locations Options Report.	As part of the environmental assessment, the Office of the Registrar for Aboriginal land claims in NSW has been consulted, and confirmation received that Aboriginal land claims exist within the project study area.
169	Other	Other	1	Don't agree the preferred option is the most balanced proposal.	The preferred option has been identified through a comprehensive options identification and evaluation process. Further details are provided in the environmental assessment.
170	Other	Other	1	Town is struggling economically and needs outside investment from tourists if it is to survive, so this should be taken into consideration in developing the preferred design.	The project is intended to improve efficiency in the Nowra-Bomaderry road network by reducing congestion. It is anticipated that this would also be of benefit to visitors to the area. The environmental assessment that has been prepared for the project includes an assessment of socioeconomic impacts and potential benefits.
171	Other	Other	1	Concerns over plans still in draft, what has the \$10million been spent on, a second-rate solution that will not solve the problems in Nowra/Bomaderry	Since the project commenced in late 2014, Roads and Maritime has carried out extensive and detailed investigations into numerous options for the project reflecting the large number of complex and inter-related issues associated with the project. The preferred option is considered to be the best balanced outcome that would contribute to alleviating the significant traffic congestion issues along this section of the Princes

Ref	Issue category	Sub-issue	Total number of submissions	Issue(s) raised	Roads and Maritime response
					Highway.
172	Other	Other	1	Appreciate the manner in which Roads and Maritime has conducted the community consultation surrounding the retention of the old bridge.	Support noted.
173	Other	Other	1	Supports proposed bridge site as planned.	Support noted.
174	Other	Other	3	Supports proposal for a new bridge to replace the existing southbound bridge.	Support noted.
175	Other	Other	1	Proposal to convert existing southbound bridge into restaurant/gift shop / 20 room hotel - an attraction for tourists making the most of the river.	Roads and Maritime has committed to the ongoing maintenance of the bridge, and to carry out further consultation into options for adaptive reuse which will be progressed separately to the project.
176	Other	Other	1	Lack of costings make it difficult for community to assess the options. In particular it is not possible to understand whether the cost of retaining the old bridge has precluded consideration of flyovers due to no money being left to pay for them.	Roads and Maritime has assessed relative costs as part of the options assessment process. The Preferred Option is considered to best meet the project objectives of which cost is just one consideration.
177	Other	Other	1	Keep concrete arch structure in park next to river.	The environmental assessment has identified this as a local heritage item. Impacts on this item are being considered through the Statement of Heritage Impact being prepared for the project. This will include

Ref	Issue category	Sub-issue	Total number of submissions	Issue(s) raised	Roads and Maritime response
					recommendations to mitigate and manage impacts which may include relocation of this item, and which would include consultation with relevant stakeholders including Shoalhaven City Council.
178	Other	Other	1	Proceed with the proposal.	Both the Australian and NSW governments have announced joint 50:50 funding for the \$310 million project and work is progressing to obtain the approvals required to commence construction.
179	Other	Other	1	Employ locals for the construction.	Procurement for the project would follow Roads and Maritime policies and procedures, however, matters relating to employment of the construction workforce would rest with the contractor appointed to construct the project.
180	Other	Other	1	Nowra Bypass and the river should be promoted for tourism.	The need for a bypass has been extensively examined and the traffic shows it would not address the local traffic demands in the area. Building a new bridge now will not preclude the future planning of a bypass if/when traffic demand is reached. Promotion of tourism rests with Council and other parties.
181	Other	Heritage	1	Preservation of heritage items in the vicinity of Nowra Bridge including Graham Lodge, Graham Family cemetery, Lynburn homestead and Illowra Homestead, and requests no physical impact on structures and minimal visual impact, and monitoring	The environmental assessment for the project has included a specialist investigation into likely and potential impacts on heritage. This has included recommendations to mitigate and manage impacts on heritage items.

Ref	Issue category	Sub-issue	Total number of submissions	Issue(s) raised	Roads and Maritime response
				for vibration during construction.	
182	Other	Heritage	1	Purchasing of Nowra Gateway Caravan Park so it can be repurposed as green space as compensation for impact to Moorhouse Park	This is a matter for council to consider. The project does not require acquisition of any land associated with the caravan park. There are no provisions under relevant legislation that would allow Roads and Maritime to acquire this land.
183	Other	Nowra Bomaderry	1	The project does not address the significant congestion in Nowra-Bomaderry. An increase in the overall capacity is urgently required.	The traffic modelling carried out for the project has considered traffic growth up to the year 2046. The modelling has identified that the project would contribute to alleviating congestion in the Nowra-Bomaderry road network, and has been informed by the 2014 traffic survey jointly carried out by Council and Roads and Maritime to update traffic models used in the assessment of upcoming projects. These projects include the assessment of Nowra Bomaderry urban release areas, the crossing of the Shoalhaven River and the Berry to Bomaderry upgrade of the Princes Highway.
184	Other	Nowra Bomaderry	1	There is likely to be significant upstream and downstream impact after construction of the Nowra Bridge project, and it will bring forward the need to extend three lanes of traffic in each direction continuous through Bomaderry and South Nowra.	The project would provide three lanes in each direction between Bolong Road and Moss Street. Roads and Maritime would continue to discuss with Council with regard to any future upgrades of other sections of the Princes Highway.
185	Other	School zone	1	Re-think the 40 km/hr limit school zone next to the Nowra Anglican College on highway.	This school is outside the project area. The NSW Government Centre for Road Safety sets the 40 km/h school zones to help protect children on their way to and from schools at the times and places where they are

Ref	Issue category	Sub-issue	Total number of submissions	Issue(s) raised	Roads and Maritime response
					often in high numbers. This lower speed limit reduces the risk and potential severity of a crash.

4 Recommendations

Roads and Maritime has assessed the feedback received from the community and has made changes to the concept design of the project. Roads and Maritime responded to community feedback by:

- Increasing the number of lanes on Illaroo Road to provide three dedicated right turn lanes and one dedicated left turn lane for traffic turning onto the Princes Highway
- Providing an acceleration and merge lane for northbound traffic turning Illaroo Road from the Princes Highway
- Amending the new local road connection to provide left in and left out access to the Princes Highway and removing the need for traffic lights at this location.

5 Next steps

Roads and Maritime will place Review of Environmental Factors (REF) on public display for a period of four weeks. The review of environmental factors assesses the potential environmental impacts of the project and identifies ways to manage them. We are seeking feedback on the environmental assessment document until Friday 28 September. All submissions made during the display period will be addressed and responded to in a submissions report and will be used to further develop the project.

Written submissions should be sent to Roads and Maritime and can be emailed to NowraBridgeProject@rms.nsw.gov.au

The project team will be at the following locations during the display period:

- Nowra School Of Arts, Berry Street, Nowra on Saturday 1 September, 11am – 3pm; and Thursday 6 September, 4 – 8pm
- Stocklands Nowra, 60 East Street, Nowra on Thursday 13 September, 4 – 8pm
- North Nowra Shops, 1–13 McMahons Road, North Nowra on Saturday 8 September, 11am – 3pm; and Wednesday 12 September, 11am – 3pm.

The REF for Nowra Bridge project will be on display until Friday 28 September 2018 at the following locations:

- Shoalhaven City Council, 36 Bridge Road, Nowra from 9.15am – 4.30pm Monday to Friday
- Nowra Library, 10 Berry Street, Nowra from 9.30am – 7pm Monday to Friday; and 9.30am – 3pm Saturday
- TAFE NSW Nowra, 60 Belinda Street, Bomaderry, from 9am – 4pm Monday to Friday.

Roads and Maritime will continue to consult with Council on issues relating to the riverfront precinct, local road changes and other local planning matters.

We will keep the community updated as the project progresses.

Appendix A – Media release



Barnaby Joyce
Deputy Prime Minister
Minister for Infrastructure and Transport



Andrew Constance
NSW Minister for Transport

Shelley Hancock

NSW Member for South Coast
Ann Sudmalis
Federal Member for Gilmore
Gareth Ward
SW Member for Kiama

19 February 2018

Preferred option released for the Nowra Bridge project

Nowra residents are invited to have their say on the preferred option for a new bridge across the Shoalhaven River, with the options report released today by the Australian and New South Wales governments.

The Australian Government has committed \$10 million and the NSW Government a further \$1.6 million for planning of the new bridge which is expected to be fully complete by late 2019.

Deputy Prime Minister and Minister for Infrastructure and Transport Barnaby Joyce said planning was progressing for the new bridge, along with intersection upgrades on the Princes Highway between Bolong Road and Bridge Road.

“This design is about planning for the future and takes into consideration the number of lanes the new bridge will need, as well as intersection layouts and the future of the old southbound bridge,” Mr Joyce said.

NSW Minister for Transport, Andrew Constance said the project’s small footprint and high traffic volumes in the area made the process of identifying, developing and evaluating options for the new bridge and intersection upgrades quite complex.

“Almost 40 possible road network options were identified, assessed and shortlisted to six. Those options were then tested and compared taking into account traffic performance, ease of construction, environmental and heritage effects, maintenance, value for money and stakeholder feedback,” Mr Constance said.

Federal Member for Gilmore Ann Sudmalis said the preferred option released today provided the best balance between travel time savings, value for money and limiting environmental and property impact.

“The design features a new northbound four lane bridge, reconfiguring the existing concrete bridge to carry southbound traffic, intersection upgrades at Illaroo, Bolong and Bridge roads, a new Pleasant Way T-intersection and keeping the old southbound bridge for pedestrians and cyclists,” Mrs Sudmalis said.

NSW Member for South Coast Shelley Hancock said she was keen to see the Nowra community provide feedback at a series of information sessions to be held in the near future.

“Five community drop-in sessions will be held in the coming weeks so people can see the preferred option and give feedback. We really want to see residents get involved so we can deliver the best traffic network possible,” Ms Hancock said.

The first two sessions will be held between 11am and 3pm Thursday 1 and Saturday 3 March at Stocklands Nowra. The third and fourth sessions will be held between 11am and 3pm Thursday 8 and Saturday 10 March at North Nowra Shops. The final session will be held between 10am and 1pm Saturday 17 March at Nowra School of Arts Annex.

NSW Member for Kiama Gareth Ward said the new bridge would be great for local tourism and local residents.

“We have seen record spends on our region’s roads since 2011 and a new bridge will help build on these major investments. The old Nowra Bridge has reached the end of its serviceable life and we cannot leave the people of Nowra and Bomaderry with one bridge. This is about future-proofing our region with a modern and new infrastructure,” Mr Ward said.

The preferred option will also be on display from Monday 19 February until Friday 23 March at Shoalhaven City Council premises, Nowra Library and TAFE NSW Nowra.

Feedback is invited until Friday 23 March. To find out more about the project including details on the drop-in sessions, email NowraBridgeProject@rms.nsw.gov.au.

Media Contacts

For Mr Joyce:



For Mr Constance

For Mrs Sudmalis:

For Ms Hancock:

For Mr Ward:

Appendix B – Newspaper advertisements



Australian Government

BUILDING OUR FUTURE

Nowra Bridge project - Princes Highway upgrade

Have your say

The NSW Government has invested \$1.6 million, with the Federal Government committing \$10 million, for planning a new bridge on the Princes Highway over the Shoalhaven River at Nowra.

Your feedback is invited from **Monday 19 February 2018** until **Friday 23 March 2018** on the concept design for the preferred option for a new northbound four lane bridge at Nowra with upgraded intersections at Bolong Road, Illaroo Road, Bridge Road and a new Pleasant Way intersection.

Old Nowra Bridge will be retained and maintained for pedestrians and cyclists.

Drop in and see us at:

- Stockland Nowra
Thursday 1 and Saturday 3 March 2018
11am – 3pm
- North Nowra Shops
Thursday 8 and Saturday 10 March 2018
11am – 3pm
- Nowra School of Arts Annex
Saturday 17 March 2018
10am – 1pm

Community display locations

More information on the preferred option is available at unstaffed displays at the following locations:

- Shoalhaven City Council
- Nowra Library
- TAFE NSW Nowra

For further information and to have your say

Further information including the Preferred Option Report and Preferred Option Survey is available on the project website at **rms.nsw.gov.au/nowrabridge**

Appendix C – Community update flyer



Australian Government

BUILDING OUR FUTURE

Nowra Bridge project – preferred option

Have your say



February 2018

The NSW Government has invested \$1.6 million, with the Australian Government committing \$10 million, for planning a new bridge on the Princes Highway over the Shoalhaven River at Nowra.

A preferred option has been developed for the project and includes:

- A new northbound four lane bridge
- Reconfiguration of the existing concrete bridge to carry three lanes of southbound traffic
- Intersection upgrades at Illaroo Road, Bolong Road and Bridge Road
- New Pleasant Way T-intersection
- Keeping the old southbound bridge for pedestrians and cyclists.

Key benefits of the preferred option:

- Improving safety, capacity and efficiency across the Shoalhaven River at Nowra
- Improving freight movements and access to the South Coast of NSW
- Better traffic flow now and into the future
- Improving pedestrian connectivity
- Preserving the heritage of the old southbound bridge.



1800 331 713



NowraBridgeProject@rms.nsw.gov.au



M1 / Princes Highway upgrade – Roads and Maritime Services



Nowra Bridge Project
PO Box 477
Wollongong NSW 2500

Drop in and see us

To view the preferred option, meet the project team and find out more about the project, drop into a number of community information kiosks.

Stocklands Nowra

60 East Street, Nowra
Thursday 1 and Saturday 3 March 2018
11am – 3pm

North Nowra Shops

1–13 McMahons Road, North Nowra
Thursday 8 and Saturday 10 March 2018
11am – 3pm

Nowra School of Arts Annex

Berry Street, Nowra
Saturday 17 March 2018
10am – 1pm

You can also submit your feedback by using our contact details below.

Feedback is invited until Friday 23 March 2018.

Display locations

The preferred option for Nowra Bridge project will be on display from Monday 19 February until Friday 23 March 2018 at the following locations:

Shoalhaven City Council

36 Bridge Road, Nowra
9.15am – 4.30pm Monday to Friday

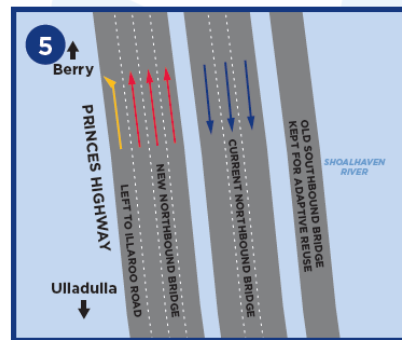
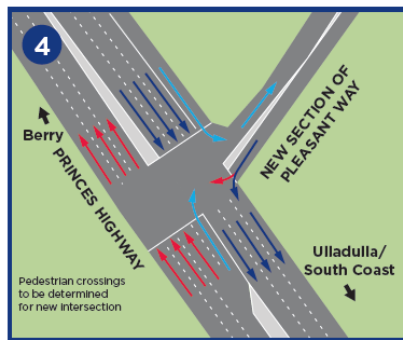
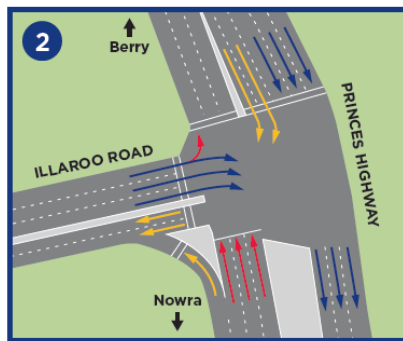
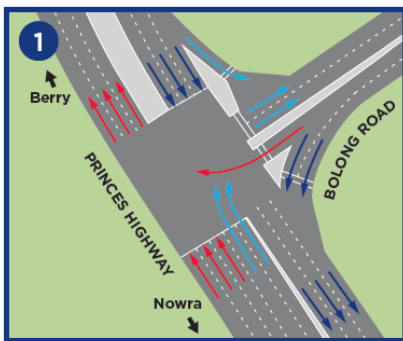
Nowra Library

10 Berry Street, Nowra
9.30am – 7pm Monday to Friday,
9.30am – 3pm Saturday


TAFE NSW Nowra

60 Belinda Street, Bomaderry
9am – 4pm Monday to Friday

Nowra Bridge project preferred option



 Better and more **reliable trips** for people, business and freight

 **Save travel time**

 **Improving traffic flow**

 **Intersection improvements**

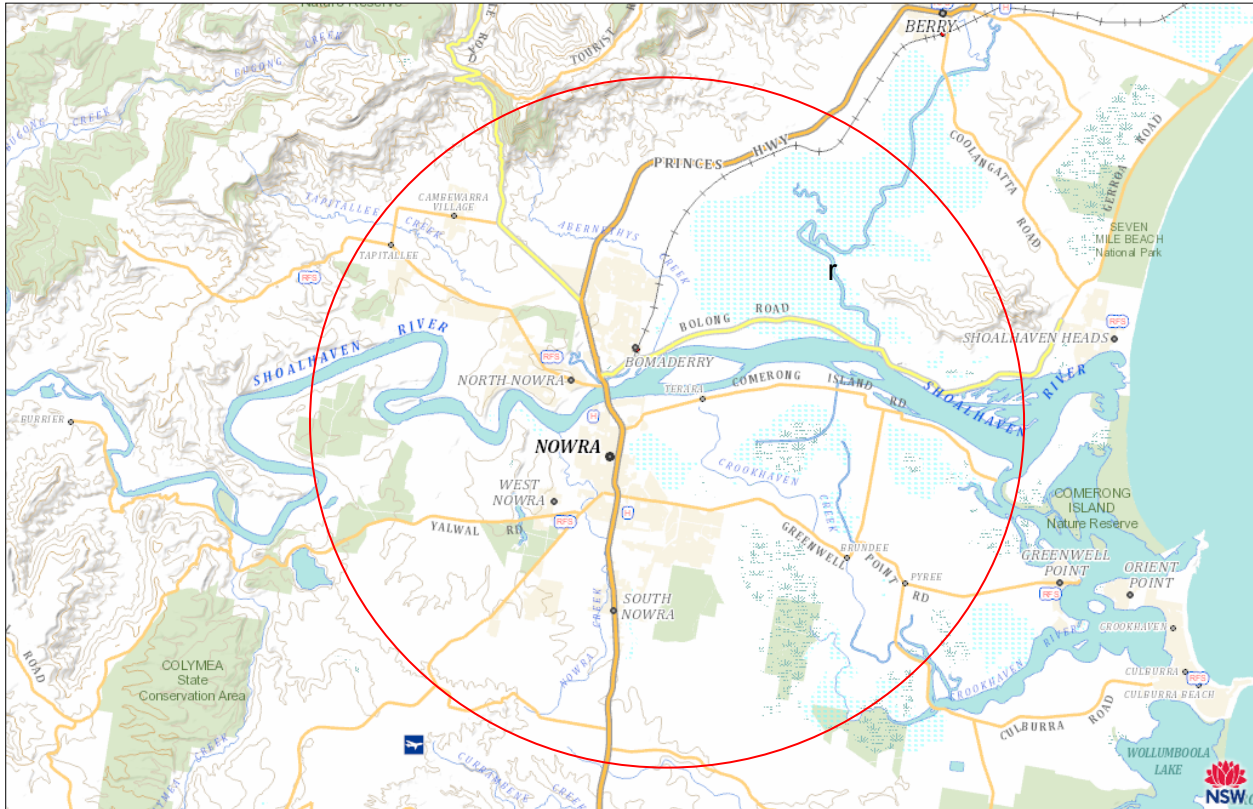
 **New pathways for active transport**

More information

More information including the Nowra Bridge Project Preferred Option Report is available on the project page at rms.nsw.gov.au/nowrabridge

Appendix D – Distribution map

Distribution map: Nowra Bridge project preferred option announcement February 2018



Appendix E – Community update



Australian Government

BUILDING OUR FUTURE



Nowra Bridge project

Princes Highway upgrade

Have your say – Preferred option

February 2018



Artist's Impression - Perspective of the new four lane bridge over the Shoalhaven River - view from the south west side of the river

The NSW Government has invested \$1.6 million, with the Australian Government committing \$10 million, for planning a new bridge on the Princes Highway over the Shoalhaven River at Nowra.

Background

Roads and Maritime Services is planning for a new Princes Highway bridge over the Shoalhaven River at Nowra. The project would provide upgraded intersections at Bolong, Illaroo and Bridge Roads and at Pleasant Way.

We have been investigating options to determine the appropriate number of lanes required for the new bridge, intersection layouts, and the future of the old southbound bridge.

Preferred option

A preferred option for the project has been identified and is on display for community feedback from Monday 19 February until Friday 23 March 2018.

The preferred option includes:

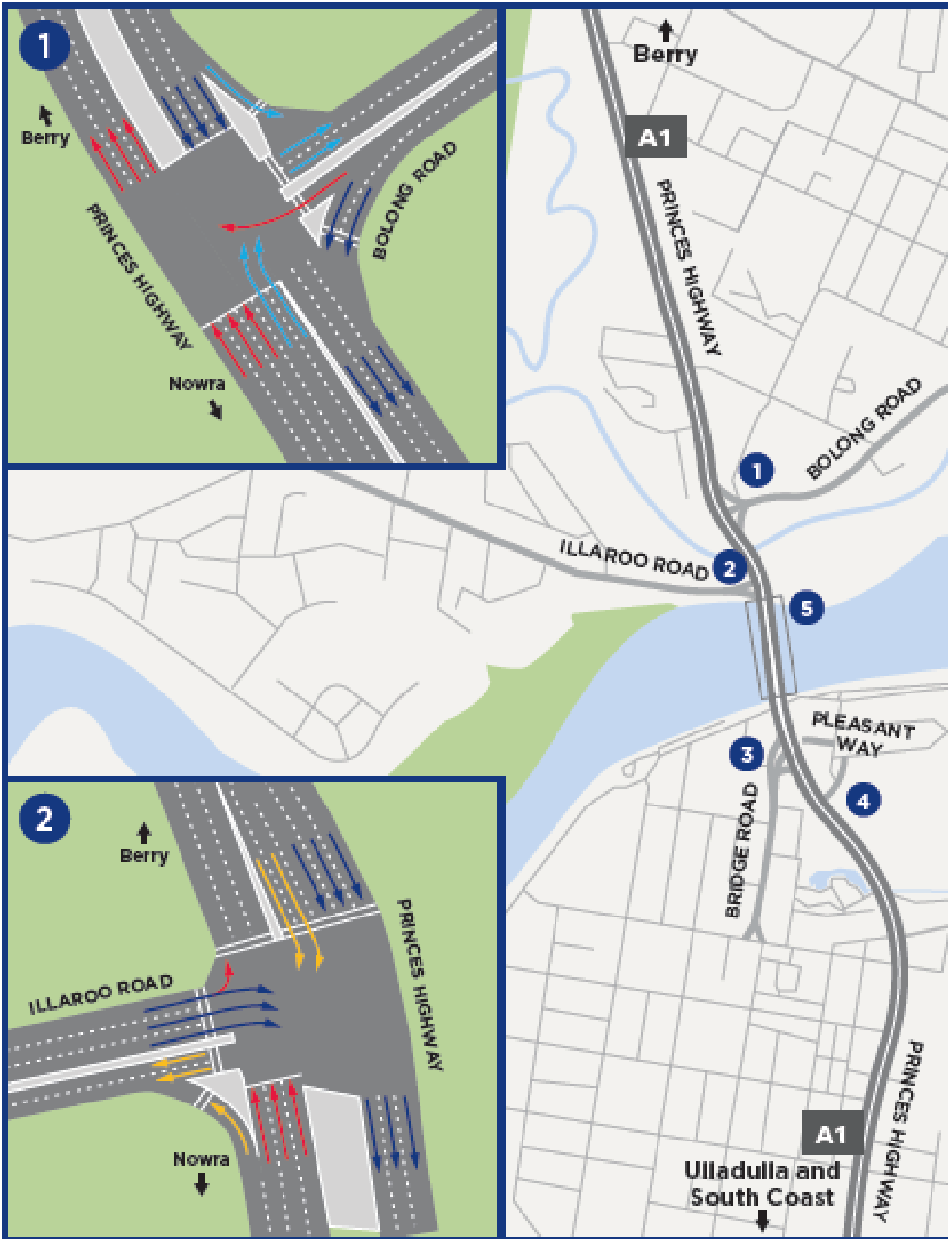
- A new northbound four lane bridge
- Reconfiguration of the existing concrete bridge to carry southbound traffic
- Intersection upgrades at Illaroo Road, Bolong Road and Bridge Road
- New Pleasant Way T-Intersection
- Keeping the old southbound bridge for pedestrians and cyclists.

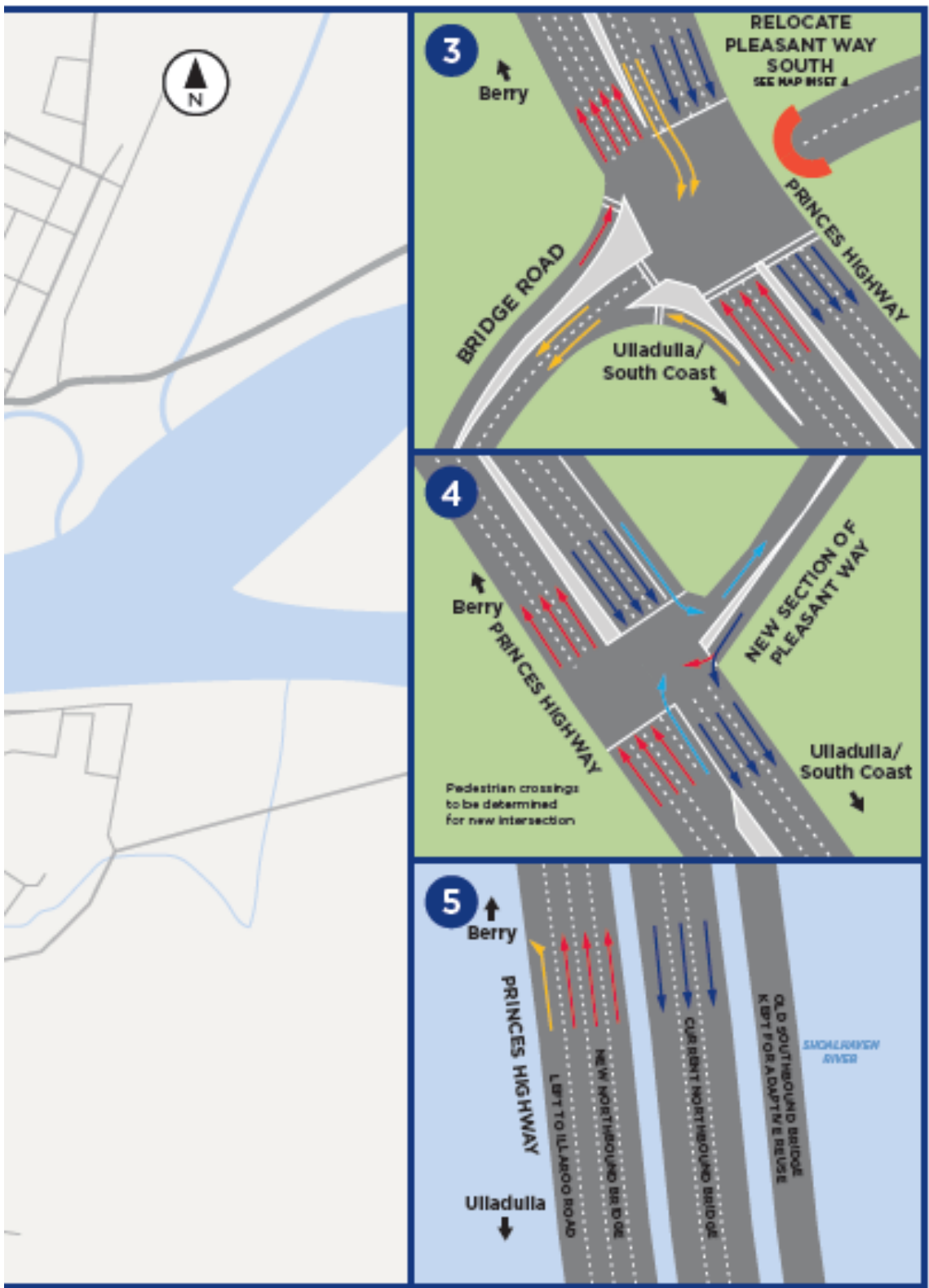
More information on the preferred option can be found in the *Nowra Bridge Project Preferred Option Summary* available at rms.nsw.gov.au/howrbridge.

Have your say

See the back on this community update for details on how you can give us your feedback.

Preferred Option - Nowra Bridge project






1 Princes Highway / Bolong Road intersection

- Minor adjustments to be confirmed during concept design.

2 Princes Highway / Illaroo Road intersection

- An additional right turn lane onto the Princes Highway from Illaroo Road
- An additional right turn lane from the Princes Highway onto Illaroo Road.

3 Princes Highway / Bridge Road intersection

- Relocated further south, see inset 
- Converted to a T-intersection
- Access to Pleasant Way closed.

4 New Princes Highway / Pleasant Way intersection

- New T-intersection located further to the south
- All turning movements catered for.

5 Bridges over the Shoalhaven River

- New northbound bridge
 - Located west of the existing bridges
 - Four northbound traffic lanes
 - Pedestrian/cyclist facilities.
- Existing concrete bridge
 - Southbound traffic would use the existing concrete bridge
 - Southbound capacity would increase from two to three lanes.
- Existing old southbound bridge
 - Kept for adaptive reuse such as a shared pedestrian and cyclist path
 - Closed to vehicular traffic once construction of the new bridge is complete.

Key benefits of the preferred option:

- Improving safety, capacity and efficiency across the Shoalhaven River at Nowra
- Improving freight movements and access to the South Coast of NSW
- Better traffic flow now and into the future
- Improving pedestrian connectivity
- Preserving the heritage of the old southbound bridge.



New pathways
for active transport



Save travel time



Intersection improvements



Better and more
reliable trips
for people, business and freight



Improving traffic flow

More information on the preferred option can be found in the Nowra Bridge Project Preferred Option Report available rms.nsw.gov.au/nowrabridge.

Option development and assessment

The Nowra Bridge project area includes the old Nowra Bridge, a concrete bridge and four intersections. The area experiences high traffic volumes that made development and assessment of potential options complex. A short summary of the decision making process is below with more detailed information in the Nowra Bridge Project Preferred Option Report available to view at rms.nsw.gov.au/nowrabridge.

Future of the old southbound bridge

The future of the old southbound bridge has been the subject of extensive investigation by RMS and external experts.

To reach a recommendation, various options were considered and assessed. The criteria used for assessment included the significance of the bridge, the expected benefits, estimated costs and community and stakeholder feedback and expectations.

We are proposing to keep the old southbound bridge for pedestrian and cyclist use. Key findings of the reviews found:

- There are considerable heritage and social impacts associated with any option to remove the bridge (in whole or part)
- The bridge is operationally unsuitable to cater for future traffic demands and should be closed to vehicles once a new bridge crossing of the Shoalhaven River is open to traffic.

Intersection options

Due to constraints including high traffic volumes in the area, the process of identifying, developing and evaluating options for the new bridge and intersection upgrades was complex.

Over 19 options for the project were initially identified, assessed and shortlisted to six main network options. These options were then tested and compared to each other considering:

- Traffic performance
- Ease of construction
- Property impacts
- Environmental and heritage impacts
- Road and urban design options
- Future maintenance needs
- Value for money
- Key stakeholder feedback.

The preferred option is a balanced proposal which addresses the project's objectives.

Involving the community

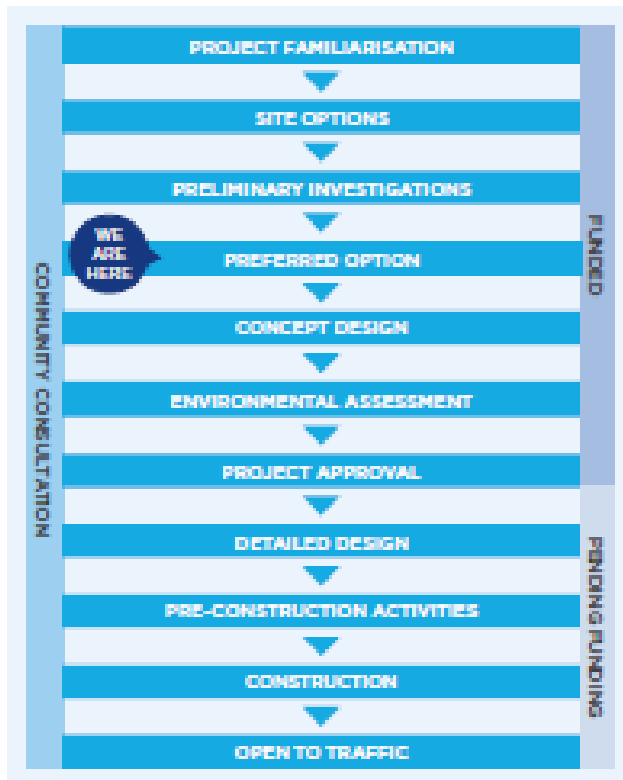
We are continuing to work with the community and stakeholders during the planning process to identify issues and minimise potential impacts.

Feedback received will be considered to finalise the concept design and prepare environmental assessment for the project.



Artist's impression - Perspective of the new four lane bridge over the Shoalhaven River - view from the north west side of the river

Project development process



Drop in and see us

Find out more about the project by dropping into a number of community information kiosks at:

- **Stocklands Nowra**
60 East Street, Nowra
Thursday 1 and Saturday 3 March 2018, 11am – 5pm
- **North Nowra Shops**
1-13 McMahons Road, North Nowra
Thursday 8 and Saturday 10 March 2018, 11am – 3pm
- **Nowra School of Arts Annex**
Berry Street, Nowra
Saturday 17 March 2018, 10am – 1pm

Display locations

The preferred option for Nowra Bridge project will be on display from Monday 19 February until Friday 23 March 2018 at the following locations:

- **Shoalhaven City Council**
36 Bridge Road, Nowra
9.15am – 4.30pm Monday to Friday
- **Nowra Library**
10 Berry Street, Nowra
9.30am – 7pm Monday to Friday,
9.30am – 3pm Saturday
- **TAFE NSW Nowra**
60 Belinda Street, Bomaderry
9am – 4pm Monday to Friday

Further information

More information, including frequently asked questions, is available on the project page at rms.nsw.gov.au/nowrabridge

Have your say

Feedback on the preferred option is invited until Friday 23 March 2018.

Please send your feedback to:



1800 331 713 (toll free)



NowraBridgeProject@rms.nsw.gov.au



M1 / Princes Highway upgrade -
Roads and Maritime Services



Nowra Bridge Project
PO Box 477
Wollongong NSW 2500

Next steps

Upcoming planning activities include:

- Reviewing community feedback
- Advancing the concept design
- Preparing the environmental assessment for the proposal.

The timing of construction is not confirmed and is subject to planning approval and funding availability.



If you need help understanding this information, please contact the Translating and Interpreting Service on 131 450 and ask them to call us on 1800 331 713.



February 2018
RMS 18.722

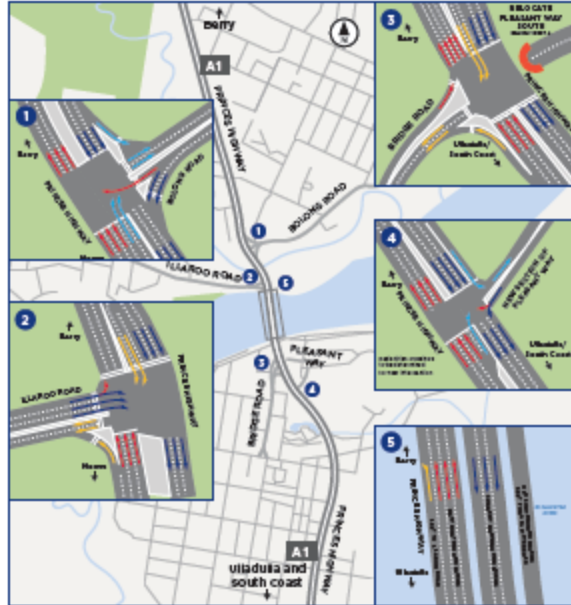
Privacy Roads and Maritime Services ("RMS") is subject to the Privacy and Personal Information Protection Act 1998 ("PPPI Act") which requires that we comply with the Information Privacy Principles set out in the PPPI Act. All information in correspondence is collected for the sole purpose of assisting in the delivery of the project. The information received, including names and addresses of respondents, may be published in subsequent documents unless a clear indication is given in the correspondence that all or part of that information is not to be published. Otherwise RMS will only disclose your personal information, without your consent, if authorised by the law. Your personal information will be held by RMS at 60 Crown Street, Wollongong NSW 2500. You have the right to access and correct the information if you believe that it is incorrect.

Appendix F – Display poster

February 2018

The NSW Government has invested \$1.6 million, with the Australian Government committing \$10 million, for planning a new bridge on the Princes Highway over the Shoalhaven River at Nowra.

A preferred option has been developed and is now on display. Stakeholders and the community are invited to provide feedback from **Monday 19 February** until **Friday 23 March 2018**.



Drop In and see us

To view the preferred option, meet the project team and find out more about the project, drop into one of our community information kiosks.

Stocklands Nowra
60 East Street, Nowra
Thursday 1 and Saturday 3 March 2018
11am – 3pm

North Nowra Shops
1-13 McMahons Road, North Nowra
Thursday 8 and Saturday 10 March 2018
11am – 3pm

Nowra School of Arts Annex
Berry Street, Nowra
Saturday 17 March 2018
10am – 1pm

You can also submit your feedback by using our contact details below.

Display locations

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9.30am – 3pm Saturday

TAFE NSW Nowra
60 Belinda Street, Bomaderry
9am – 4pm Monday to Friday

More information including the Nowra Bridge Project Preferred Option Report is available on the project page at rms.nsw.gov.au/nowrabridge

Key benefits of the preferred option:

- Improving freight movements and access to the South Coast of NSW
- Improving safety, capacity and efficiency across the Shoalhaven River at Nowra
- Better traffic flow now and into the future
- Improving pedestrian connectivity
- Preserving the heritage of the old southbound bridge.



1800 331 713

NowraBridgeProject@rms.nsw.gov.au

PO Box 477, Wollongong NSW 2500

M1 / Princes Highway upgrade - Roads and Maritime Services



1800 331 713



Nowra Bridge Project
Roads and Maritime
PO Box 477,
Wollongong NSW 2520



rms.nsw.gov.au/NowraBridge

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